

Strafford, Missouri

DOWNTOWN
REVITALIZATION &
ECONOMIC
ASSISTANCE FOR
MISSOURI



BUILDING AND
STREETScape
DESIGN GUIDELINES
NOVEMBER 2011



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CITY OF STRAFFORD, MISSOURI

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1.0: INTRODUCTION

1.1 Historic Overview

Stafford is a small community located in Green County, Missouri, just east of the City of Springfield, and incorporated as a village in 1861. The area is famous for its trails and transportation links over the years including early Native American paths that eventually became immigration routes for westward travelers. The Trail of Tears (the route used for the relocation of Cherokee Indians by the government) and the Military Road (used by Union and Confederate troops to move men and equipment during the Civil War) were in close proximity to present day Stafford. The community established itself after the Civil War with the construction of a railroad line between St. Louis and San Francisco. The men who helped to build those tracks named the community. The Atlantic and Pacific Railway historically provided a link to distant markets for crops and goods produced in and around Stafford, Missouri, and helped the city develop into a center of commerce for the immediate area. Integral to the town's growth was Downtown serving as the commercial and civic hub for Stafford. The introduction of historic Route 66 would dramatically increase Downtown's importance. Similar to many towns in the country, Downtown served the local community as the place to go for trade, religious services, education, and socializing. Although the city's founding and initial growth were due to the railroad, it was the rise of the automobile and location of Route 66 through Downtown that had the most significant impact on the city's growth and identity.

In the 1950's, American lifestyles changed with interstate highway construction and the affordable automobile. Freeways were constructed to bypass the congested portions of cities resulting in neighborhoods and commercial areas moving further away from the traditional downtown business district. Downtowns, while still the center of much community life, started to experience a loss of commercial viability. Consumers expected easy access and parking for their cars. New and modern design was preferred over traditional and old. As a result, by the mid 1970's many American downtowns suffered from a lack of investment. Downtown Stafford was no exception. Interstate 44, built north of Downtown Stafford, supplanted Route 66 as the major roadway and many businesses moved away from Downtown. Eventually buildings in Downtown began to fall into disrepair or were renovated to appear more modern. The random renovation of existing structures or the demolition of entire buildings had a significant and negative impact on the architectural character of Downtown Stafford.



Former Railroad Depot, Stafford



Aerial view of Stafford- 1954

1.2 Intent of Guidelines

While these guidelines are written for Downtown Strafford, the design recommendations are sound advice with applicability elsewhere in the community. These guidelines are written primarily for commercial areas, however the guidelines also briefly address issues regarding residential buildings and sites. The main intent of the guidelines is to help provide architectural character and improve the visual appearance of Downtown Strafford.

In America, downtowns traditionally have had a sense of place. Businesses, courthouses, city halls, shops and houses of worship were almost always located in the downtown. Downtown was the business and civic center of the community. The architectural style, size and materials of the buildings in downtown often reflected the success and wealth of the community. In Strafford, the strength and vitality of Downtown centered first around the railroad and then Route 66. Downtown was the center of commerce, transportation, and the community during this period. Eventually Interstate Highways would supersede the older Federal Highway system and construction of I-44 just north of Downtown relegated Route 66 to lesser status. Development in Downtown Strafford mirrored this trend and began to stagnate to the point where eventually there were very few significant buildings left.

Downtown Strafford has a very limited number of buildings which individually have any design merit and character. However, collectively these buildings and other structures, although lacking a conformity of architectural character, still contribute to the existing context of Downtown. The guidelines will provide the recommendations to help preserve the existing buildings and spaces which still have merit. This report will also suggest methods of improvements for buildings which have lost their character due to alterations or neglect. Restoration of buildings to the original design is not the goal of the guidelines, especially since there are no buildings on the National Register of Historic Places in Downtown. The guidelines focus on improving the public façade and appearance of structures, streets and public spaces to help create a positive identity for Downtown. There are three types of structures which form the collective whole of Downtown: those that contribute, those that detract, and those that do neither. The objective is to maximize contributing elements and minimize detracting elements thus creating a stronger, more attractive Downtown Strafford.

The improved identity and appearance of Downtown Strafford will provide an incentive for more redevelopment and interest. To successfully support revitalization, the property owners, City staff, and other community organizations will need to make a long term commitment to Downtown Strafford. The guidelines are a resource for local leaders who agree to the commitment of improving the collective visual appearance and activity of Downtown. The improved appearance and identity will then reflect the investment, vitality and new civic pride of Downtown Strafford.

1.3 Existing Context

Downtown Strafford, Missouri, differs from the classic Midwest downtown layout in that there is no courthouse square or civic space surrounded by mercantile, office buildings, and other institutions. Instead, Downtown has two primary streets, running east-west for about 2-3 blocks, which act in a more typical “main street” format. These streets are Pine and Old Route 66. Pine Street has buildings along both sides with occasional vacant lots creating gaps. Old Route 66 consists of buildings along the north side and none on the south side which is fronted by the railroad right-of-way. Interestingly, some of the buildings along Old Route 66 are actually the back of the buildings fronting Pine but were converted to have a second front entrance to face Route 66. Location of the Post Office, Fire House, and City Hall form a government services anchor at the western edge of the DREAM boundary while banking, gas stations, and retail uses form an anchor at the eastern edge of the district along Highway 125. Also, the Strafford School District campus is located on the northern boundary of the DREAM area. Together these anchors and the uses between them comprise a loosely defined central business district for the City of Strafford.

The primary access route into Downtown Strafford is Highway 125 from the north, which connects at an interchange on I-44 to Old Route 66 (Highway 125). Old Route 66 then runs east-west parallel to I-44. Pine Street also connects to 125, as does Chestnut Street further to the north. Other streets running north-south through Downtown include Jefferson, Madison, and Washington, the latter of which provides the only at-grade crossing of the railroad tracks south of Old Route 66 within the DREAM area. South of the tracks are Commercial and Olive Streets, running east-west, housing a mix of residential and light industrial uses.

Downtown contains a fairly indiscriminate massing of buildings with a number of vacant parcels. Most of the buildings have little to no architecturally significant elements, and several were the subject of inappropriate alterations, including scale, materials, signage, and finish. Existing buildings along Pine Street with negative alterations include the MFA Building with the addition of a sloped roof requiring an angled cut to the cornice and dental molding. Also, the Family Pharmacy building was remodeled to significantly alter the façade with the addition of new brick on the ground level and metal siding completely wrapping the second floor causing the loss of any existing windows and overall historic character. There are also more recent building rehabilitations which utilized inappropriate windows, doors and siding materials.

Through the differing styles, alterations, and building stock Downtown Strafford still provides a tremendous opportunity to enhance the architectural character and civic identity of Strafford. As Downtown Strafford experienced numerous changes throughout history, many positive attributes still exist and provide an opportunity for a design identity to be forged toward a unique built environment.



Existing commercial buildings of Downtown Strafford.

1.4 Downtown Strafford Map



Exhibit 1 DREAM Boundary

Downtown Study Area City of Strafford, Missouri

Legend [Red Box] Study Area

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2.0 BUILDING DESIGN GUIDELINES

2.1 Rehabilitation and Maintenance Guidelines for Existing Buildings

Any original element or material that still exists, particularly on the storefront, should be retained if possible. Original elements provide a historic value that can not be replaced. Prism glass in transom windows or a decorative wooden door with beveled glass would be examples of original materials.

Replacement of missing architectural elements should be based on accurate duplications of original features. When an entire detail must be reconstructed the new material should match the original in design, color, texture, and other visual qualities. Where reconstruction of an element is impossible because of a lack of historical evidence, then a new design that relates to the building in general size, scale and material may be considered. Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be acceptable.

2.1.1 Differences Between Rehabilitation, Restoration and Renovation

The Standards for Rehabilitation define *rehabilitation* as "the act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values." Rehabilitation should be distinguished from *restoration*, which is "the act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work or by the replacement of missing earlier work."

As opposed to rehabilitation and restoration, *renovation* seeks to modernize a building. Little attention is paid to retaining historically significant architectural features of a building. Renovation, by its very nature, destroys the historic integrity of a building. Once a building is renovated it may no longer be eligible for rehabilitation tax credits or listing on national or local historic registers.



Downtown Strafford - Pine Street, looking east



Downtown Strafford - Old Route 66, looking east

2.1.2 The Benefits of Rehabilitation

The Benefits of Rehabilitating Buildings

Rehabilitation of buildings can provide numerous long term benefits to the property owner and for the collective good of Downtown.

The appropriate rehabilitation of existing buildings will add to the value of the structure. Rehabilitation of façades, updating mechanical, electrical and plumbing systems and new finishes is an investment. The rehabilitation helps to limit long term maintenance costs. Repairs to a building due to code and safety reasons not only allow the building to be used but such repairs also make the building more marketable. The investment creates new value in a building and allows for a positive reflection on the occupant and owner. Rehabilitation of a individual building also adds to the overall image of Downtown.

Rehabilitation of the Front Façade

The front façade of a building is the first image a customer or user often sees. The image needs to be positive so that a customer or user will want to enter the building. The rehabilitation of the front façade is critical for the occupying business and is more inviting when completed in a scale and proportion which respects the original building as well as the user.

Such rehabilitation efforts will contribute to creating marketable space. The appropriate rehabilitation of a building will also create a standard for the front or public façades in Downtown. A high standard for the design and rehabilitation of public façades will eventually produce a collective visual quality for Downtown Strafford.



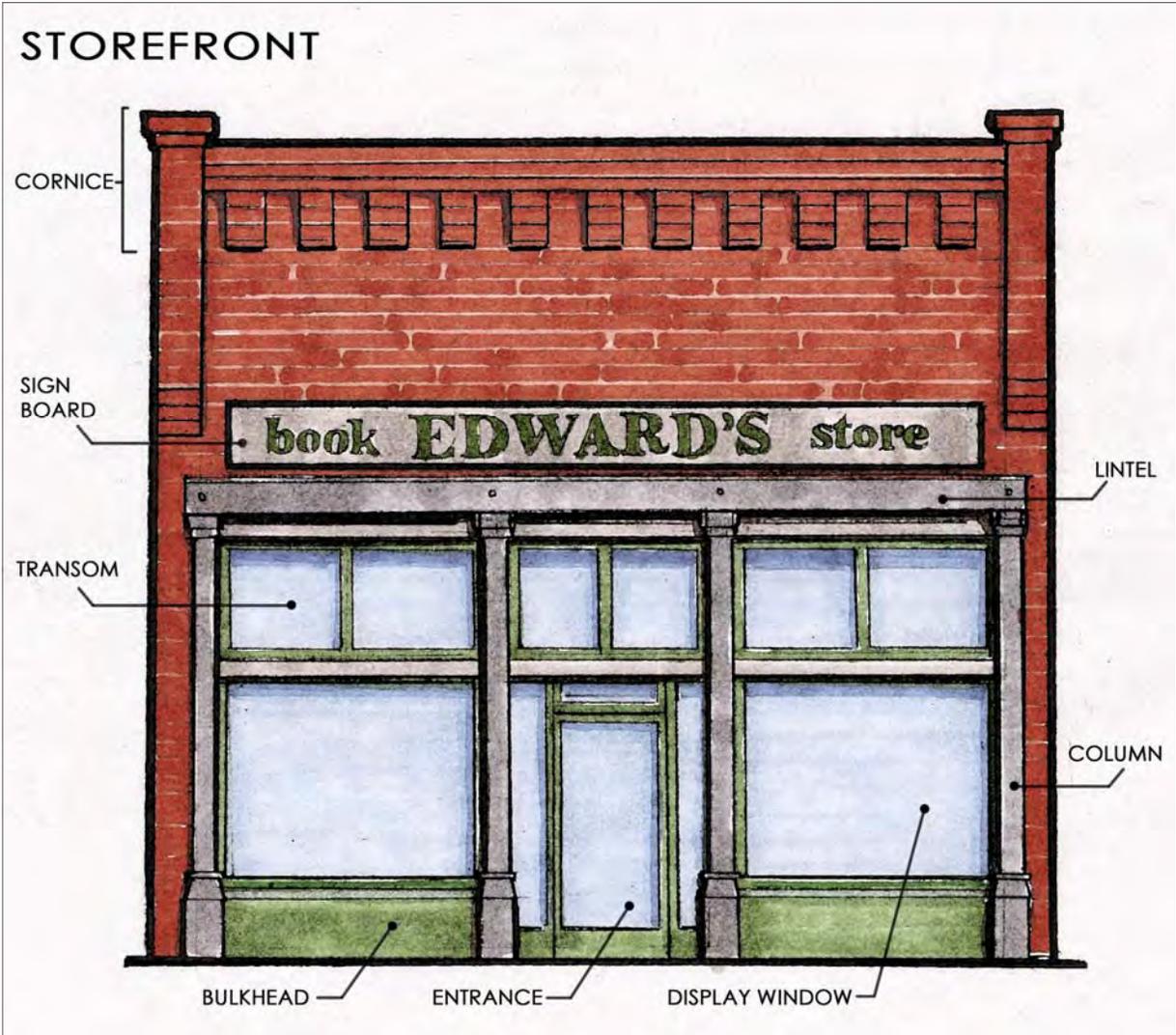
Downtown Strafford - Buildings along Pine Street



Well designed & coordinated storefront.
(St. Charles , Missouri)

2.1.3 Building Zones

Improvements to individual buildings will be discussed in the context of three distinct 'zones'; the Storefront, the Upper Façade, and the Rear Elevation. The elements of the Front Façade zones are depicted in this diagram.



2.1.4 Façade Elements

The various elements of a façade must be balanced. Appropriate massing, building and floor heights, proportions, roof lines, materials, and setbacks are critical considerations in new construction. Any future development should be encouraged to implement a design that contributes to the fabric of Downtown. Any future design that will detract from the fabric should be denied.

Other aspects like architectural details, colors, and cornices are more important to the restoration of historic buildings, but can be used effectively in new construction as well. Developing a well balanced character between all elements can allow a building to be very individual in its charm, but at the same time be a complementary thread woven into the overall fabric and feel of Downtown.

2.1.5 Rhythm and Scale

Adherence to uniform lot widths, building widths, and window spacing will work to establish a defined rhythm along street frontages in Downtown Strafford.

- New infill buildings and structures should maintain the rhythm through proper repetition of details and orientation to the street.
- Rehabilitation efforts, new construction and building amendments should work in a unified manner to emphasize this rhythm.
- Vertical elements, entrances, lighting, and other street furnishings can also develop the rhythm of a specific block.



Existing vacant building lot along Pine Street.



Proposed example of new building infill.

2.1.6 Alterations

Encourage removal of inappropriate alterations or additions that disrupt the fabric of the Storefront Zone. It is possible that non-historic and new construction can complement the building fabric that has developed, therefore some alterations may not need to be removed. Decks, ADA structures, and other 'detachable' alterations can be utilized, but should be as unobtrusive as possible and located on the rear or sides of the building.

As a rule, any and all alterations or additions to the Upper Façade zone should be removed. Alterations in this zone can significantly change the appearance of the face of the building. This includes any and all signs and lighting, as these should be restricted to the Storefront Zone. Avoid removing or altering any historic material or significant architectural features. Care should be taken during the removal process due to the possibility of damaging original elements hidden behind the alterations. When disassembly of a historic element is necessary, use methods that minimize damage to the original materials.



Original design.



Minor alterations.



Storefront is lost.



Significant alterations.



2.1.7 Masonry

Masonry is typically the preferred façade material for downtowns. Most existing construction will utilize some masonry. In most instances metal and wood siding are not comfortable choices for the downtown building fabric. These types of siding provide harsh lines, stark contrast, and no relief or warmth to the buildings. If wood was the historic material, it may be restored.

- Maintain the original color and texture of masonry walls. Stucco or paint should not be removed from historically painted or stucco masonry walls. Likewise, paint or stucco should not be applied to historical masonry walls.
- Clean masonry and mortar only when necessary to limit deterioration or to remove heavy soiling. Sandblasting, caustic solutions, and high-pressure water blasting should not be used. These methods erode the surface and accelerate deterioration.
- Masonry restoration, particularly on historic structures, should be done with great care.
- If the masonry has been painted or stained, a minimally intrusive removal process should be used.
- Never resort to sand blasting as this will permanently damage the brick.
- Unpainted masonry should remain natural, not painted or sealed.
- Damaged masonry should be repaired or replaced with similar color, texture, and style masonry products. Re-point masonry walls when there is evidence of disintegrating mortar, cracks in mortar joints, loose bricks, or moisture retention in the walls. The new mortar should duplicate the old mortar in composition, bonding strength, profile, color, and texture. Do not use cement mortar in brick construction; cement is far too hard and will cause spalling and cracking of the softer bricks.



Masonry façade is poorly maintained.
(Mexico, Missouri)



Maintenance of masonry façade.

- Re-pointing should be done with an appropriate mortar material with a consistent color across the entire façade and all elevations.
- Masonry replacement and/or repair should only be done with appropriate materials.
- Portland cement as a patch for masonry is unacceptable.
- If a historic façade has been covered with metal or wood siding it should be removed. Exposing the underlying brick masonry will help re-establish the character of the building and contribute to the visual continuity of the block. Siding also hides interesting details that can enhance building identity. If, after removing the covering material, portions of the original must be replaced, use a material that is similar to the original in color and texture.

Strafford, Missouri



Brickwork which has been inappropriately painted.



Metal-cladding, covered brickwork and new, mismatching brickwork, which has no tie into Downtown framework.

2.1.8 Windows

Windows are a major feature of the building exterior and vary with each building style. Windows have a proportional relationship to the structure as a whole, and they also have a decorative function. The shape and glazing pattern of windows on a building may be one of the principle characteristics in identifying its historic period and style. Thus, if original windows are removed and replaced with incompatible modern windows, the basic character of the building will be altered substantially.

- The number, size and locations of existing window openings should be retained. Do not “block-in” windows to reduce the size of the window opening or to fit stock window sizes. New window openings should not be added on elevations that are subject to view from a public street.
- Retain and repair window frames, sash, decorative glass, panes, sills, heads, hoodmolds, moldings, and exterior shutters and blinds whenever possible. If replacement of any window part is necessary due to deterioration, the replacement should duplicate the material and design of the older window. Replacement sash of wooden windows, for example, should be made of wood. If duplication of the original window or window part is not technically or economically feasible, a simplified version of the original may be acceptable as long as it has the same size and proportion.
- Modern window types that are inappropriate include large picture windows, casements and bow windows, unless they are original to the building.
- Do not install shutters on windows that did not originally have shutters. Replacement shutters or blinds should be sized to cover the entire window when closed. In other words, the shutter should measure the full height of the window and half its width. Fasten shutters to the window frame and not to the siding.



- Inappropriate modern window features such as plastic and metal awnings or fake, non-operable, synthetic shutters and blinds distract from the historic appearance of a building and should not be used.
- Storm windows should have wooden frames, or if metal, should be anodized or painted to blend with the trim. Interior, rather than exterior, storm windows are recommended.
- Typical upper windows are vertically oriented and uniformly spaced across the building front. This rhythm of upper story windows is an important unifying feature of Downtown.
- Masonry infill, wood panels, or mismatched windows should be removed and replaced with appropriate materials.
- If the original window still exists, it should be restored to serviceable condition when possible.
- Replace only missing portions of original elements where feasible. Sometimes trim elements and other materials must be removed for repair. Always devise methods of replacing the disassembled materials in their original configuration.
- Installation of interior storm windows should be considered.
- If the existing window is beyond repair an appropriate replacement window of the same size and profile should be installed.
- If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the window. Any windows covered by masonry infill, wood panels, or mismatched windows should be removed.
- Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be considered.
- Window shades or curtains in colors that coordinate with accent trim should be encouraged.



Shutters are encouraged on upper level windows. They should be proportioned so that if closed, they would completely cover the window.
(Washington, Missouri)



Building which preserves significant window features.
(Hannibal, Missouri)

2.1.9 Architectural Details

- Replacement of missing cornices or architectural elements should be based on accurate duplications of original features. In some cases, an entire detail must be reconstructed. In the event that replacement is necessary, the new material should match the original in design, color, texture, and other visual qualities. Photographic evidence is a good source for research.
- If the cornice is missing, a similar cornice of like size and scale should be installed.
- If no evidence exists as to form and detail, the reconstructed cornice should be as simple and non-intrusive as possible.
- If the cornice is intact it should be repaired and maintained as required.
- Where architectural details have been removed, refer to historic photos for details to use as patterns for new designs.
- Where exact reconstruction of details is not feasible, consider developing a simplified interpretation of the original, in which its major forms and lines are retained.



Building rehabilitation that preserved the ornamental cornice at the top of the building. (Neosho, Missouri)

2.1.10 Entrances

- Recessed entries help invite customers into the store.
- Maintain recessed entries where they exist. These areas provide protection from the weather, and the repeated rhythm of these shaded areas along the street helps to identify business entrances.
- Avoid doors that are flush with the sidewalk.
- If the original recessed entry has been removed, consider establishing a new one. Use doors with large panes of glass where feasible, these will improve the visibility of the business to outside viewers.
- Consider using an accent color on the door.
- Center signs over door.



Recessed entrance with detailed bulkheads creates an inviting experience for the customer. (Washington, Missouri)

2.1.11 Awnings

Awnings used in the storefront zone provide shade for merchandise, shelter for pedestrians, and bring a colorful accent to the building front that can be changed frequently and without great expense. The following suggestions enhance appropriate use of awnings and improve Downtown aesthetics:

- Mount the top edge to align with the top of the transom, or if no transom is present align with the framing that separates the facade from the main display window. This will help strengthen the visual continuity of store fronts.
- Roll-up awnings were a common site on historic storefronts and can be used following a similar approach to the original application. If a roll-up awning is not operable, the awning should follow the shape of an operable awning.
- Like the storefront, awnings should be confined to the extent of the original storefront opening.
- Awning colors should coordinate with the color scheme for the entire building.
- Awning signage or lettering should not be allowed where another flush-faced sign exists.
- Awnings will wear and should be acknowledged as an operating cost of doing business which can be changed every few years for a fresh look.



Existing awnings in Downtown Strafford.

2.1.12 Storefronts

2.1.12.1 Entrance:

- The entrance door should be recessed to emphasize the entry, provide a bit of shelter and remove the open door from the path of pedestrians on the sidewalk. These areas also create a rhythm of shaded areas along the street to help to identify business entrances.
- If the original recessed entry has been removed, consider establishing a new one.
- The recessed entrance door should also be ADA compliant.
- The door should provide a view into the building as well as a sense of openness. Solid doors should be avoided.
- Consider using an accent color on the door.

2.1.12.2 Windows:

- Preserve any of the large panes of glass that make-up the original store front, if they still exist. These transparent surfaces allow pedestrians to see goods and activities inside.
- Any new or replacement storefront should be built of similar materials compatible with the original façade design and craftsmanship.
- Wood framing similar to the original is preferred, but metal framing with the appropriate historic profile is acceptable.
- Clear insulated glass with 'Low-E' coating is a good choice for replacement storefronts.
- Tinted or reflective glass and interior reflective films should not be used on the storefront.



Existing storefronts in Downtown Strafford.

2.1.12.3 Spandrel Panels:

- Maintaining the original spandrel panel, if it exists, is preferable, but if the panel is missing, reconstruction using old photographs as a guide is acceptable.
- Coordinate the color scheme of the spandrel panel with other façade elements.
- If original design information is not available, another option is to design a simplified panel using appropriate materials such as painted wood or metal.

2.1.12.4 Transoms:

- These bands of glass are often found on many storefronts and often align at the same height in a block. Maintaining this line will help to reinforce a sense of visual continuity for the street.
- When transoms are covered and original moldings and window frame proportions are concealed, the impact of the store front is weakened. If the interior ceiling is now lower than this glass line, move the dropped ceiling back from the window to maintain its historical dimensions.
- Some transoms have hinged panels to allow natural ventilation. Restore these to working order where feasible. Used in combination with ceiling fans these operable transoms can be very effective in improving comfort levels when full air-conditioning is not as necessary.
- In Downtown Strafford, many of the buildings either have no transoms or had them completely removed during past alterations. Although most of the building stock in Downtown is single-story, transoms can still be an important feature to add or bring back if desired to improve the architectural design of the district.



Single-story buildings which preserve the transom.
(St. Charles, Missouri)

2.1.13 Signage

For a successful business environment each shop must have its own identity while also maintaining the continuity of the district. Appropriate signage identifies the business without detracting from the architecture of the building and the fabric of Downtown. In most cases, sign types and their locations should be kept simple and consistent for ease of public awareness. Signage should also be restricted to the storefront or rear entrances of a building. Overall, the following guidelines will help enhance these aspects. In the case of Downtown Strafford there may be exceptions to these standards which are illustrated in Section 6 of this report.

2.1.13.1 General Design Issues

- The sign should be a part of the building design. Do not hide building features. Find an element or space that will naturally accommodate the sign.
- In most cases, the size of the sign should be of an appropriate scale for the building and street. Large signs may not be as necessary since the signage in a downtown area is more oriented to the pedestrian than the motorist.
- Flush-mounted signs positioned to fit within architectural features is preferred. This type of signage will help reinforce horizontal lines along the street.
- Locate flush signs so they do not extend beyond the outer edges of the building front.
- The material and color of the sign should complement the building materials and color scheme.
- The message of the sign should be simple and easy to understand. The name of the business and type of business should be sufficient. A logo or symbol of the type of business could substitute for a “type of business” message.
- Typically, rooftop, blade, pole, abandoned, electronic message boards, and billboard signage should not be allowed or severely restricted. The use of neon to tie into the vintage Route 66 theme should be allowed with clear guidelines in place.
- Place signs near the business entrance, to guide a customer’s eyes to the door.
- Where several businesses share a building, coordinate the signs by aligning several smaller signs or grouping them onto a single panel as a directory to make them easier to locate. Use similar forms or backgrounds for the signs to tie them together visually and make them easier to read.



Current building signage in Downtown Strafford.

- Mount signs so they will not obscure any architectural details.
- Sign materials should be compatible with the façade materials.
- Good craftsmanship will pay off in longer service for a sign and will convey a stronger image to the public. Select high quality materials. Signs are exposed to extreme weather conditions, and a deteriorating sign presents a poor image to customers.
- Encourage the use of “custom” designs that portray a business as being unique. Mass-produced signs, especially rectangular plastic panels with internal lighting, fail to make a lasting impression and also may not tie into a Route 66 theme.
- Illuminate signs in such a way as to enhance the overall composition of the façade.
- External lighting cast from period style, non-intrusive fixtures is preferable to internal sign lighting.

2.1.13.2 Style and Location of Signs

Projecting Signs: Projecting wall signs that give the name or the logo of the business or product sold, such as a watch for a jeweler or a drug company logo. These signs should have the following characteristics:

- Material: Unframed painted wood or metal panels hung from painted wall brackets. Wood signs with carved or sandblasted designs that are painted are also appropriate.
- Color: Sign colors should complement the paint scheme and masonry color of the building.
- Lighting: Non-illuminated or externally illuminated with spotlights. Some signs may also have the letters outlined in neon.
- Location: Bottom of sign should be 8'-0" above the sidewalk and below the building parapet or the second floor windows.
- Locate projecting signs along the first floor level of the façade. If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the window.
- Use symbols in projecting signs; these are more easily identified and remembered and will add interest to the building.



Examples of projecting signs.
(Mexico, Missouri)



Flush mounted signs over store entrances in
Downtown Strafford.

Wall Signs: Painted signs on the brick wall above the windows or on the side of the building. The old faded signs on the sides of the buildings are commonly called “ghost signs” and should be preserved wherever possible. Wall signs should have the following characteristics:

- Material: Painted on brick wall or on wood or metal panels. The signs painted on brick were usually white lettering on black backgrounds unless they advertised a product, such as Coca-Cola or Wrigley’s, which were multi-colored.
- Lighting: Natural light or externally illuminated with spotlights.
- Location: Many of these signs were in recessed brick panels above the storefront windows. Oftentimes examples of these can be found in historic photos and they may still exist beneath paint and metal or wood panels. Wall signs typically should not be located above the building parapet.

Window Signs: Painted or foiled lettering on the display window glass. These often advertised a doctor, dentist or attorney. Window signs should have the following characteristics:

- Material: Painted lettering, or gold or silver foil lettering. Lettering colors should complement the paint scheme of the building.
- Lighting: Natural lighting or the inside lights of the building. However the use of neon lighting may be considered as long as it is limited in size and brightness.
- Location: On the glass of the entry door or the display window at eye level. These signs were fairly simple and did not attempt to dominate the window. The merchandise inside is what you are trying to sell. Window signs are also appropriate in second floor windows to identify second floor businesses.

Awning and Canopy Signs: Awning or canopy signs should have the following characteristics:

- Material: Lettering silk-screened on awning fabric or painted on wood or metal sign panels.



Example of existing wall sign
(Pontiac, Illinois)



Example of a window sign in
Downtown Strafford.



Example of existing awning sign.
(Mexico, Missouri)

- Location: Six to eight inch high lettering on the front valence of a fabric awning or a hung sign panel. These panels should be a maximum of twelve inches high.
- Mount the top edge to align with the top of the transom, or to align with the framing that separates the transom from the main display window. This will help strengthen the visual continuity of store fronts.
- Roll-up awnings were a common sight on historic storefronts and can be used following a similar approach to the original application. If a roll-up awning is not operable, the awning should at least follow the shape of an operable awning.
- As with the storefront, awnings should be confined to the extent of the original storefront opening.
- Awnings should be trapezoidal in profile with closed ends, not rounded or curved, and a consistent color.
- Awning colors should coordinate with the color scheme for the entire building.
- Awning signage or lettering should be limited to the hanging vertical flap of the awning and be complementary in color to the building.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Awnings will wear and should be acknowledged as an operating cost of doing business. They can be changed every few years for a fresh look.
- Aluminum and/or steel awnings and structures are not original building elements and typically detract from the overall appeal of the Downtown façade. These awnings should be removed and points of attachment repaired on the building façade.

Sidewalk signage: Symbolic signage, such as barber poles, were often set on the sidewalk. Sidewalk placards were also used to advertise merchandise. Sidewalk signage should have the following characteristics:

- Material: Painted wood or metal.
- Lighting: Natural illumination. Do not internally illuminate.



Free-standing entrance sign.
(St. Charles, Missouri)



Signboards on sidewalk.
(Hannibal, Missouri)

- Location: At the edge of the sidewalk or at the building face. Most signs of this type should be portable so that they can be taken inside at night or during special activities such as parades.
- Signboards under the awning intended to assist pedestrians should be a limited, uniform size and complement the awning and building.

Signs to Avoid:

- Flashing or animated signs, or signs with moving parts or the effect of movement
- Internally illuminated signs or awnings, with the exception of neon as previously discussed.
- Signs that make sounds or music
- Temporary signage used as permanent signs (monument, banner, etc.)

2.1.13.3 Number and Area of Signs

Principal Business Signs: Signs that identify the name and nature of the principal business should be limited to two per building storefront. These signs could be any combination of the sign types discussed above.

Auxiliary Signs: In addition, each business could have a sign stating hours of business and an “open” sign. These should be limited to two square feet each.

Side Street Directories: Side walls of corner buildings could be used for directions to side street locations such as parking, churches and businesses. These signs should be of uniform size and design, and be mounted below a directional arrow. A suggested sign panel size would be 12 inches high by 48 inches long with 6 inch high lettering.

Sign Area: The aggregate area of all principal signs should not exceed 100 square feet, except buildings with front wall area of 1000 square feet or less, where the aggregate sign area should not exceed approximately 10% of the front wall area.

Lettering Size: The size of lettering or any sign type should not exceed 12 inches high, except for the first letter of each word, which should not exceed 18 inches high.

Lettering Style: Because the historic signs spanned a long time period, a variety of lettering styles existed together. Lettering style for new signs could be either simple block letters or more elaborate lettering styles. Each business should express their individuality in their sign design.



Example of signage to avoid in Downtown Strafford.

2.1.14 Lighting

Buildings should be interesting to pedestrians and motorists at night, as well as by day. A well-lit storefront or rear façade creates a positive impression about Downtown. The following lighting conditions can be implemented to enhance the attractiveness and safety of the Downtown:

- Use lighting as a design element to draw attention to the entire building, not just the sign.
- Any lighting at the storefront should be used to accent the entrance, signage, or architectural elements as well as provide light for safety and security.
- Light fixtures should be the lowest wattage possible and of a concealed, simple, and non-intrusive design or a style that is appropriate to the period of the building.
- Sign lighting should be balanced in color and intensity with light in display windows.
- Warm-colored light is preferred for all exterior lighting, since this is more pleasing to the eye, and will more easily draw attention to window displays.
- Lighting on rear façades should provide illumination at the entry door as well as along the pedestrian path from the parking area. This lighting should be similar to the lighting in the front of the building.



Inappropriate style of lighting.



Good examples of lighting to highlight building, signage & entrances.

2.1.15 Rear Elevation

The typical rear elevation often faces an alley or parking lot and provides access for deliveries and maintenance. In some cases customer parking is provided behind a building and entry to the business through the rear is desirable. Downtown Strafford faces a unique situation in that buildings along the south side of Pine Street also face the north side of Old Route 66. In either case, attention to the appearance of all elevations can be extremely important to the quality of the customers' shopping experience. Consider how image can be improved here, while accommodating service functions.

2.1.15.1 Entry Door

- The rear door will no longer be just for service but should project a sense of openness and welcome.
- Customers might also feel a loyalty or sense of 'special access' by using this door and the business can build on this loyalty by catering to that customer and improving that experience.
- A new door and hardware with a large area of glass may be considered.
- A small canopy or awning can provide some form of shelter.

2.1.15.2 Upper Rear Elevation:

- The upper rear elevation elements should be treated similar to the front. Too often this is the elevation that is neglected and allowed to deteriorate.
- Windows should be restored or replaced.
- Gutters and downspouts should be in good repair and painted.
- Use materials and colors that coordinate with the main façade so customers will learn to recognize both entrances are related to the same business.
- Use a smaller version of the front sign to identify the rear entrance.
- New exit stairs and balconies can enhance the marketability of second story space, especially when these lead out onto parking lots located on the back side of the building. Encourage installing new stairs that comply with current building codes.



Well maintained rear entrance & upper rear façade. (St. Charles, Missouri)



Rear Entrance with good variety of landscape. (St. Charles, Missouri)

2.1.15.3 Fences:

- Fences should be designed to harmonize with the surrounding structures in both scale and color.
- Some materials which may be appropriate include masonry, wood, ornamental metal and wrought-iron.

2.1.15.4 Trash Dumpsters, Ancillary Structures, and Utilities:

Sensible, yet firm enforcement of the city's building and nuisance codes will be required and should be a priority throughout the Downtown.

- Waste receptacles should be placed in an enclosure or behind a screen.
- Use landscaping to help screen waste receptacles and dumpsters.
- Enclosures and screens should harmonize with the surrounding buildings in scale and color.
- Landscaping can also be used to screen air-conditioning condensers and utility transformers.
- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Any ancillary structures should match the surrounding buildings style and scale. These structures must be maintained well.
- Keep electrical service boxes and conduits in good repair and painted.
- Encourage using a color scheme on these screens that matches that of the rest of the building.

With the unique situation of double frontage between Pine Street and Old Route 66, the location of electric meters, trash enclosures, and other utilitarian features should be grouped together in communal sites, where possible, with screening to minimize any negative impacts on customers, visitors, and the overall aesthetics of Downtown.



Screened waste receptacles and dumpsters.



Regulations for property owners and refuse haulers should be enforced.

2.1.16 Building Color Guidelines

Color can enhance the details and patterns of façades. The most effective and economical schemes often start with the natural colors of the building materials themselves as a base, such as the red of many brick buildings. The following techniques should be encouraged:

- Use only one base color for the majority of the background wall surface, but use a different color for accents. Do not paint a building entirely one color.
- Base colors should be muted earth tones or pastels.
- Look for “built-in” features of the façade that can be highlighted with an accent color.
- Window frames, sills, moldings, and cornices are potential elements to highlight with a contrasting color.
- Use bright colors only in small amounts. Place them at the first floor level to direct the customer’s eyes to the business.
- Consider accent colors for signs, awnings, and entrance doors.

The use of various color schemes for buildings should receive an approval process. Such steps could be developed through the existing building code and permitting process. Existing codes may need to be supplemented with design standards for Downtown Strafford.



Example of a good use of color which accents the building entrance. (Washington, Missouri)



Block that follows same basic design principles. (St. Charles, Missouri)

3.0 NEW CONSTRUCTION GUIDELINES

The design of any new structure within Downtown Strafford is of great importance because it offers the best chance to establish an architectural identity for the district. New buildings should be compatible with existing structures in most cases and harmonize the visual characteristics of the neighborhood.

The following guidelines for new construction cover additions to existing buildings as well as entirely new infill buildings. These guidelines are not intended to dictate particular architectural styles or features. They are intended to identify a range of design options that will encourage new development which is harmonious with the character of the district. The important elements to consider in new construction are scale, design quality, and relationship to neighboring buildings, rather than the degree to which new construction imitates an historic style or period. In fact, it is best to avoid recreating historical styles exactly or repeatedly in order to avoid a “theme park” type of atmosphere. While new buildings can be inspired by past design, creating a false past should be avoided.

- New construction should be of design considerate of traditional storefront elements described in these guidelines or on nearby buildings that contribute to the fabric of Downtown.
- Use a simple design, complementary to the Downtown, with three basic elements; a unified paint and color scheme, an awning, and signage. For instance, a Route 66 theme will include many similar aspects to be considered.
- Emphasize horizontal features that can align with other buildings.

New construction should be compatible with the rhythm of neighboring buildings along the street. Rhythm is defined by the relationship of buildings to open space along the street, the relationship of solids to voids on building façade, and the relationship of entrance and porch projections to the street. The directional emphasis- whether vertical or horizontal in character- of new construction should relate to that of neighboring buildings.



The defined rhythm of Downtown Strafford should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing.

- New infill buildings and structures should maintain the rhythm through proper repetition of details and orientation to the street.
- Vertical elements, entrances, lighting and other street furnishings can also develop the rhythm of a specific block.

New construction should be evaluated in terms of the following: siting, massing, rhythm and directional emphasis, materials, and building elements.

3.1 Design Guidelines for Residential Buildings

3.1.1 Building Orientation

The setback and orientation of new buildings in Downtown Strafford should align with neighboring buildings in most cases. Within the service area, principal elevations of buildings characteristically face the street with a strong sense of entry. New buildings with main façades and entrances oriented to the side yard, or new buildings having a courtyard arrangement may not be appropriate.

3.1.2 Building Materials

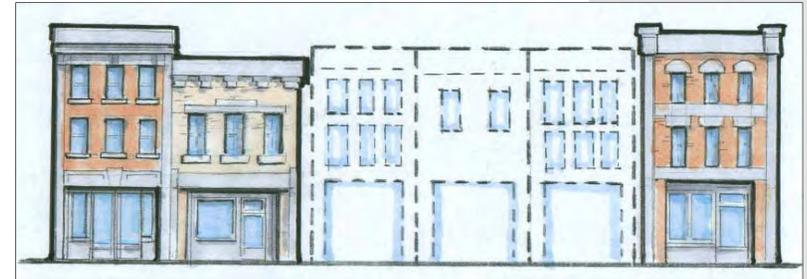
The exterior materials used in new construction should be compatible with appropriate materials of neighboring buildings or the district as a whole.

3.1.3 Building Design Elements

The various individual elements of a building—the roof, windows, doors, porches and trim—should be carefully integrated into the overall design of new construction. These elements also should complement those on neighboring buildings.



New buildings are same in scale and are aligned with sidewalks.



New buildings follow the rhythm of existing buildings.

The shape and pitch of the roof should be considered. Window and door proportion, size, design, and pattern of spacing between the openings should be compatible with historic treatments of windows and doors in the district. Although the front porch is uncommon in modern construction, the inclusion of porches may be important in new construction within Downtown Strafford.

3.2 Design Guidelines for Commercial Buildings

3.2.1 Building Orientation

The setback and orientation of new buildings in Downtown Strafford should align with neighboring buildings whenever possible. Within the service area, principal elevations of buildings characteristically face the street with a strong sense of entry. New buildings with main façades and entrances oriented to the side yard, or new buildings having a courtyard arrangement are not appropriate.

3.2.2 Building Form and Scale

New construction should conform to the massing proportion, volume, scale, and height of neighboring buildings. The bulk and area requirements in the zoning ordinance regulate the specific height and area coverage of buildings allowed in the Downtown district.

3.2.3 Building Materials

The exterior materials used in new construction should be compatible with historically appropriate materials of neighboring buildings or the district as a whole.

3.2.4 Franchise Architecture

Large franchises and national chains typically have a 'downtown style' in addition to their trademarked brand. The City of Strafford should insist that companies use a downtown design standard and adapt their brand to create a building front which compliments Downtown.



A franchise façade that enhances the surrounding street fabric.

3.3 Sustainable Design

3.3.1 Introduction

The construction of sites and buildings have a significant impact on the natural environment. The operations of a site and a building, can also affect the air, land and soil of the downtown. Sustainable Design measures seek to lessen the impact on the natural and built environment. Such design efforts also aim to increase the efficiency at which buildings operate, in regard to energy use and operating costs. The design process is comprehensive, beginning with site selection and orientation; through specification of sustainable materials to energy efficient operating systems.

Downtown Strafford is a built environment of buildings, public streets, parking lots, a few vacant lots and open space. Sustainable Design measures can be applied to both existing buildings and new buildings. The U. S. Green Building Council (USGBC) has become the leading organization in developing standards for sustainable design and operations of buildings. The U. S. Green Building Council's certification system is known as Leadership in Energy and Environmental Design (LEED). The majority of LEED designated buildings are new construction projects, however the USGBC has also developed standards for the upgrade of existing buildings.

Sustainable design is a broad and encompassing initiative which strives to create a built environment which is good for both man and nature. The following recommendations only introduce the basic fundamentals of sustainable design regarding downtown buildings and environments. For additional information beyond these guidelines, numerous resources exists, such as the following:

- U. S. Green Building Council (USGBC) www.usgbc.org
- Whole Building Design Guide www.wbdg.org
- American Society for Testing and Materials International (ASTM)
ASTM E2432— Standard Guide for General Principles of Sustainability
Relative to Buildings www.astm.Standards.e2432.htm



Permeable pavers for parking area allow stormwater to percolate back into the soil and groundwater.



Interior flooring fabricated from bamboo, a rapidly renewable resource.

3.3.2 Fundamentals

Sustainable design measures are constantly changing, however there are six fundamental principles which constitute sustainability:

- 1) **Optimal Site Potential:** Consider site selection, building orientation and existing natural features of a site including topography, drainage, landscape and natural habitats. The rehabilitation and reuse of existing buildings should always be evaluated as an alternative to new building construction.
- 2) **Efficient Use of Water:** The design and use of water systems in a building maximize efficiency and recycle water for on-site use when feasible. Site design should seek to reduce stormwater run off from the site. Use best management practices (BMP) to limit stormwater run off, clean storm water and trap pollutants in the water before discharging into the sewer system.
- 3) **Environmental Materials and Resources:** Utilize building materials with a high percentage of recycled content or contain rapidly renewable materials such as cork flooring, bamboo cabinetry, wool carpeting, etc. Specify or use materials or items which are manufactured within proximity to the project site. Ideally, this proximity is no more than 500 miles.
- 4) **Optimal Energy Use:** The operation of a site and building identify methods for increased energy efficiency or use renewable resources such as solar or geo-thermal energy.
- 5) **Interior Environmental Quality:** Identify methods for creating a healthy environment, and increasing the comfort of building users. Proper ventilation, use of natural light, and moisture control are a few methods to ensure a quality interior space.
- 6) **Optimal Operations and Maintenance Methods:** Utilize building systems, furnishings and finishes which will have minimal operations and maintenance needs. Such systems will require less energy, less water and can be maintained with natural cleaners which are not toxic to the environment or occupants.



"Green Roofs" reduce stormwater runoff, reduce heat gain and provide aesthetics for building users.



Solar panels provide an additional energy source for building power needs.

3.3.3 Elements

Sustainable design elements are extensive. The following list seeks to introduce only a few recommendations which may be applicable to Downtown Strafford:

- 1) **Parking and Service Areas:** Minimize stormwater runoff by using pervious pavement materials such as pervious paver systems or pervious concrete. Such systems will allow stormwater to percolate into the soil and not into the public stormwater sewer system.
- 2) **Building Materials:** Utilize materials which are composed of recycled materials or manufactured from rapidly renewable materials, which are made from plants that are typically harvested within a 10 year cycle. Examples include: bamboo flooring, linoleum flooring (made of wheat flour and linseed oil) cotton batt insulation and wheatboard cabinetry. Recycled bricks from demolished buildings should also be used for new building construction or restoration projects.
- 3) **Alternative Transportation:** Promote by providing secure bicycle storage and changing/shower facilities for employees.
- 4) **Solar Energy Alternatives:** Install solar panels to supplement the power system for commercial and residential buildings. Utilize prefabricated solar water heaters to provide the majority of the hot water needs for buildings.
- 5) **Stewardship:** New wood products, including construction lumber, should be certified by the Forest Stewardship Council, which promotes responsible forest management.
- 6) **Lighting:** Develop a lighting plan for public spaces which minimizes excessive lighting, which affects night sky viewing and the migratory patterns of birds. Flags which require lighting should be lit from the top shining down on the flags instead of being lit from the ground, projecting light into the sky.
- 7) **Operations:** Use timers on public fountains and lights in non-essential areas to shut off lights after 1:00 a.m., in order to reduce energy consumption.
- 8) **Landscaping:** Plant native landscape materials which can survive on natural rainfall once established.
- 9) **Street Furnishings:** Specify site furnishings such as benches, waste receptacles, bollards, and planters which are made from recycled plastic materials.
- 10) **Water Conservation:** Capture rain water runoff from roofs in rain barrels for irrigation use or direct to rain gardens on site. Inside buildings, consider waterless urinals or low flow water closets to limit potable water use.



Permeable pavement system installation.



Rain garden with native landscape plants.



Solar water heater.

4.0 STREETScape DESIGN GUIDELINES

4.1 Design Guidelines for Public Streets

The term Streetscape typically refers to exterior public spaces located between the building façades on one side of the street and the building façades on the other side of the street. An organized streetscape combines many design elements to create an inviting and attractive public street.

4.1.1 Design Coordination

A comprehensive design approach to the Downtown will result in a more successful project. Downtown Strafford can display a sense of order and rhythm through the repetition of design elements of buildings and street furnishings. A sense of arrival should exist upon entering the Downtown.

- The Downtown should have well maintained streets and walks.
- Particular care should be devoted to the main entries and corridors into Downtown, as these will be the visitors' first impressions of the area.
- Good, clear signage provides an invitation to enter, navigate Downtown and visit attractions.
- Establishing uniformity in streetscape furnishings helps to give a visual cohesiveness to the Downtown district.

4.1.2 Infrastructure

- Poorly working storm drains can create an undesirable situation at street intersections when runoff water collects in large pools. This condition makes pedestrian access virtually impossible and must be corrected. Curb and gutter are vital to Downtown Strafford to improve the drainage and markedly enhance the aesthetics of Downtown.
- New or replacement curbs should be vertical curbs.
- Curbs should be in good repair and a consistent material along the street. There should be no gaps or areas of uneven elevation along the curb line.
- Sidewalks at all street intersections should have accessible curb ramps.



View along Hwy 125 upon exiting I-44 and entering the Downtown area.



Lack of consistent sidewalks, curbing, and proper drainage makes Downtown Strafford uninviting.

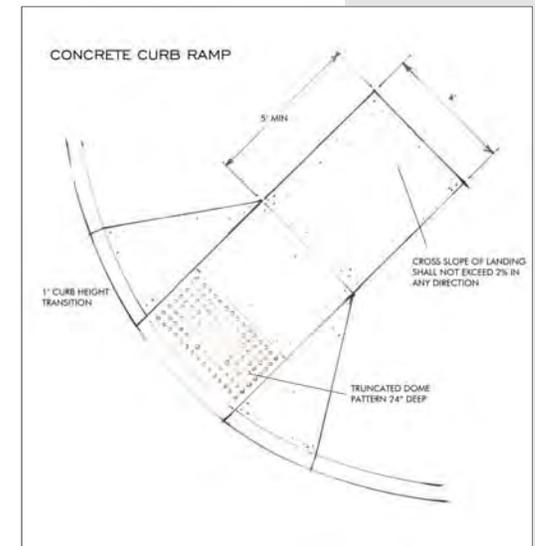
- Overhead utilities may pose a visual distraction from the overall unity of Downtown. Although costly, relocation of overhead utilities should be considered, especially with each new development/redevelopment project.

4.1.3 ADA Accessibility

Accessibility on public sidewalks is required by law, per the Americans with Disabilities Act (ADA). Without the required curb ramps, sidewalk travel in urban areas is dangerous, difficult, and in some cases impossible for people who use wheelchairs, scooters, and other mobility aids. Curb ramps allow people with mobility impairments to have access to the sidewalks and buildings.

4.1.4 Pedestrian Access and Sidewalks

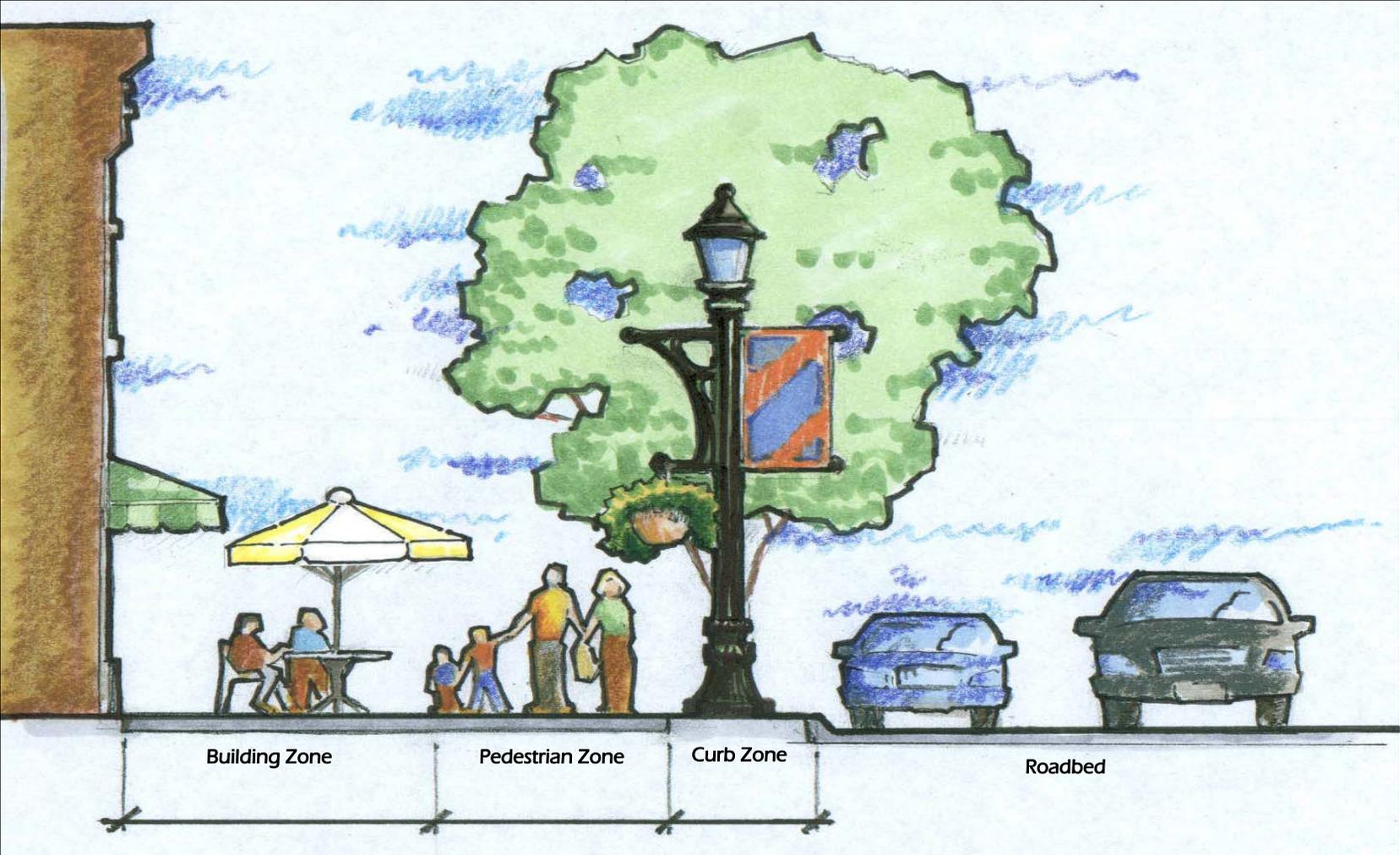
- A clean, clear and well-lit pathway for pedestrians should be provided from any remote parking area to the Downtown. This pathway will also need to comply with Federal ADA Accessibility Guidelines.
- Sidewalks should run continuously through an entire block to create a clearly defined pedestrian pathway and minimizing conflicts between people and vehicles.
- All roadway crosswalks should be clearly marked with signage and striping.



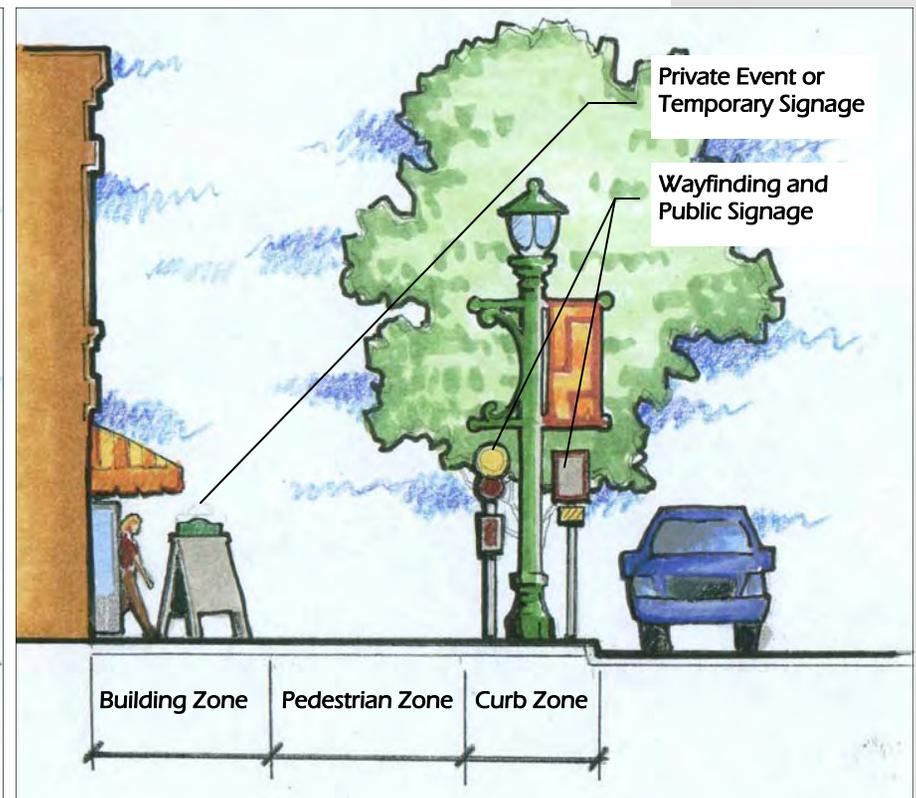
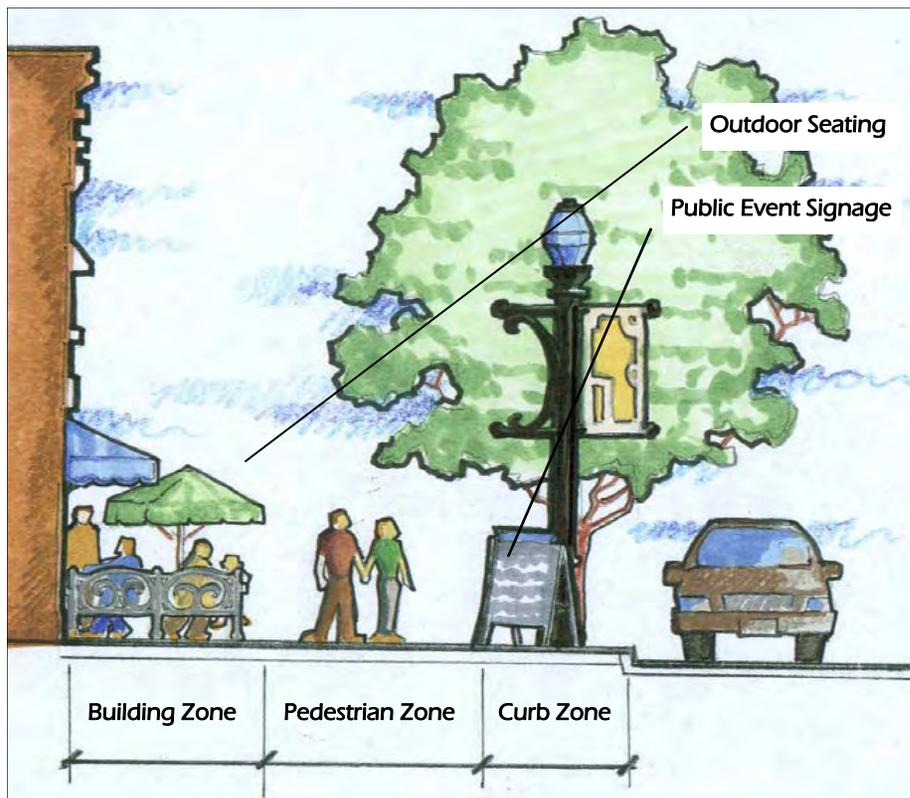
Detail design drawing of an accessible ramp.

4.1.5 Sidewalk Zones

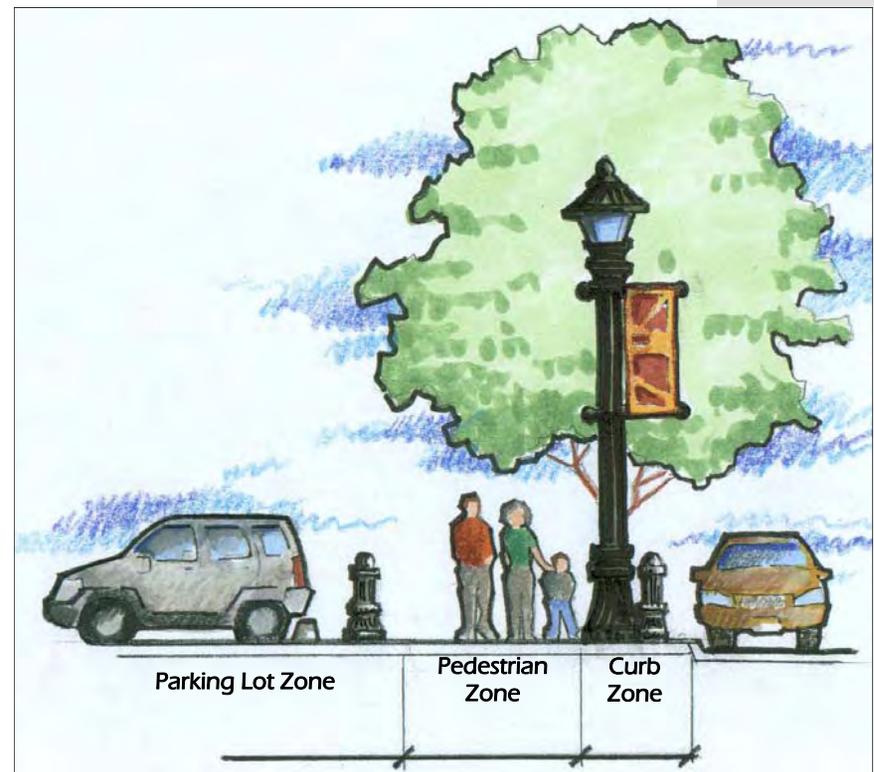
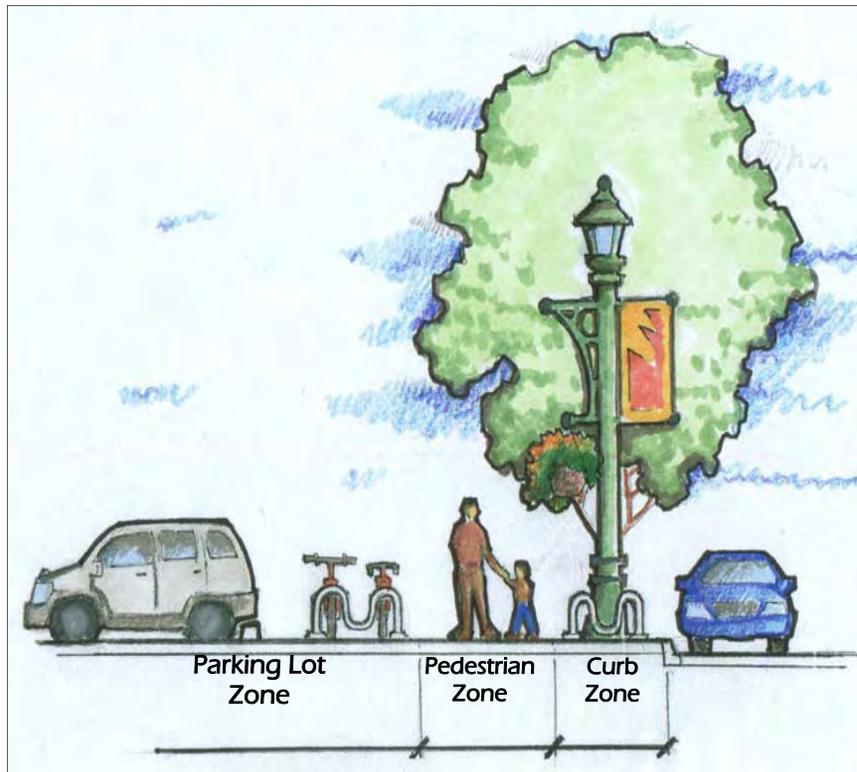
Appropriate zones in front of a building shall be maintained. The Building Zone, Pedestrian Zone, and Curb Zone all have unique characteristics that should be regulated to ensure that private elements do not adversely impact public improvements.



- Aside from ADA accessible pavement improvements, Streetscape Amenities should remain clear of the Pedestrian Zone and allow for free movement of pedestrians. These elements will enhance the pedestrian experience, but must not obstruct them.
- Businesses should be informed on the importance of maintaining Sidewalk Zones. Each business should care for the zones within their building's street frontage.



- Items such as bicycle racks, bollards, and benches can add to the streetscape.
- Businesses should consider providing and maintaining such elements within their Building or Parking Lot Zones to enhance service to their patrons.



4.1.6 Fountains and Public Art

The introduction of fountains and public art can enhance Downtown Strafford and the pedestrian experience. These features will be most effective as simple interactive elements which Downtown visitors can enjoy.

- Fountain water should be left in its natural state (no coloring).
- Pedestrians should be allowed to touch and splash in the water of a fountain, where codes allow.
- Fountains can be combined with signage and landscaping to create a gateway for Downtown.
- Fountains should receive regular maintenance by properly trained staff.
- Repairs to fountains should be performed by a qualified professional.
- Public art and sculpture should be of high quality, produced by a trained professional.
- Graffiti and vandalism of public art should be repaired as soon as possible.
- Temporary and seasonal art exhibits should be encouraged to showcase Downtown Strafford.
- Public art should, if possible, reflect local stories or history.
- Public art can also serve as memorial to local persons or to commemorate historical events.



Examples of public fountains.



Examples of public art.

4.1.7 Signs and Banners

Signage should be used to identify, define and promote Downtown and its activities. Individual building and business signage is discussed in Section 2.1.13. Wayfinding techniques and components to assist in navigation through the area will be addressed in Section 4.2. Some basic guidelines for effective usage of signage are as follows:

- Street name signs should be chosen and installed that are distinctively different from the street name signs located in the rest of the community. This will reinforce a feeling of 'place' in Downtown Strafford.
- The style of the street name and street address signs should complement if not match completely.
- In an effort to reduce visual clutter, regulation and direction signage should be combined where possible.
- Banners and other temporary signs should be allowed, but restricted as to size, prevalence, and length of display.
- Hung from the light poles, seasonal banners or decorations approved by the City can create seasonally festive streets.
- Banners can add a sense of civic identity while providing information about upcoming events or festivals.
- Banners should be well designed and are most effective with a simple design, repeated throughout Downtown, and with minimal lettering. Sponsor panels should only be allowed within a uniform design panel.
- The banner brackets used for these banners should be maintained by the City.
- Banners should be changed on a regular schedule and replaced as needed. Banners which have been faded or worn due to long term use, should be replaced.
- Balloons, pennants, and other distracting sign novelties should be strictly regulated in the Downtown area.
- Murals must have an artistic component and must meet approval by the City. Murals should be professionally installed. Any mural not approved should be considered in violation of the sign code.



Regulation signage combined on ornamental post.
(Washington, Missouri)



Example of banner signage.
(Mexico, Missouri)

4.1.8 Parking and Service Areas

- Adequate parking to support business and retail tenants must be provided. Street parking will accommodate some but not all of the required parking spaces.
- Parking lots on previously vacant property near Downtown is a good solution for additional parking spaces.
- Provide planting buffers at the edges of parking lots or use decorative paving to define the site border.
- Include landscape islands throughout the lot. This will improve the aesthetics and minimize the storm water run-off.
- Side or rear locations off the main street are preferred for parking lots.
- A clear and well lit pathway for pedestrians from any parking area should be provided.
- The street, alley and sidewalk pavement should be in good condition with no tripping hazards for pedestrians.
- Crosswalks should be clearly marked and free of obstacles to provide a clear view for traffic.
- Care must also be taken that lots are policed in the evening as they will tend to become a security concern for some patrons.



Buffer the edges of parking lots with landscaping.
(Washington, Missouri)

4.1.9 Lighting

Streetscape lighting should enhance the pedestrian and nighttime image of Downtown while also providing an attractive installation during the day. Lighting guidelines are listed as follows:

- The sidewalks should be provided with light to clearly define the walking surface.
- Storefront lighting can add to the pedestrian walkway illumination.
- Streetscape lights should be on 12'-14' height poles and project light down onto the sidewalk, not into second floor windows.
- Lighting should be uniform in style, type, height, and brightness throughout the area.
- The streetscape lights should be of a vintage and style reminiscent of Historic Route 66 and continued throughout Downtown.
- Light poles with brackets for banners and electrical outlets can effectively display temporary or seasonal decorations.
- An overall lighting design strategy should be developed to ensure appropriate lighting levels.
- The lighting plan should address parking areas, rear building entrances, and alleys.



Potential style of streetscape light post for Downtown Strafford.

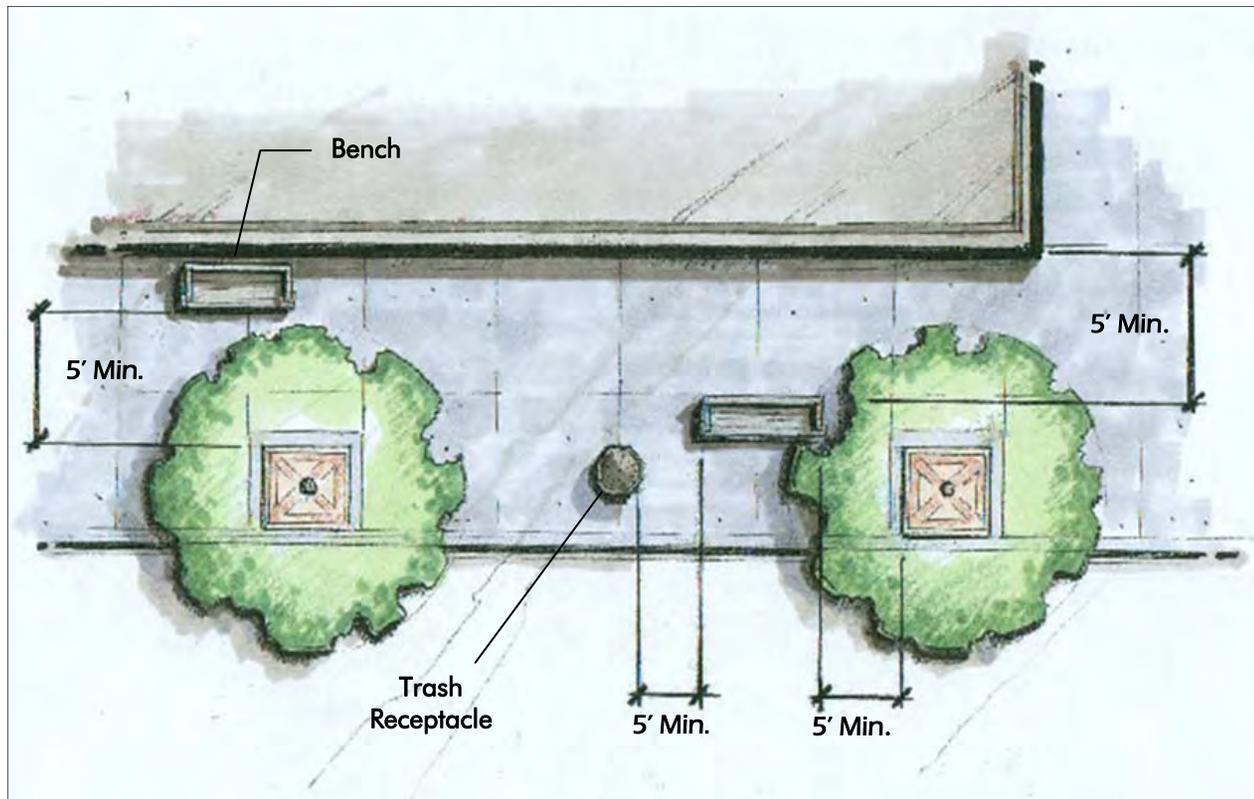
4.1.10 Site Furnishings

- Benches within the streetscape encourage social interaction which contributes to a successful Downtown. To help achieve this, street side benches in Downtown should face the street or public space rather than the building façade.
- Planters and window boxes provide seasonal color and can be a volunteer opportunity if maintained by a local club or organization.
- Public art and sculpture add to the identity of Downtown.
- Trash receptacles help to keep Downtown clean.
- Grouped together, such furnishings will enhance Downtown and provide a gathering place for pedestrians.
- Furnishings should be coordinated with light and sign poles to present a unified look to the streetscape.
- Street furnishing will invite people to walk around and linger in Downtown.



Proposed site furnishings for Downtown Strafford.

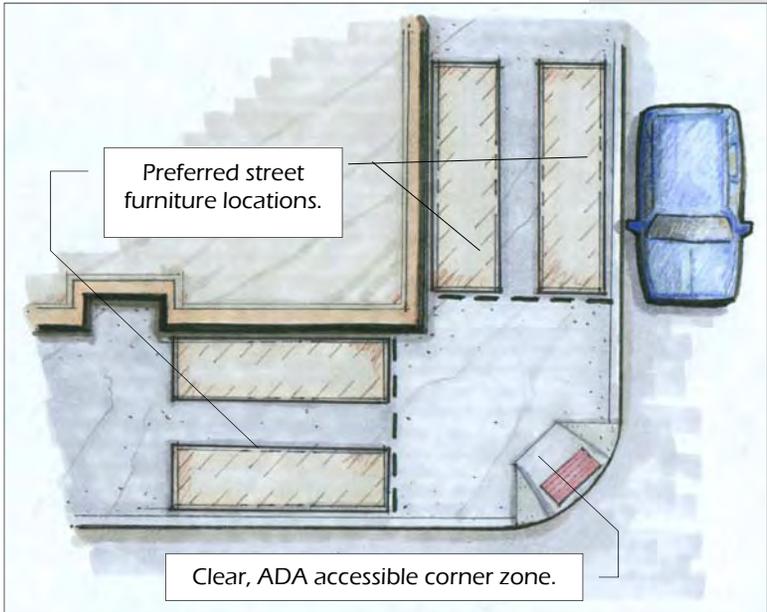
- The minimum distances shown represent suggestions for spacing of site furnishings. Actual distances may vary due to site conditions.



4.1.11 Outdoor Café Seating

Outdoor Café or sidewalk seating is a good option for restaurant/business patrons and can contribute to the life of a street. Seating areas for restaurants should be encouraged, but monitored by the City. The following are suggestions for proper arrangement:

- Areas should be located in the sidewalk area fronting the restaurant.
- Allow a clear and unencumbered path along the sidewalk for pedestrian traffic or be located close enough to the building. In either case the sidewalk must maintain ADA compliance. The restaurant owner is responsible for keeping the sidewalk and this pathway clear at all times.
- Areas adjacent to the building should not block entrances or exits to the building.
- Provide a clearly defined area connected with the restaurant.
- Utilize appropriate umbrellas or other patron covering in a uniform color, matching the building colors, and with only the restaurant name. Any other wording or message should not be allowed.
- Temporary outdoor seating material must be kept in top condition to provide an attractive image for the restaurant and all of downtown. Such furnishings should be durable, weatherproof, and sturdy enough to prevent movement by winds.
- Plastic furnishings should not be used.
- Furnishings should be stored in a secure location.
- At least one sturdy trash receptacle should be provided.



Proper location of cafés is important for safety, access, and service.



Outdoor café seating.
(St. Louis, Missouri)



Outdoor café seating.
(University City, Missouri)

4.1.12 Landscaping

- Landscaping zones can also be identified along side streets to complement, but not obstruct building façades.
- Landscaping in front of a business is recommended. Plants in movable containers should be considered where no available landscape planting area is available. Containers should never be placed in the pedestrian pathway, but rather, immediately adjacent to buildings or curbs.
- Trees work best when planted in groups or islands where they can thrive in larger volumes of soil.
- Trees and shrubs should be of a hardy variety, common to the region but specified at a size which will allow a minimum of seven feet of vertical clearance before any lateral branching begins.
- Trees should also be chosen for their root growth structure. Specify varieties with downward growing taproots, not lateral growth that will damage surrounding pavement.
- Shrubs should be massed in groupings of five to seven plants with no more than two different species within a planting bed.
- Locate plantings in traditional areas of the site. Plantings along fences, walks, foundations, and at porch edges are good locations.
- Landscaping should be installed adjacent to alleys, driveways and other areas in between buildings.
- Garden clubs or volunteers should be encouraged to help maintain landscaping of perennials and annuals on public property.
- Street Trees should be located based on existing conditions to minimize blocking the view of store windows, door ways and signs.



Good example of landscaping along an alley.
(Sikeston, Missouri)



Containers with seasonal landscaping at a store
entrance. (Washington, Missouri)

4.1.13 Bicycles

A downtown area should not only be pedestrian friendly, but bicycle friendly as well. The scale of Downtown and its location on Route 66 makes bicycle travel an ideal means of transportation in Strafford. Downtown plans should implement bicycle facilities which can be used by local citizens and tourists. The City should identify opportunities for future bicycle facilities in Downtown and throughout the community. Such facilities may include the following:

- Bicycle racks with the uniform design of materials, color and style as other site furnishings.
- Directional and regulatory street signage which identifies local streets as bike routes and share the road routes.
- Wayfinding signage to direct cyclists to various destinations within Downtown.
- Public restrooms and drinking fountains available for cyclists.
- Dedicated bicycle lanes on streets, where feasible and possible.

The City of Strafford should capitalize on Route 66 travelers to promote the use of cycling to, and through, the town. Developing a bicycle network program to identify local streets as the defined bike routes through town would provide a safe and well organized plan for cyclists and automobile drivers to understand. Implementing bicycle facilities will provide an alternative means of transportation and another recreational experience for visitors and locals.



Bicycle rack.
(Webster Groves, Missouri)



Example of bicycle signage.
(St. Louis, Missouri)

4.2 WAYFINDING

4.2.1 Wayfinding Principles

Wayfinding is an indispensable tool for directing travelers to destinations while at the same time creating a positive first impression. The term wayfinding was originally coined by Kevin Lynch in his seminal 1960 book *The Image of the City*. Lynch presented the concept that people use a cognitive map to move through their environment to their destination. Wayfinding develops a system to assist travelers in interpreting the map.

Wayfinding systems have been used for many years in institutions such as colleges and corporate headquarters. As it is commonly recognized today wayfinding helps travelers *find* their way to the destination, in this case Downtown.

In relation to community planning and specifically to Downtown Strafford, the goal of the wayfinding system is to make the journey to Downtown as transparent and seamless as possible. Furthermore, by taking a comprehensive approach in developing the wayfinding system it can reinforce the community's unique identity and sense of place. This can be accomplished through four main aspects that can be controlled and enhanced through appropriate design and building codes:

Architecture:

- Visual clues of buildings and other features of a street aid people in knowing their location and the direction of their destination without the use of signage.
- Notable architecture serves as landmarks and orientation points. These points are often destinations as well as starting points and other wayfinding techniques should utilize this aspect. The absence of notable architectural landmarks in Downtown Strafford allows the opportunity for new projects and construction to fill these roles.
- Buildings have visual aids, such as pediments or lights, that draw our eyes to where we expect an entrance or a shop window to be located.



Wayfinding signage
(Webster Groves, Missouri)

Sight Lines:

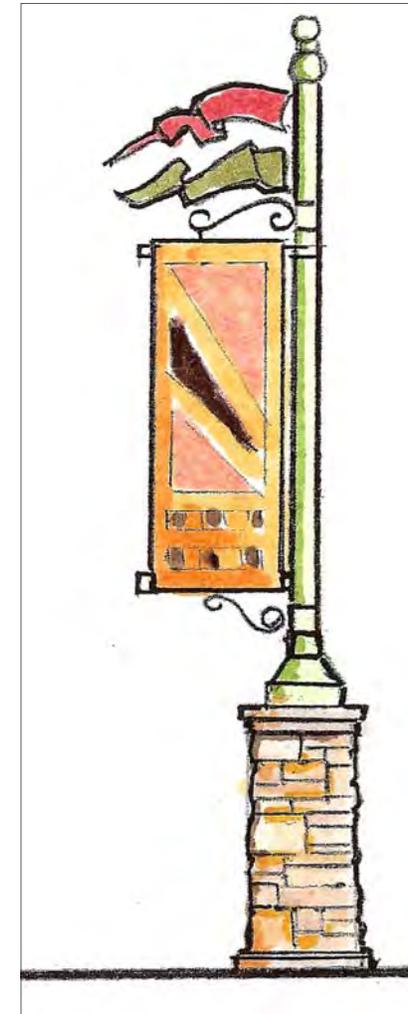
- The motorist will feel most comfortable in maintaining visual contact with his or her destination and will want to make as few direction changes as possible.
- Clean and clear lines of sight at key intersections should be maintained.
- Avoid allowing buildings to encroach or block these lines.
- Repetitive landscaping and site furnishings can enhance and draw the eye down these streets, but care must be taken that these items do not obstruct important navigational landmarks.

Lighting:

- Lighting can be used to define routes and pathways.
- Warmly lit storefronts and entrances draw the eye and provide the customer with the information needed to locate the business.
- A repetitive line of lighting can be a very effective navigation tool.
- Poor lighting causes missed information and leaves an unsafe impression.

Signage:

- Uniform signage at important decision points is a critical element of downtown wayfinding.
- Excessive signage will lessen the effectiveness of individual signs. Fewer, easy to read, appropriately placed signs are preferred.



Example of a historic district gateway sign.

4.2.2 Wayfinding Components

Wayfinding systems are made up of components that create an arrival sequence to the Downtown. The system consists of common themed signs, of various types that direct travelers to attractions.

A successful wayfinding system is made up of many components that can be utilized in a variety of configurations. Some communities may require only a few of the components or can gradually add components into a complete system. Determining how many of the components a wayfinding system needs, depends upon the size, number and type of street.

Arterial and local collector streets are typically the location of gateway, historic, historic district, and trailblazing signs. These streets comprise the transportation corridors which focus the traveler into the downtown. They are discussed in detail as follows:

- Historic District Corridor— The central component and primary focus of the streetscape improvements and wayfinding system. This corridor includes the final approach, as well as the main arteries of the historic district. Signage types for this corridor typically include Historical District Gateway and Proximity signs.
- Residential/Commercial Entry Corridor— The areas immediately surrounding Downtown, a secondary focus for streetscape improvements and primary focus for residential improvement programs. Usually contain a small to moderate amount of commercial businesses, converted residential, and residential. Signage types for this corridor typically include Trailblazing and Proximity signs.
- Commercial Entry Corridor— Typically the beginning of the wayfinding system for travelers. The corridor consists of commercial and institutional uses and should focus on simple design treatments that reflect the scale and traffic. Signage types for this corridor typically include Primary Gateway and Trailblazing signs.



Example of a wayfinding system
(Clinton, Missouri)

As stated earlier, a successful wayfinding system is made up of many components, including but not limited to these categories of signage:

- Primary Gateway Sign— Serves as the “Welcome” to a visitor, creating the first impression of the community. The sign should be significant, serving as a landmark.
- Trailblazer Sign— Utilitarian purpose combined with unique branding and design elements. Attractions to consider as destinations on Trailblazer Signs include; Downtown, Historic Districts, museums, event areas, government offices, parking, colleges/universities, and visitor centers. Signs should be located at or near a key transportation node.
- Historic District Gateway Sign— Creates a boundary for the Historic District, should be used within the Historic District Corridor. These signs should reflect the size, scale and character of the architecture within the district.
- Proximity Signs— In close proximity of the attractions these signs direct visitors to the destination.

Trailblazer and Proximity signs are likely the most appropriate for Downtown Stafford. All signs and banners included in the wayfinding system should have a simple and uniform design. Above all, the signs need to be brief and easy to read with large type face, appropriate coloring, and adequate character spacing. The wayfinding system signs should be unique and stand out in their surroundings. Sign guidelines should be developed using the Manual of Uniform Traffic Control Devices, published by the Federal Highway Administration (FHWA).



Potential trailblazer sign for Downtown Stafford

5.0 DOWNTOWN STRAFFORD STREETScape PLANS & SIGNATURE PROJECTS

5.1 Typical Streetscape Plan

An effective streetscape plan is one which establishes an identity through consistent use of the visual and functional elements. Portions of the Downtown area have the basics of an established rhythm, however this rhythm is not consistent. Therefore some basic concepts can be used as a starting point to guide the effort of creating a complete streetscape which is both functional and aesthetically pleasing.

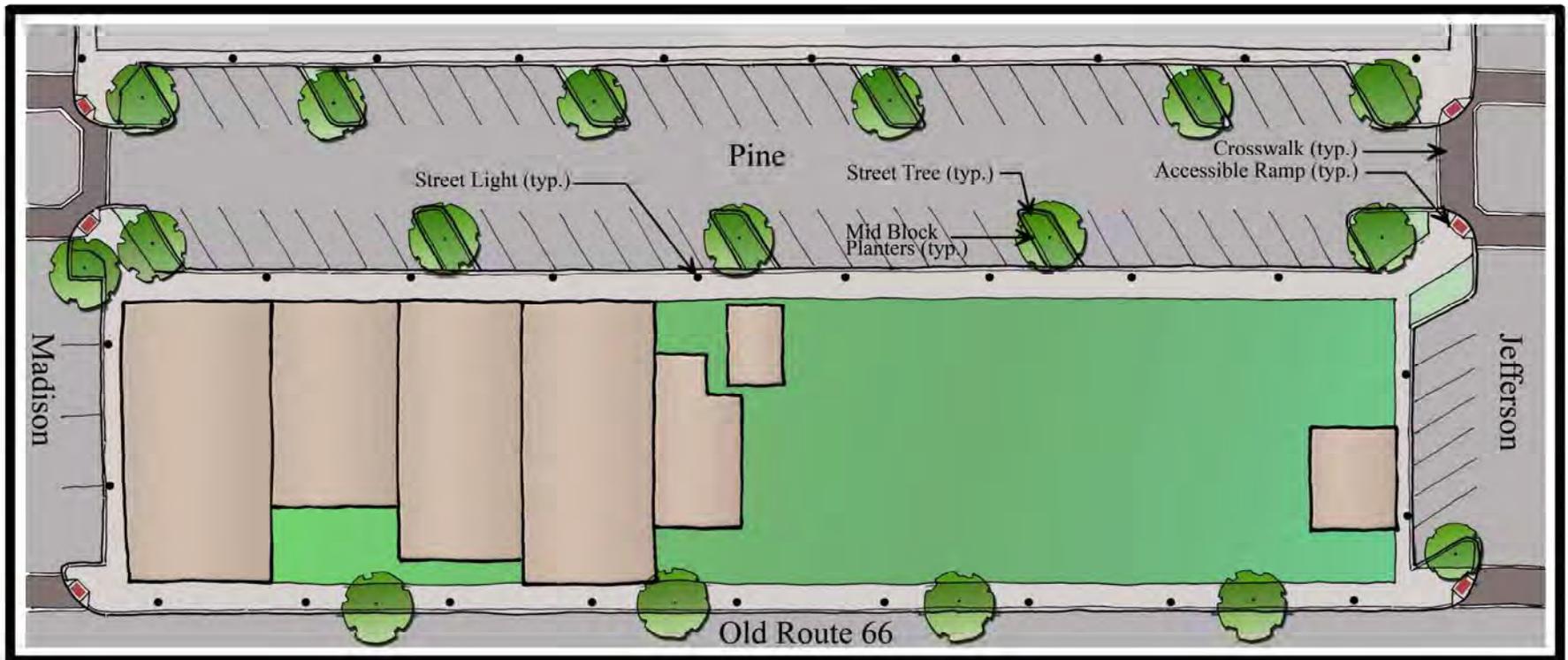
5.1.1 Streetscape Design Concept

Creating a complete streetscape for Downtown Strafford starts with recognizing the existing framework and design elements, and then using these to establish a consistent rhythm throughout the designated area of the City. Since there is little to no existing streetscape elements, Downtown Strafford presents a “clean slate” from which to begin. The first step will be the implementation of infrastructure improvements including sewer and stormwater upgrades. Undergrounding the utility lines should occur during this process also. Then the addition of curb, gutter and sidewalks can begin. Only after these improvements are made can additional elements be introduced such as paver sidewalks and crosswalks, period light and sign poles, tree planters and other landscaping and site furnishings. Lighting needs to be established with consistent spacing between. Consistent forms of site furnishings will be implemented throughout the design area. Benches need to be positioned to help encourage public interaction.

Enhanced crosswalks and curbside planting areas are additional elements which will help to establish the identity of Downtown. The following illustration shows a typical block with potential elements of what an established streetscape could resemble for Downtown Strafford. The design concepts presented are only suggestions to encourage and assist the city and other interested parties with revitalization efforts.

5.1.2 Typical Block Streetscape Plan

The block shown is Pine Street (and Old Route 66) from Madison St. (west) to Jefferson Street (east).



Suggestions for new elements include:

- Consistent curb and gutter installed
- Improved street corners with designated crosswalks, bump-outs and ADA accessibility
- Clearly delineated parking spaces
- Planting of street trees and location of lighting in organized manner

5.2 Signature Projects:

Downtown Strafford is bisected by Old Route 66 (Highway 125) and the railroad, both of which are integral to the history and development of Strafford. While these elements can present a challenge, the opportunity is even greater to capitalize on their potential significance to the revitalization of Downtown and continue the desired design theme overall.

Highway 125, coming off the interchange with I-44, serves as the major entrance into Downtown from the north. Currently there is no identification that visitors are about to enter the Downtown as they travel south on 125. As such the opportunity exists for some type of monument signage to clearly identify the unique attributes of the Downtown district. The signage should include the theme, color, and other elements represented throughout Downtown.

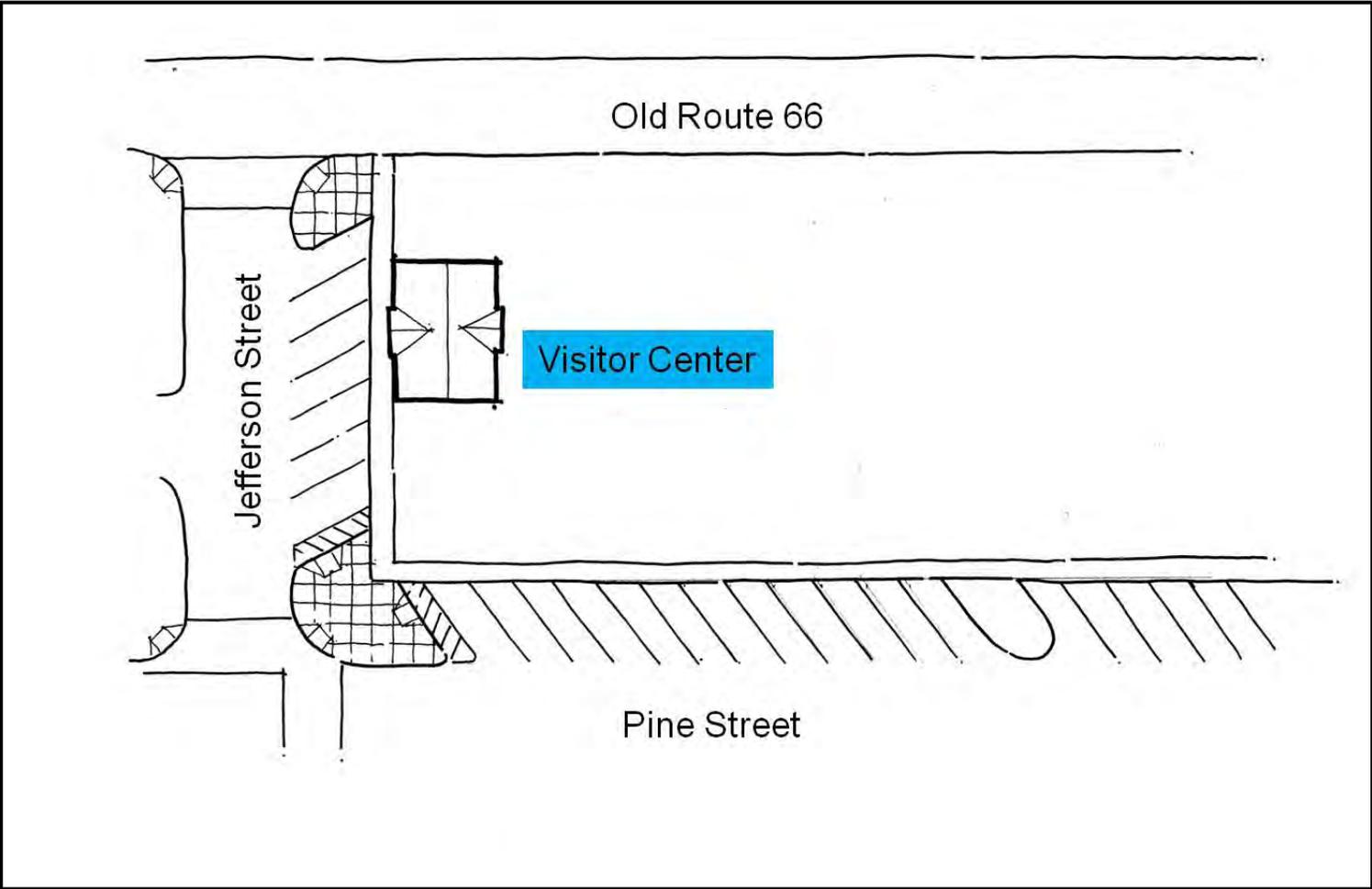
In addition, there are several land uses or vacant lots along Pine Street and/or Old Route 66 which may not represent the highest and best use for Downtown or present visitors with the City's best face. The City and Chamber have recently looked at the feasibility of constructing a visitor center in the form of a replica of the Frisco Depot that once stood along Route 66 near Madison Street. There is a great opportunity for utilizing some of the land fronting both Pine Street and Old Route 66 for the Depot building and other kinds of civic and celebratory uses, such as the Route 66 Days festival.

The City of Strafford should determine that improvements such as these are significant projects for Downtown's revitalization. There are definite challenges including costs and property ownership, however, the opportunity these improvements offer for the revitalization of Downtown Strafford is tremendous. The design priorities for these concepts are listed as follows:

- 1) Establish an attractive "entryway" for Downtown Strafford.
- 2) Create a visitor and destination space in the form of a replica depot building with complimentary parking/circulation, and design.
- 3) Create dynamic public space around depot building for civic events, visitor attraction, and civic pride.
- 4) Develop identity of the public space through naming, signage, and design.
- 5) Reduce the amount of pavement and add green space.
- 6) Identify areas for monument and wayfinding signage which reflects the heritage of Strafford.
- 7) Adopt a creative design plan based on flexible use, attainable cost and long term maintenance.

5.2.1 Strafford Station Concept - Phase 1

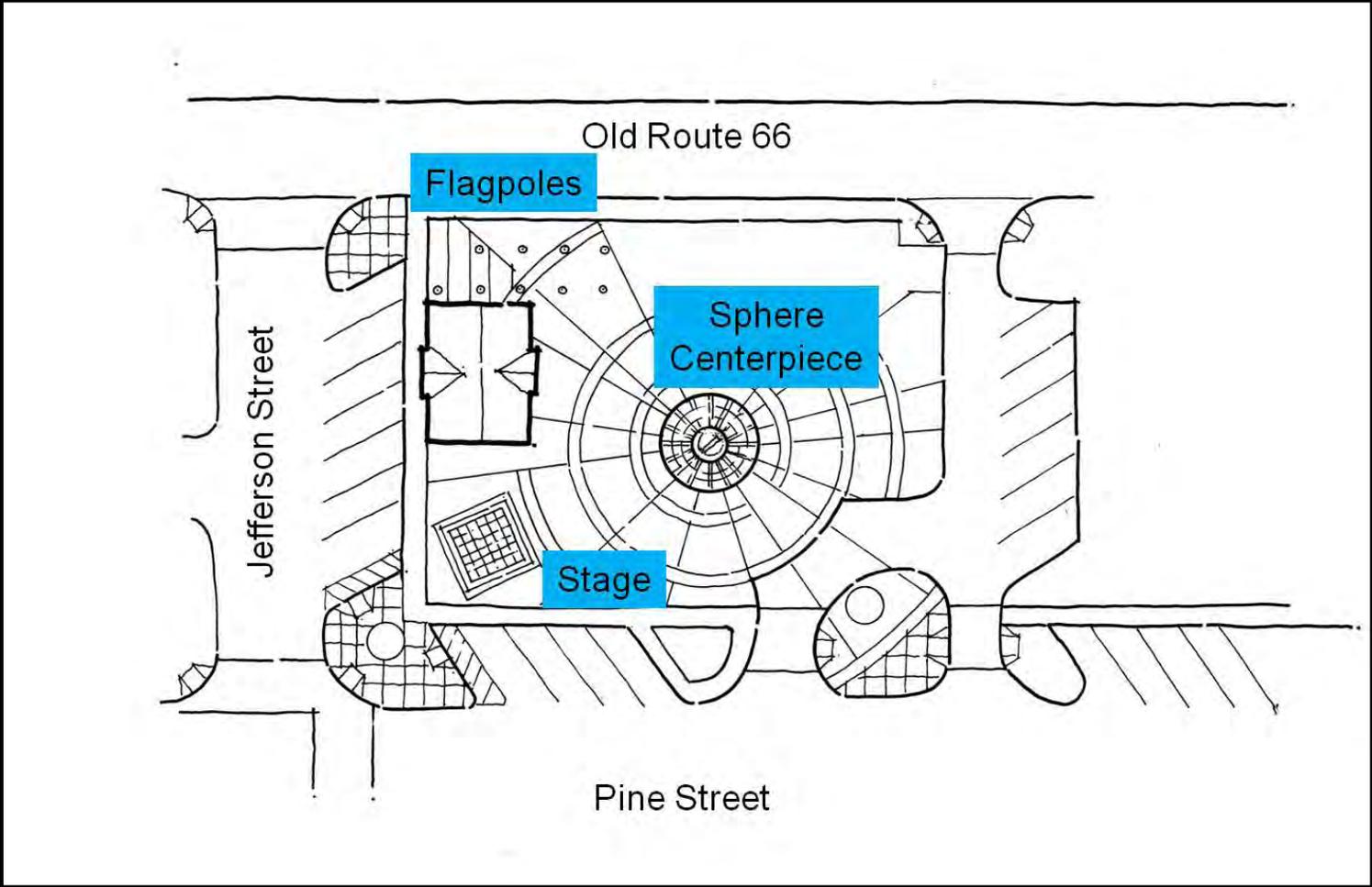
POTENTIAL IMPROVEMENTS:
Visitor's Center and Chamber of Commerce office



Plan View

5.2.2 Strafford Station Concept - Phase 2

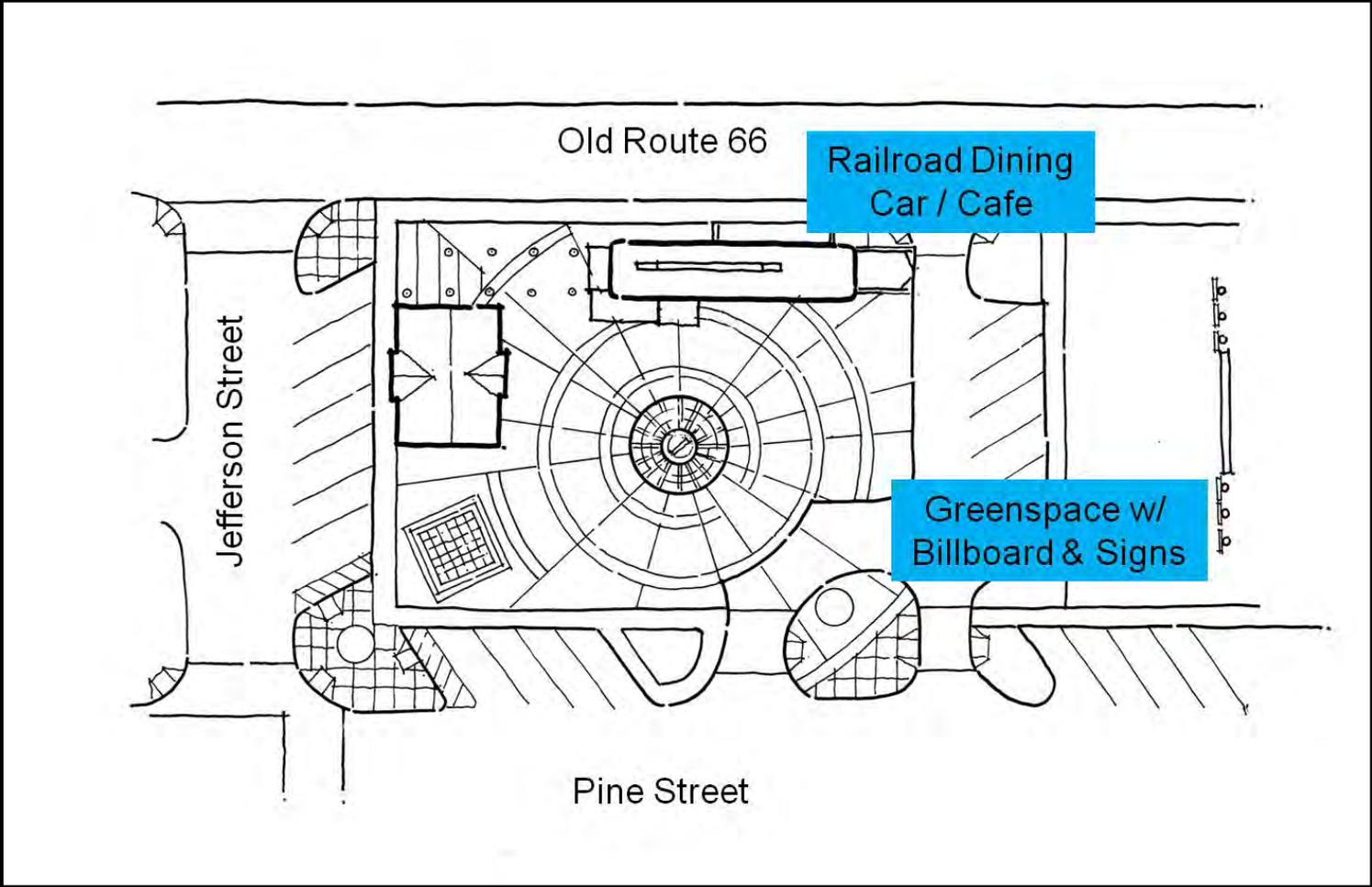
POTENTIAL IMPROVEMENTS:
Public Space and Amenities



Plan View

5.2.3 Stafford Station Concept - Phase 3

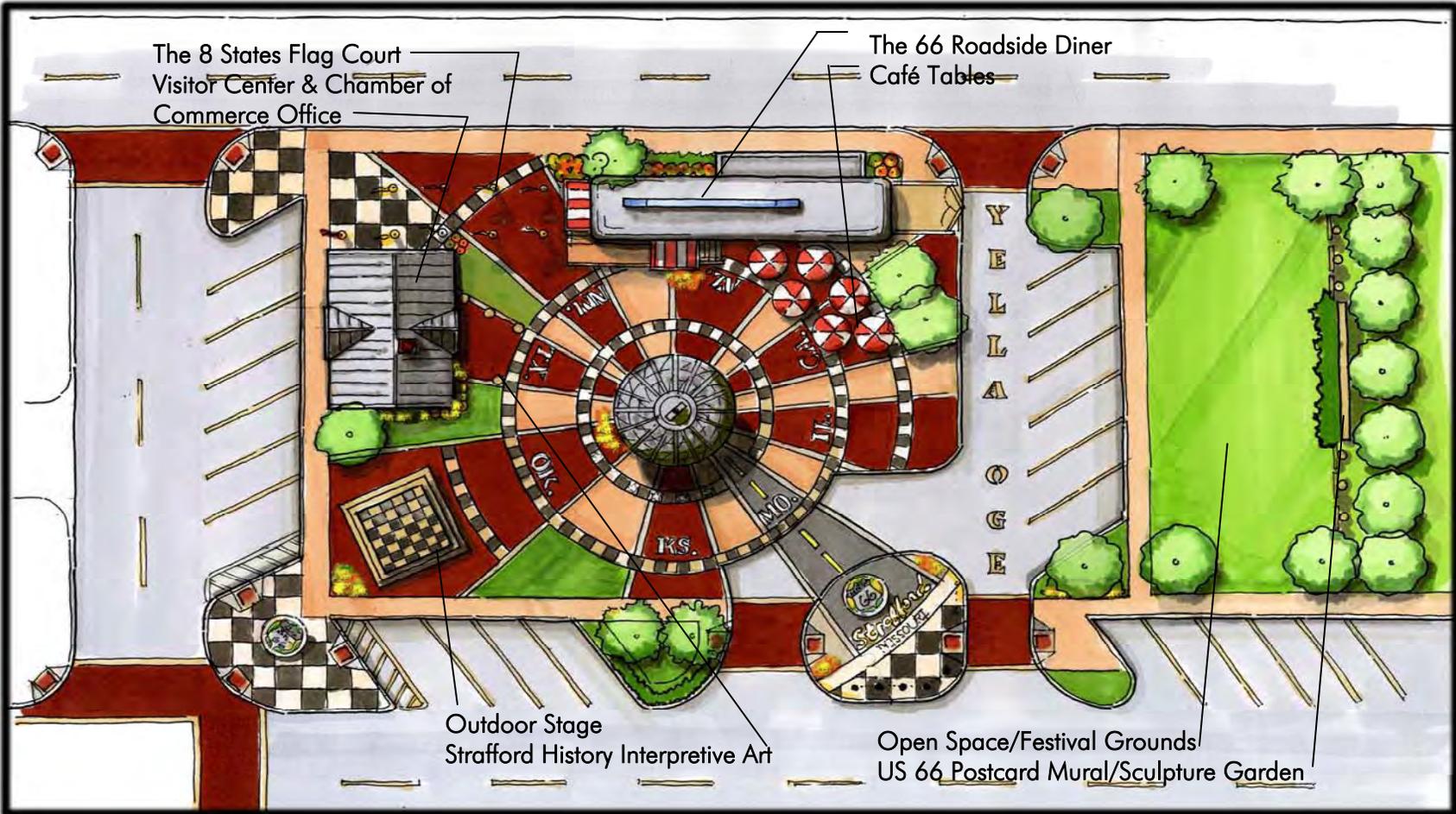
POTENTIAL IMPROVEMENTS:
Roadside Diner, Greenspace, Billboard & Sculpture Garden



Plan View

5.2.4 Stafford Station Plan View

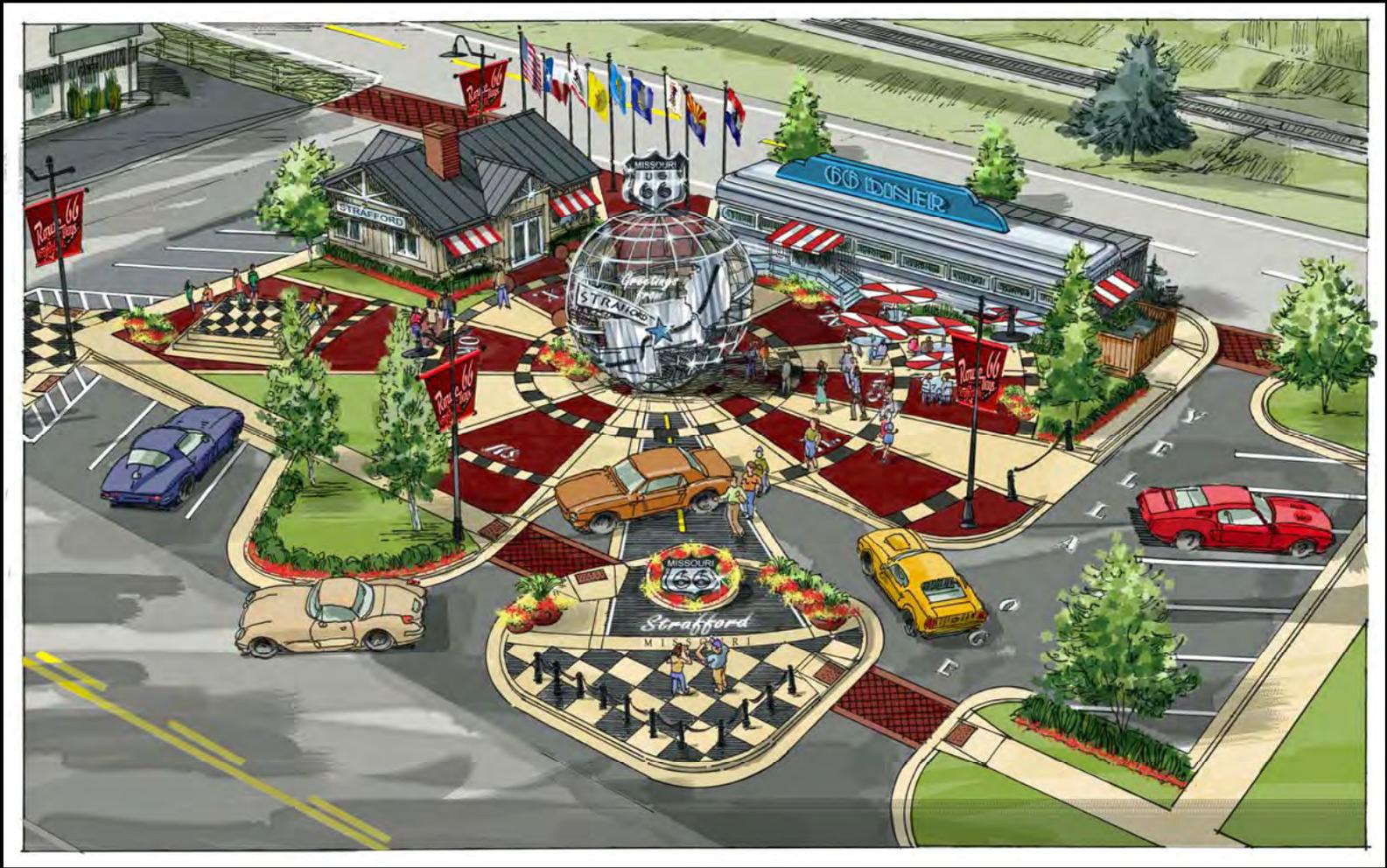
POTENTIAL IMPROVEMENTS:
Chamber Office, Visitor's Center and Route 66 Tourism Destination



Plan View

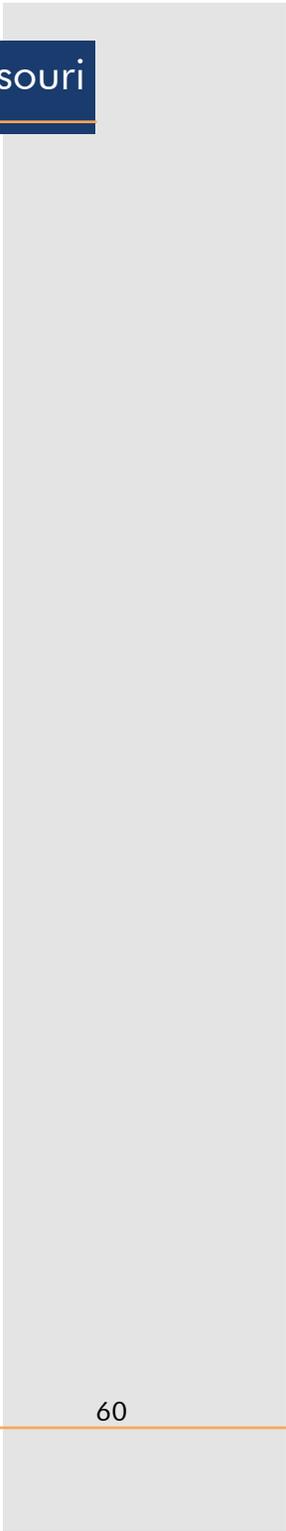
5.2.5 Strafford Station Perspective

POTENTIAL IMPROVEMENTS:
Chamber Office, Visitor's Center and Route 66 Tourism Destination



Perspective Rendering

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6.0 DOWNTOWN STRAFFORD BUILDING ELEVATIONS

6.1 Building Elevation Illustrations:

In addition to the streetscape improvements previously discussed, the opportunity exists for façade enhancements and other site improvements as part of the Downtown Strafford revitalization efforts. An important task item within the DREAM planning process for Strafford was the Community Focus Groups. During these discussions, the participants enumerated on multiple occasions their desire for adopting a Route 66 theme to draw visitors, building upon the annual Route 66 Days festival, marketing Downtown with a focus on Route 66, and adding greenspace, planters, murals, and other creative aesthetic improvements.

The City of Strafford provided PGAV Planners with a list of 10 specific buildings within Downtown to complete before-and-after views and show potential façade and site improvements. On the following pages are conceptual ideas of enhancements that could be made to each building to build upon what is existing, carry out the desired Route 66-era theme, and enhance the architectural design and visitor experience of Downtown. The design concepts presented are only suggestions to encourage and assist property owners, the city, and other interested parties with revitalization efforts.

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6.1.1 A-1 Towing

POTENTIAL IMPROVEMENTS:

Existing Façade



Potential Façade



- Restore cornice where applicable.
- Utilize canvas/fabric awnings to match business color scheme.
- Install banding for business signage of appropriate scale and design above door.
- Use compatible paint colors.

- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.
- Provide accessory items: Route 66 signs, flags, pole banners, wall mural.
- Provide landscaped median / island.

6.1.2 C & C's Kuntry Fied Cafe

POTENTIAL IMPROVEMENTS:

Existing Façade



Potential Façade



- Restore cornice where applicable.
- Provide similar awning over front door.
- Install business signage of appropriate scale and design above door.
- Use compatible paint colors.
- Utilize ornaments sign lighting.

- Streetscape of lights, trees & site furnishings.
- Clearly delineate parking / pedestrian area.
- Provide outdoor seating area / tables & umbrellas.
- Provide landscaping.

6.1.3 Police Building

POTENTIAL IMPROVEMENTS:

Existing Façade



- Remove frame, metal & shingled canopies.
- Install business signage of appropriate scale, design, and lighting.
- Use compatible paint colors.

Potential Façade



- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.
- Provide retail windows wherever possible.

6.1.4 MFA Building, Family Pharmacy, Main Street Salon & Student Center

POTENTIAL IMPROVEMENTS:

Existing Façade



Potential Façade



- Remove paint from masonry façade using correct removal methods.
- Restore original windows or provide retail windows wherever possible.
- Restore original entrances.
- Restore masonry veneer.
- Restore cornice where applicable.
- Square up parapet wall with dental trim.
- Utilize canvas/fabric awnings to match business color scheme.
- Install business signage of appropriate scale and design above doors (excepting Family Pharmacy).
- Use compatible paint and trim colors.
- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.

6.1.5 S & S Video

POTENTIAL IMPROVEMENTS:

Existing Façade



Potential Façade



- Provide retail windows wherever possible.
- Clearly delineate front door/public entrance/pedestrian areas/parking.
- Use compatible paint colors to match color scheme of business.

- Install business signage of appropriate scale and design above door.
- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.
- Provide permanent monument sign.

6.1.6 Joe's Diner

POTENTIAL IMPROVEMENTS:

Existing Façade



Potential Façade



- Install business signage of scale and design to advertise the building.
- Use compatible paint colors.
- Provide screening to nonpublic areas.

- Clearly delineate parking and pedestrian areas.
- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.

6.1.7 Potter Station

POTENTIAL IMPROVEMENTS:

Existing Façade



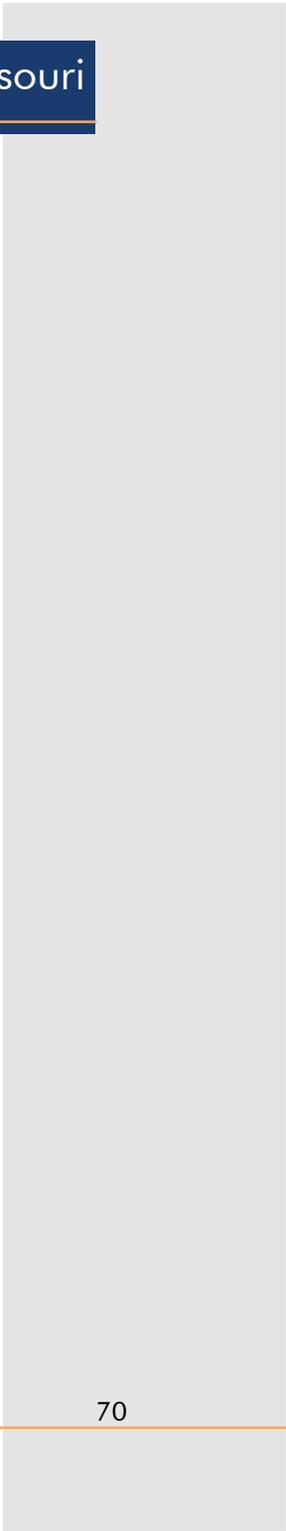
Potential Façade



- Remove paint from masonry façade using correct removal methods.
- Restore original windows.
- Restore original entrances.
- Restore masonry veneer.
- Restore cornice where applicable.

- Utilize canvas/fabric awnings.
- Install business signage of appropriate scale and design above door.
- Use compatible paint colors.
- Utilize ornaments sign lighting.
- Streetscape of lights, trees & site furnishings.

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7.0 IMPLEMENTATION

7.1 Introduction

The general purpose and intent of the building design guidelines is to promote the rehabilitation and preservation of contributing buildings and the development of appropriate new construction within Downtown Strafford.

Any existing architectural character of Downtown must be preserved and enhanced for the future stability of the City of Strafford. The existing buildings, density and open space help to define Downtown and give identity to the City of Strafford, as well as provide a basis for all future work.

The Building Design Guidelines are also developed as a resource for property owners to utilize in the rehabilitation of existing structures and the development of new buildings. The long term goal of these guidelines is to develop a higher standard of quality for any development in Downtown. Implementation of the guidelines and developing an appreciation for preservation and quality design will help to sustain the character of Downtown Strafford. Important implementation measures include the following:

7.1.1 Downtown District

- **Establish the Downtown Zoning District:** The Downtown Zoning District will be the defined area for the supplemental design standards. The Downtown District should be the same boundaries of any community improvement district. The intent of this zoning district is to ensure the correct preservation and rehabilitation of existing structures where possible, and that new infill construction adheres to the higher standards.

7.1.2 Supplemental Design Standards

- **Develop supplemental design standards:** A long term goal would be to eventually develop supplemental design standards. The design standards are the next step, after design guidelines, to elevate the level of design and rehabilitation in Downtown Strafford. Supplemental design standards should only be developed after the design guidelines have been implemented by downtown property owners. The existing building and zoning codes should be supplemented with design standards for the Downtown district.

The supplemental design standards should be specific enough to describe what is acceptable, or not acceptable, regarding design, materials, means and methods of the construction of exterior architectural features. An example of the text and graphics for the supplemental design standards is on the following pages. The standards should establish a concise and easy to understand direction for property owners and contractors involved in the rehabilitation and construction of new buildings in the Downtown district. These design standards will supplement the building code of the City of Strafford. The supplemental design standards should not be interpreted as superseding, nor an abandonment of, the existing building code of the City.

EXAMPLE OF TYPICAL DESIGN STANDARDS

The following text and graphics are an example of a set of design standards which supplement existing building codes. The design standards are written to give specifics in regard to design, dimensions, materials and methods. The following text is listed as an example for windows.

V. Windows

V.I Windows at Public Façades.

- A. Windows in Public Façades shall be one of the following (Refer to Figure A):
1. The existing window repaired and retained.
 2. A replacement window which duplicates the original and meets the following requirements;
 - A. Replacement windows or sashes shall be made of wood or finished aluminum.
 - B. The profiles of muntins, sashes, frames and moldings shall match the original elements in dimension and configuration.
 - C. The number of window lites, their arrangement and proportion shall match the original or be based on a Model Example.
 - D. The method of opening shall be the same as the original with the following exception: double-hung windows may be changed to single-hung.
- B. Reconstructed windows and sashes in a Public Façade shall be based on the following;
1. An adjacent existing window in the same façade which is original; or
 2. If all windows on a façade are being replaced than they shall be based on a Model Example or the window detailed in Figure B.
- C. Glass Types at a Public Façade
1. Glass in historic windows on a Public Façade shall be one of the following:
 - A. Clear glass or other original glazing;
 - B. Glass based on a Model Example; or
 - C. Insulated glass set 3/8" back from the exterior face of the sash.

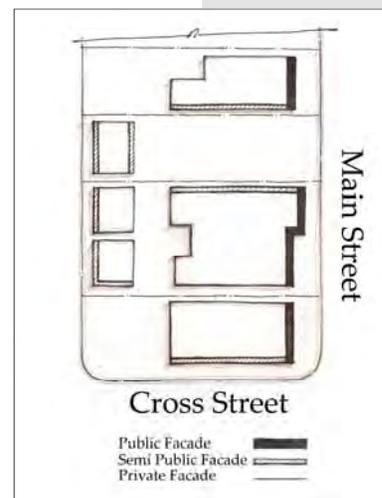


Figure A

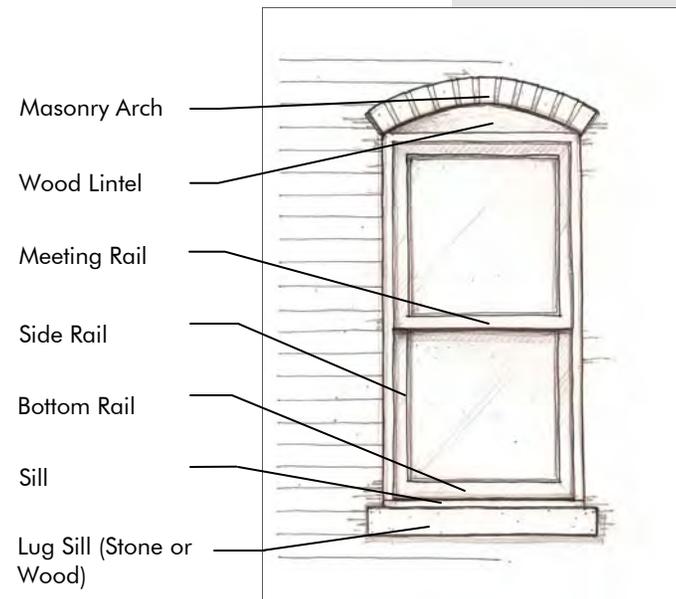


Figure B

2. The following glass types are prohibited in Public Façades:
 - A. Tinted glass;
 - B. Reflective glass;
 - C. Glass block; and
 - D. Plastic (Plexiglas) except Lexan or an equivalent.

- D. Abandoned Windows in a Public Façade Windows which are to be abandoned on the interior shall be infilled by closing them with wooden shutters set ½" back from the face of the wall with the window opening left intact including the frame, sash, sub-sill and lintel.

- E. Storm Windows and Screens at a Public Façade *Comment: Storm windows and screens may be installed at the interior or at the exterior. Interior installation is preferred because of the increased visibility of the exterior of the window and its details.*
 1. Materials
 - A. Exterior storm windows and screens shall be made of wood, aluminum or plastic. Wood shall be painted; aluminum shall be factory or field painted. Clear anodized aluminum is prohibited.
 - B. Interior storm windows and screens are not regulated by these Standards.
 2. Storm windows and screens shall also meet the following requirements:
 - A. The dimensions of the area of glass or screen shall be the same as the area of glass in the window being protected.
 - B. The meeting rail of the storm or screen window shall be In line with the meeting rail of the window being protected. Additional meeting rails are prohibited.

- F. New Window Openings Are Prohibited in a Public Façade, except as required by City Health and Safety Codes.
 1. No new window openings shall be created in a Public Façade.
 2. No existing window opening in a Public Façade shall be altered in length or width.

7.1.3 Approval Process

The goal of the Downtown District is to develop a higher standard regarding design, preservation and maintenance. The method to achieve the higher standards for the Downtown District, is a simple and well defined project approval process. The process should be easy to understand for developers, property owners and the City staff overseeing building and zoning codes. The following text describes a project approval process for the future. This process should only be implemented once the market and the property owners of Downtown justify the system. A graphic illustrating the potential process is illustrated, at right, and described in detail below.

STEP 1: Plan Review: Property owners should meet with the proper City officials, including a possible future Community Development Department, for the review of the project plans. The project plans should include drawings of a site plan, elevations and details regarding building materials, colors, accessibility, and dimensions. The plans should also list the proposed use of the site and building and whether such use complies with existing zoning codes. The project plan review then proceeds to the Planning and Zoning Commission (P&Z).

STEP 2: Planning and Zoning Commission: City Officials (Community Development Department) submits the project report and their staff recommendation to the Planning and Zoning Commission. The Planning and Zoning Commission should meet monthly to review project proposals. The Commission should create a Seat for an individual with an established background in Architectural design for the purpose of establishing consistent, implementable design standards.

The Planning and Zoning Commission (P&Z) would review the report as prepared by City Staff, which could include site plans, building elevations, cross sections and illustrations. City Staff should present their findings and recommendations to the Planning and Zoning Commission. The property owners should be given the opportunity to present and answer questions regarding their project. Adjacent property owners, business/neighborhood associations and the general public should also be allowed to speak on

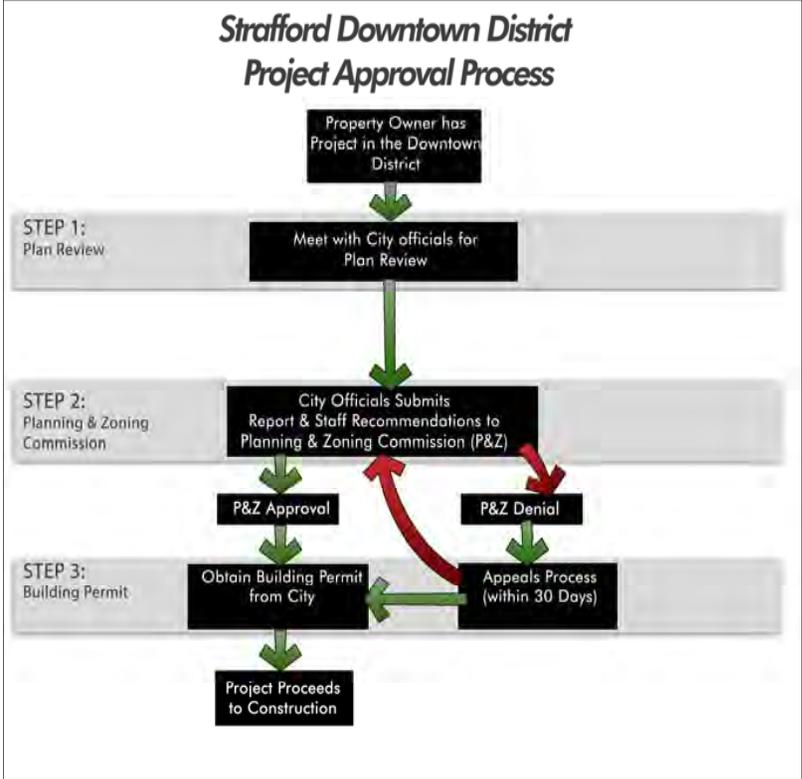


Illustration of the proposed project approval process for the Downtown District.

proposed projects at the board meetings. The Planning and Zoning Commission will approve or deny the proposal based on staff recommendations and their professional acumen.

If necessary, denials from the Planning and Zoning Commission could be appealed through the City's normal appeal process. Review of the appeal should occur within thirty days so as to not hinder the progress of the project and property owner.

STEP 3: Building Permit: Project resolutions approved by the Planning and Zoning Commission would then return back to City Staff for a building permit. Inspections during construction, for compliance to the approved plans, would allow for a final occupancy permit. The occupancy permit would only be issued once final construction is complete.

7.1.4 Service

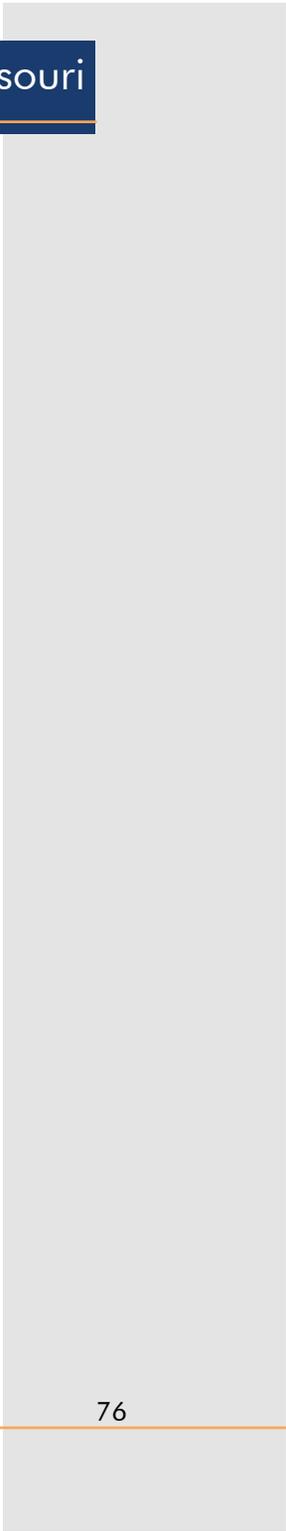
Municipal officials and employees, as well as members of community organizations, should view the citizens that they serve as valued customers. They provide a service to the residents and citizens of the community, and thus should approach their daily tasks with the professionalism that the proud members of the community deserve.

7.1.5 Next Steps

Implementing building design guidelines requires public support and buy-in. Suggestions for next steps include:

- The Strafford DREAM Committee needs to open a dialogue between City and Downtown property owners about building and development issues.
- The DREAM Committee should work with local contractors to develop a program to address common issues such as infrastructure improvements.
- Other activities such as rehabilitation training, recognition, and a streamlined permitting process will be well-received by Downtown property owners. The DREAM Committee should continue work with City departments to address building maintenance issues by educating property owners.
- Continue to enhance Downtown's public spaces and streetscape. Ideas such as the Strafford Station concept plan and related streetscape improvements can reflect the City's investment in Downtown. However, this investment must continually be demonstrated by proper maintenance of improvements.

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APPENDICES

APPENDIX A: SECRETARY OF THE INTERIOR STANDARDS FOR REHABILITATION

(36 CFR Part 67)

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

APPENDIX B: GLOSSARY

WEATHERBOARDS: Long, thin horizontal boards with a square cross section that are overlapped and applied as the exterior surfacing material on homes and buildings.

BASE: The lowest part of a column, below the shaft; the supporting, or lowest, part of a building.

BALUSTRADE: A railing or low wall consisting of a handrail on balusters (vertical posts) and a base rail.

CAP: The top member of a column or pilaster.

CLAPBOARDS: Long, thin horizontal boards with a triangular cross section that are overlapped and applied as the exterior surfacing material on homes and buildings.

CLERESTORY: An upper portion of a wall which has windows for the purpose of admitting light into a large room.

CONTEXT: The surrounding environment (streets, buildings, landscape, etc.) in which a building or site exists.

COPING: A covering (or capping) course on the top of a wall or parapet.

CORBEL: An architectural member (of stone, wood or metal) which projects from the side of a wall to serve as a support for another element, such as: a cornice, the spring of an arch, a balustrade.

CORNICE: A projecting ornamental molding which caps the top of a building.

DORMER: A window set vertically in a small gable projecting from a sloping roof; the roofed projection in which this window is set.

ELEVATION: A scaled, non-perspective drawing of a building façade.

FAÇADE: An exterior face of a building, usually the front.

FASCIA: A horizontal band of vertical face trim.

FREESTANDING SIGN: A sign which is detached from the building, and is mounted to columns, posts, or any upright member that is supported from the ground or other object; or a detached sign which is erected on the ground.

GABLE: The triangular wall section, formed by ends of a sloping roof.

HOOD MOLDING: A projecting molding on the face of a wall, over an opening (doorway or window), to deflect the rain.

INDIRECT LIGHTING: Light from a concealed source, which reflects onto the sign face.

INTERNAL ILLUMINATION: The means of lighting from a concealed or contained source within the sign, which becomes visible through a translucent surface.

KICK PLATE: A solid panel beneath a storefront display window.

LANDMARK: A prominent building or feature officially designated as having special status and protection.

LATTICE: An openwork screen or grill made of interlocking or overlapping strips.

LINTEL: A horizontal structural member (such as a stone or beam) which spans an opening.

LUMINAIRE: A complete lighting unit or the housing for a light bulb or lamp.

MOLDING: A decorative, or shaped strip of wood, metal, brick, etc., usually mounted horizontally, and used to ornament or finish the surface of a structure.

MOTIF: A significant, repeated element of design in a composition.

MONUMENT SIGN: A free-standing sign, generally low to the ground with a continuous connection to the ground (as opposed to being supported on a pole).

PARAPET: The top section of a wall which projects above the roof line.

PRESERVE: To protect and keep in an unaltered condition. Preservation usually includes the overall form of the building, its structural system and finishes, decorative details, and even landscaping. Preservation may also include keeping alterations and additions that have become important.

RECONSTRUCT: To reproduce, in detail, a structure as it existed at some time in the past, either through the original construction methods, or other methods which produce the same visual result. Accurate reconstruction requires knowledge and evidence of the original design.

REHABILITATION: The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values.

REMODEL: To remake; to make over. In remodeling, the appearance is changed by removing original detail and altering spaces. New materials and forms are installed. Applying a modern front to an older building is an example of remodeling. Often, these changes are not reversible.

RENOVATION: The act or process of modernizing a building without making an effort to retain historically significant architectural features. Renovation permanently destroys the historic integrity of a building.

RESTORATION: The act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work and/or by the replacement of missing earlier work.

SHAFT: The main portion of a column, between the base and capital.

SILL: The bottom horizontal member of a window or door frame.

SPANDREL: The area between two arches and a horizontal cornice above them.

STABILIZE: To make resistant to change in condition. A building is usually stabilized to retard deterioration until it can be repaired. A weather-resistant closure and a safe structural system are minimum stabilization efforts.

STRING COURSE: A thin projecting horizontal strip of masonry on the façade of a building.

TERRA COTTA: A decoratively molded ceramic material, often glazed, used for architectural motifs or ornamentation on a building.

TRANSOM: A horizontal cross bar in a window, over a door or between a door and the window above it. This also refers to the window (often hinged) above a door.

VOUSOIR: One of the wedge like stones of which an arch is composed.

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