

DOWNTOWN
REVITALIZATION &
ECONOMIC
ASSISTANCE FOR
MISSOURI

BUILDING
AND
STREETScape

DESIGN GUIDELINES

September 26, 2008



ACKNOWLEDGMENTS



DOWNTOWN REVITALIZATION AND ECONOMIC ASSISTANCE FOR MISSOURI (DREAM) PROGRAM SPONSORS:



PLANNING CONSULTANT



DOWNTOWN
REVITALIZATION &
ECONOMIC
ASSISTANCE FOR
MISSOURI

BUILDING AND
STREETSCAPE
DESIGN GUIDELINES

Table of Contents

INTRODUCTION 4
 Background 4
 Intent of Guidelines 5

BUILDING DESIGN GUIDELINES 7
 Downtown Fabric of Buildings 7
 Façade Elements 7
 Rhythm 7
 Building Zones 8
 Alterations 9
 Masonry 10
 Awnings 12
 Lighting 15
 Signage 16
 Bicycles 18
 Outdoor Café Seating 18
 Sidewalk Zones
 Franchise Architecture 23
 Maintenance of Facades 24
 Historic Buildings 25
 Original Elements 25
 Storefront 25
 Entrance
 Windows
 Spandrel Panels
 Transoms
 Upper Façade 27
 Windows
 Cornice and Architectural Details
 Rear Façade 29
 Entry Door
 Upper Rear Façade
 Fences
 Trash, Ancillary Structures, & Utilities
 Existing Buildings and New Construction 31
 Color Guidelines 31

BUILDING DESIGN EXAMPLES 32
 Mitchell Drug Store / Security Finance 33
 A Cut Above / Firestone / Vacant 34
 Rose’s Boutique / Ted’s Jewelry / Doyne’s 35
 Vacant / Insurance / Nail Salon 36

STREETSCAPE DESIGN GUIDELINES 37
 Kennett Square 37
 First Street Approach 38
 Sheryl Crow Aquatic Center 38
 Design Coordination 42
 Infrastructure 42
 Pedestrian Access and Sidewalks 43
 Signs and Banners 44
 Parking and Service Areas 45
 Parking Meters 45
 Lighting 47
 Site Furnishings 47
 Landscaping 49

WAYFINDING 53
 Wayfinding Principles 53
 Wayfinding Components 54
 Wayfinding Planning Areas 55

IMPLEMENTATION 58
 Recommendations 58
 Short-Term 58
 Long-Term 59

APPENDIX

Attachment 1 – Missouri Financing Mechanism Matrix

This Page Intentionally Left Blank

INTRODUCTION

Background

From an appreciation of the original architecture of Main Street to the introduction of new buildings and structures into the streetscape, all of the elements must have a look and feel of belonging in the same composition. This document is a guide to recapturing the charm and historic feel of Kennett while promoting new development.

Kennett was laid out as the seat of Dunklin County, the first “Bootheel” county, in 1846. The town was first called Chilletecaux for a Delaware Indian living here at the time. Later it was known as Butler. In 1851 it was renamed Kennett for the Mayor of St. Louis, L. M. Kennett. The county name honors previous Governor Daniel Dunklin.

Kennett developed as a trade and legal center as Dunklin County grew into a noted cotton, soybean and livestock farming area. During the Civil War the county was known as the “Independent State of Dunklin” after adoption of a resolution in 1862 to secede from the Union. After the war development began with the arrival of the Little River Valley and Arkansas Rail Road (Cotton Belt) to Kennett. Effective land reclamation began in 1893 when the state provided for organization of county drainage districts and levees on the St. Francis River. Dunklin County is in the Little River Drainage District, one of the largest drainage systems in the United States.

By 1893, Kennett boasted of newly-built brick shops on the south side of the downtown square, four attractive churches and a \$9,000 public school building. Continuous growth brought a population increase to 45,329 by 1950.

The Courthouse Square area of Kennett during the late 19th and early 20th century was made up of traditional street front commercial facades. These facades, which typically lined both sides of the street, had a unity of materials, scale, and style. Because of their similarities in design the street had an appearance of rhythm and order. Similar patterns and elements were repeated giving any facade a visual connection with its neighbors. This backdrop of buildings created a harmonious public space filled with activity. This space remained intact for generations with little or no change until after World War II.

At that time there was a movement in America to remove all things that were considered old and replace them with something new and modern. During the years since subtle changes took place that had a commutative effect on the entire block. Original glass storefronts were removed and replaced with smaller windows and entrances. Upper facade windows were removed or covered up. Building cornices and ornaments were removed in an attempt to “clean-up” the old looking facade and new inappropriate materials were added. In some cases entire buildings may have been removed and replaced with new buildings with little or no respect for the rhyme and scale of the street. Historic light poles and fixtures were removed and replaced with out-of-scale “cobra-head” fixtures and poles. These changes began to add up and over the years the sense of the main street community space was lost.



Intent of Guidelines

This space can be recovered but it will take a committed effort from the entire community to accomplish. The first step is to evaluate what is there by a comprehensive survey of the current building stock. What is the condition of an existing building? When was it built? How has it changed? Have any buildings been torn down and if so, what has replaced them? A search for historic photographs or drawings can be useful as they provide a great resource in determining what is appropriate and complementary.

The name ***Kennett Square District*** has been chosen to describe the area of study and is a working name that can be changed at any time. The next step is to develop a plan to accomplish the renewal of the ***Kennett Square District***. This means resolving what design steps the community can agree on that will bring back the vitality of to the district and restore a sense of civic pride. In order for the ***Kennett Square District*** to be a success it must respect the traditional rhythm and unity that existed, and not just create a copy of the historic past. Work with the fabric of what is there and develop a sensible approach to the renewal of the district. This includes the removal of inappropriate materials and making good choices for new materials so that the harmful mistakes of the past are not duplicated. Preservation of character-defining elements of historic buildings should be a top priority, and any renovation or repair should accurately represent the historic qualities of the buildings. Original documentation therefore should be used for restoration work whenever possible. Where original documentation is not available, interpretations of similar elements that occurred in the area may be considered.

The building facades along with the sidewalks and street make up the outdoor living room of our city. This space can be the heart of our community, alive with activities and events. The responsibility of redevelopment falls to both individual property owners and the city. The individual building facades are owned and maintained by the property owners. The street improvements, utilities and sidewalks are the responsibility of the city. An individual owner cannot be expected to invest in redeveloping his building without the city commitment to restore the streetscape and the city can't be expected to make these investments without commitment from the property owners. Both areas must be redeveloped at the same time and pace for a successful redevelopment of the ***Kennett Square District***.

Important aspects of the building façade portion of a plan will include removal of the continuous metal awnings, removal of inappropriate building materials, and reestablishing the look of the original storefront. Because of the many changes that have occurred over the years to the building fabric there may be facades of different eras along any one block. Although a facade may not be what can easily identified as historic, a well designed facade of any era has a value and should be retained. Provided a building has an appropriate relationship with its neighbors in proportion, composition and rhythm it can contribute to the sense of place. Newer buildings should be evaluated on their own qualities and not by how historic they look.



122 College.

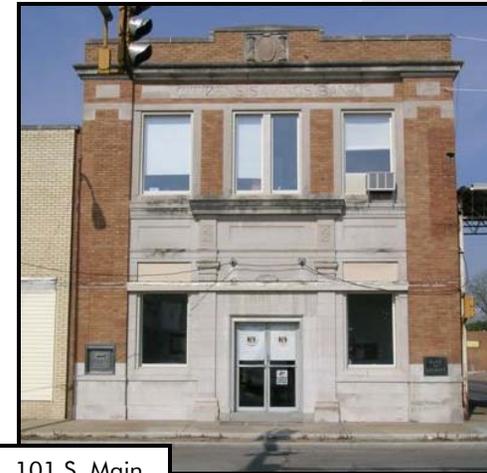
Buildings listed on the National Register of Historic Places or located within the boundaries of a National Historic District may be eligible for National and State Historic Tax Credits provided the Secretary of the Interiors Guidelines for Historic Properties are followed.

A successful streetscape plan will incorporate improved sidewalks, parking and street lighting. The addition of large green spaces, trees, shrubs, new trash receptacles, and benches will help soften and humanize the environment of the ***Kennett Square District***.

It is important that Kennett City Officials and other leaders understand that this document does not achieve any design ends unto itself. This report will guide the City as it seeks to implement the ideas included in this report. The City building codes, nuisance ordinances, zoning overlays, sign restrictions, and other ordinances will create the legal documents needed to achieve the design concepts that these guidelines represent.

As the City of Kennett researches and develops this legislation with its legal counsel, critical questions to be considered and decisions to be made will include the following:

- Will these design ideas be voluntary (guidelines), or will they be mandatory (standards)?
- What will be the main mechanism used to enforce these concepts?
 - Zoning Overlay
 - Building codes
 - Historic District
 - Incentives(likely a combination of all of these will be most effective)
- What are the procedural elements required?
 - Review Board composition
 - Clearance certificates for construction
 - Appeals process
 - Penalties for non-compliance
- Do other codes need to be enacted?
- Do existing City codes need enhanced enforcement or updating?
- How will the community receive this legislation?



101 S. Main.

BUILDING DESIGN GUIDELINES

Downtown Fabric of Buildings

While these guidelines are written for the *Kennett Square District*, the design recommendations are sound advice that might be applicable elsewhere in the community. These guidelines are written for commercial areas, not residential. The principles to be discussed, in many cases, can be altered and adapted to apply to an aspect of the entire town or a specific neighborhood, but care should be taken that the *Kennett Square District* and the overall downtown area should remain unique in character.

To successfully support the revitalization of downtown Kennett, the *Kennett Square District* property owners, City staff, elected officials, and other community organizations will have to change the way they think about downtown Kennett and make a long term commitment to an overall unifying theme, feel, and sense of atmosphere. This process will not happen overnight, in a week, nor in a month or a year. Kennett will not wake up one morning and be “finished” with the establishment of the downtown place. This will be an ongoing effort that will evolve, pick-up speed, slow down, be applauded, and be criticized. The one constant should be the desire to slowly adjust the downtown core to an atmosphere that is attractive to Kennett residents and its visitors. In this overall “fabric of downtown” there will be 3 types of structures; those that contribute, those that detract, and those that do neither. The objective is to maximize contributing elements and minimize detracting elements, over time.

Façade Elements -

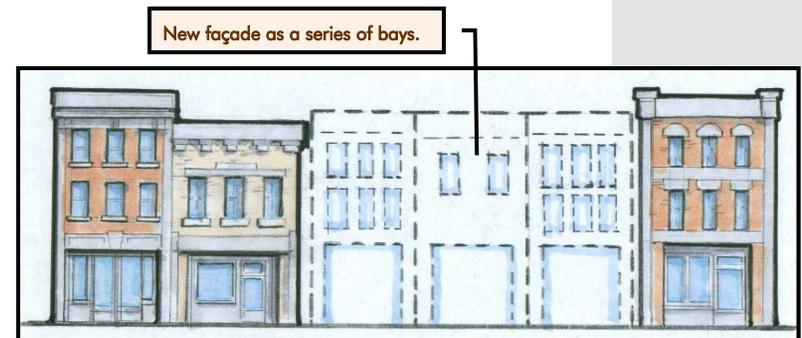
The various elements of a façade must be balanced. Appropriate massing, building and floor heights, proportions, roof lines, materials, and setbacks are some critical considerations in new construction. Any future development should be encouraged to implement a design that contributes to the fabric of downtown. Any future design that will detract from the fabric should be denied.

Other aspects like architectural details, colors, and cornices are more important to the restoration of historic buildings, but can be used effectively in new construction as well. Developing a well balanced character between all elements can allow a building to be very individual in its character, but at the same time be a complementary thread woven into the overall fabric and feel of downtown.

Rhythm -

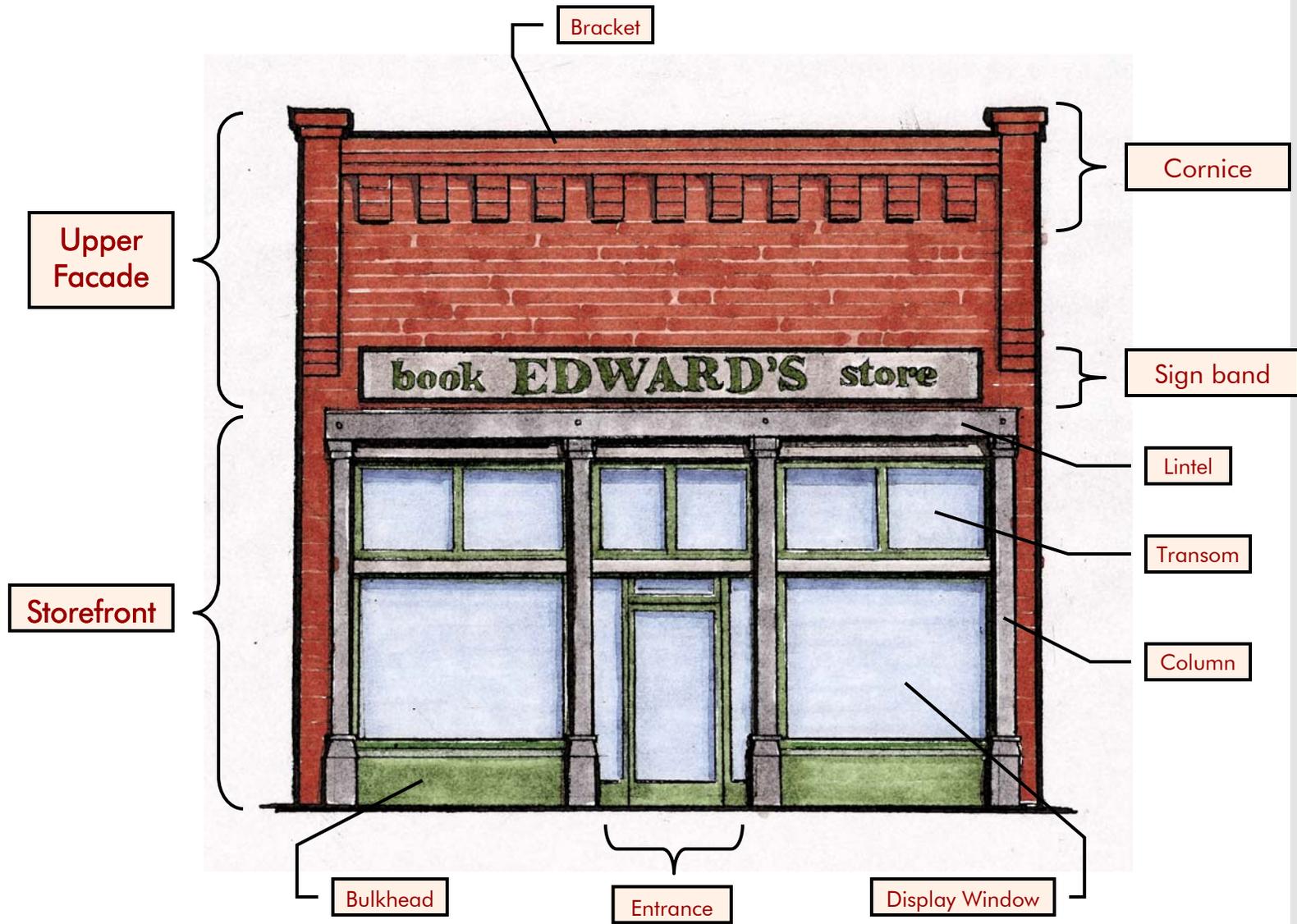
The defined rhythm of Kennett should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing.

- New infill buildings and structures should maintain the rhythm through proper repetition of details and orientation to the street.
- Vertical elements, entrances, lighting, and other street furnishings can also develop the rhythm of a specific block.



Building Zones -

Improvements to individual buildings will be discussed in the context of three distinct 'zones'; the **Storefront**, the **Upper Façade**, and the **Rear Façade**. The elements of the front façade zones are depicted in these diagrams.



Alterations -

Inappropriate alterations or additions that disrupt the fabric of the Storefront Zone should be removed. It is possible that non-historic and new construction can complement the building fabric that has developed, therefore some alterations may not need be removed. Decks, ADA structures, and other 'detachable' alterations can be utilized, but should be as unobtrusive as possible and located on the rear or sides of the building.

As a rule, any and all alterations or additions to the Upper Facade zone should be removed. Alterations in this zone can significantly change the appearance of the face of the building. This includes any and all signs and lighting as these should be restricted to the Storefront Zone. Avoid removing or altering any historic material or significant architectural features. Care should be taken during the removal process due to the possibility of damaging original elements hidden behind the alterations. When disassembly of a historic element is necessary, use methods that minimize damage to the original materials. The illustration at right depicts how alterations progress and damage the building fabric.



Original design.



Minor alterations.



Storefront is lost.



Significant alterations.

The Kennett buildings shown here underwent drastic alterations that undoubtedly saved the structures from demolition, but has disrupted the elements of the street. Likely the intricate facades shown in the photograph are gone, but with guidelines and attention to the past configuration these buildings can begin to recapture some of the elements of the building fabric and rhythm. It will never be the same, and for that reason cities should be very critical of any alterations.



Original design.

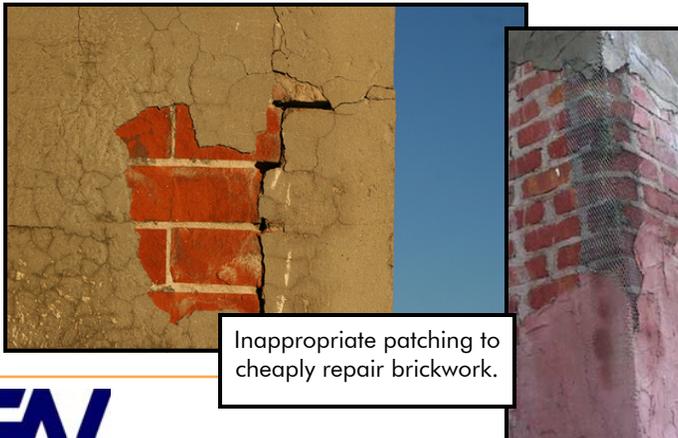


Storefront is lost.

Masonry -

Masonry is typically the preferred façade material for downtown. Most existing construction will utilize some masonry. In most instances metal and wood siding is not a comfortable choice for downtown building fabric. These types of siding provide harsh lines, stark contrast, and no relief or warmth to the buildings. If wood was the historic material, it may be restored.

- Masonry restoration, particularly on historic structures, should be done with great care.
- If the masonry has been painted or stained a minimally intrusive removal process should be used.
- Never resort to sand blasting as this will permanently damage the brick.
- Unpainted masonry should remain natural, not painted or sealed.
- Damaged masonry should be repaired or replaced with similar color, texture, and style masonry products.
- Tuck pointing should be done with an appropriate mortar material with a consistent color across the entire façade.
- Masonry replacement and/or repair should only be done with appropriate materials.
- Portland cement as a patch for masonry is unacceptable.
- If a historic façade has been covered with metal or wood siding it should be removed. Exposing the underlying brick will help re-establish the character of the building and contribute to the visual continuity of the block. Metal cladding often is easy to remove, and only small areas of the underlying material will have been damaged.
- Cover-ups also hide interesting details that can enhance building identity. If, after removing the covering material, portions of the original must be replaced, use a material that is similar to the original in color and texture.



Kennett, Missouri

Inappropriate building materials should be removed...these don't match.



Building has great potential and materials, but window unit AC should not be allowed.



Storefront maintained, although painted. Attention to upper façade zone needed.



Conflicting building materials and the discordant nature it creates.



Awnings -

Awnings used in the storefront zone provide shade for merchandise, shelter for pedestrians, and bring a colorful accent to the building front that can be changed frequently without great expense. The following suggestions are should enhance appropriate use of awnings and improve downtown aesthetics:

- Mount the top edge to align with the top of the transom, or to align with the framing that separates the transom from the main display window. This will help strengthen the visual continuity of store fronts.
- Roll-up awnings were a common site on historic storefronts and can be used following a similar approach to the original application. If a roll-up awning is not operable, the awning should at lease follow the shape of an operable awning.
- Like the storefront, awnings should be confined to the extent of the original storefront opening.
- Awnings should be trapezoidal in profile with closed ends, not rounded or curved, and a consistent color.
- Awning colors should coordinate with the color scheme for the entire building.
- Awning signage or lettering should be limited to the hanging vertical flap of the awning and be complementary in color to the building.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Signboards under the awning intended to assist pedestrians should be a limited, uniform size and complement the awning and building.
- Awnings will wear and should be acknowledged as an operating cost of doing business which can be changed every few years for a fresh look.
- Aluminum and/or steel awnings and structures are not original building elements and typical detract from the overall appeal of downtown façades. These awnings should be removed and points of attachment repaired on the building façade.



Kennett, Missouri



Awning installed too high.



Although in disrepair, good use of awning to locate rear entrance.



A community should never allow an awning in this condition. The meager bench and planters do not begin to make up for the condition of the awning.

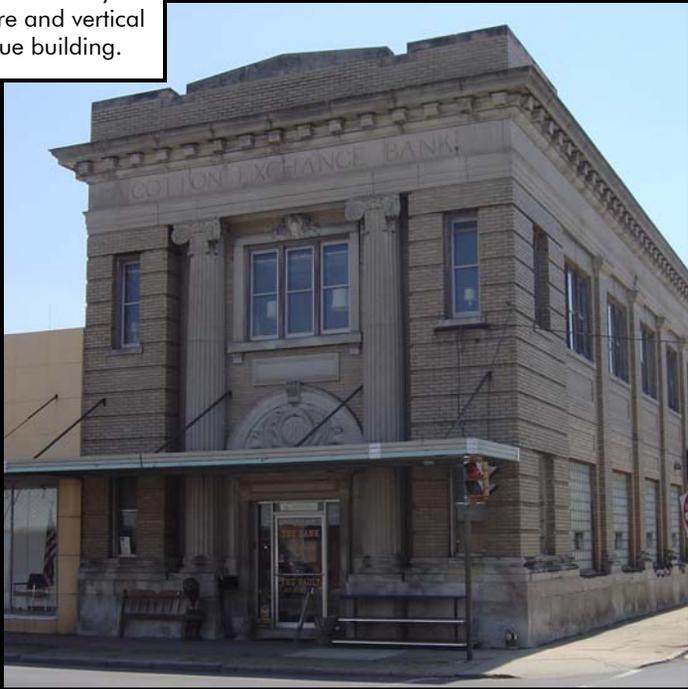
Awning use for upper façade zone.





Awning should be at the same level across the building front.

This awning hides the very detailed architecture and vertical lines of this unique building.



Upper Façade awnings disrupt the rhythm. Multiple awnings over each window would be preferable.

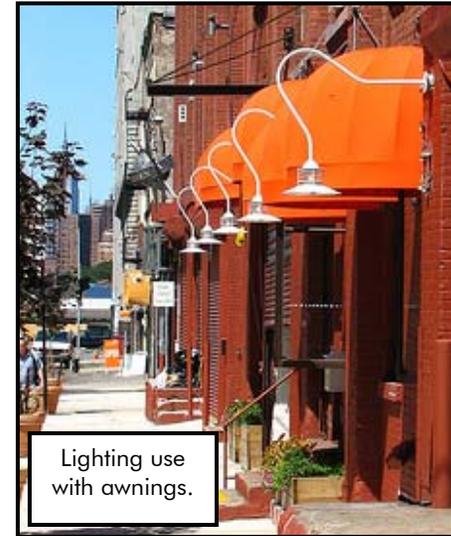


Attractive renovation, but lower awning not needed. It is installed too high and obstructs the sign band.

Lighting -

Buildings should be interesting to pedestrians and motorists at night, as well as by day. A well-lit storefront or rear façade creates a positive impression about downtown. The following lighting conditions can be implemented to enhance the impression and safety of the downtown:

- Use lighting as a design element to draw attention to the entire building, not just the sign.
- Any lighting at the storefront should be used to accent the entrance, signage, or architectural elements as well as provide light for safety and security.
- Light fixtures should be the lowest wattage possible and of a concealed, simple, and non-intrusive design or a style that is appropriate to the period of the building.
- Sign lighting should be balanced in color and intensity with light in display windows.
- Warm-colored light is preferred for all exterior lighting, since this is more pleasing to the eye, and will more easily draw attention to window displays.
- Neon lights and cool fluorescent lights should not be used.
- Lighting on Rear Facades should provide illumination at the entry door as well as along the pedestrian path from the parking area. This lighting should be similar to the lighting in the front.
- Building lighting, in particular up-lighting, should be coordinated with regulations set forth by LEED guidelines.



Lighting can be effectively and attractively combined with other façade elements to be functional and make the building more interesting. Care must be taken so that the lighting does not overwhelm the nature of the street and become garish or "too much" in relation to the other buildings.



Signage -

For a successful *Kennett Square District* each business must have its own identity while maintaining the integrity of the district. Proper signage will identify the business without detracting from the architecture of the building and the fabric of downtown. Sign types and their locations should be kept simple and consistent for ease of public awareness. Signage should be restricted to the Storefront or Rear Façade Zones. The following guidelines will help enhance this aspect of Kennett’s downtown:

- The size of the sign should be of an appropriate scale for the building and street. Large signs should not be needed as the signage in a downtown area is more oriented to the pedestrian than the motorist.
- Rooftop, blade, pole, abandoned, neon, electronic message boards, and billboard signage should not be allowed or severely restricted.
- Position flush-mounted signs that fit within architectural features are preferred. This type of signage will help reinforce horizontal lines along the street.
- Coordinate color schemes with the building front.
- Locate flush signs so they do not extend beyond the outer edges of the building front.
- Look to see if decorative moldings define a “sign panel” for flush-mounted signs.
- Locate projecting signs along the first floor level of the façade, not above.
- Place signs near the business entrance, to guide a customer’s eyes to the door.
- Use symbols in projecting signs; these are more easily identified and remembered and will add interest to the building.
- Where several businesses share a building, coordinate the signs by aligning several smaller signs or grouping them onto a single panel as a directory to make them easier to locate. Use similar forms or backgrounds for the signs to tie them together visually and make them easier to read.
- Mount signs so they will not obscure any architectural details.
- Sign materials should be compatible with the façade materials.

The amount and style of the signs conflict with the vintage streetscape light pole. Too many signs for one business.



- Good craftsmanship will pay off in longer service for your sign, and it will convey a stronger image to the public. Select high quality materials: Signs are exposed to extreme weather conditions, and a deteriorating sign presents a poor image to customers.
- Encourage the use of “custom” designs that portrays a business as being unique. Mass-produced signs, especially rectangular plastic panel ones with internal lighting, fail to make a lasting impression.
- Illuminate signs in such a way as to enhance the overall composition of the façade.
- External lighting cast from period style, non-intrusive fixtures is preferable to internal sign lighting.
- Awning signage or lettering should be limited to the hanging vertical flap of the awning and be complementary in color to the building.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Signboards under the awning intended to assist pedestrians should be a limited, uniform size and complement the awning and building.



Too many signs can clutter the view down the street and make any streetscape attempt useless.

Temporary banners, decaying signage, and an inoperable awning on what could be a nice, complementary storefront.



Bicycles -

A downtown area should not only be pedestrian friendly, but bicycle friendly as well. Concerns for a bicyclist should include routes of travel, clearance, type of traffic signals, traffic lanes, signage, drainage grate and curbing obstacles, and parking.

- Bicycle parking racks should be a uniform and integral component of the downtown furnishings.
- Racks should be located at useful activity nodes and on all four corners of the Courthouse square.
- Established bike routes within the vicinity should be linked to downtown.

Outdoor Café Seating -

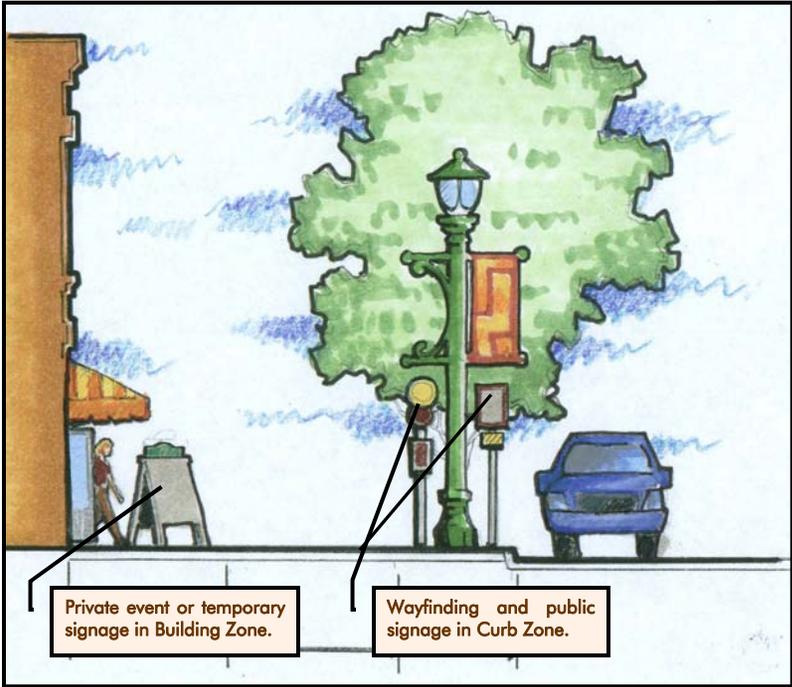
Outdoor Café or sidewalk seating is an exciting tool that adds to a vibrant impression about downtown. Seating areas for restaurants should be encouraged, but monitored by the City. A proper arrangement will:

- Be located in the sidewalk area fronting the restaurant.
- Allow a clear and unencumbered path along the sidewalk for pedestrian traffic or be located close enough to the building. In either case the sidewalk must maintain ADA compliance. The restaurant owner is responsible for keeping the sidewalk and this pathway clear at all times.
- Areas adjacent to the building should not block entrances or exits to the building.
- Provide a clearly defined area connected with the restaurant.
- Utilize appropriate umbrellas or other patron covering in a uniform color, matching the building colors, and with only the restaurant name. Any other wording or message should not be allowed.
- Temporary outdoor seating material must be kept in top condition to provide an attractive image for the restaurant and all of downtown. Such furnishings should be durable, weatherproof, and sturdy enough to prevent movement by winds.
- Plastic furnishings should not be used.
- Furnishings should be stored in a secure location inside at the end of the day.
- At least one sturdy trash receptacle should be provided.
- The use of exterior heaters for outside seating should be considered for extending the seasonal appeal of outdoor cafes and restaurants.

SIDEWALK ZONES: Appropriate zones in front of a building should be maintained. The Building Zone, Pedestrian Zone, and Curb Zone all have unique characteristics that should be regulated to ensure that private elements do not adversely impact public improvements. These are also important aspects of the streetscape plan to be discussed later in this document.



Aside from ADA accessible pavement improvements, Streetscape Amenities should remain clear of the Pedestrian Zone and allow for free movement of an upright pedestrian. These elements will attract pedestrians, but must not obstruct them.



Businesses must be educated on the importance of maintaining Sidewalk Zones. Each business should care for the zones within their building's street frontage.



Items such as bicycle racks, bollards, and benches have traditionally been City provide streetscape enhancements.

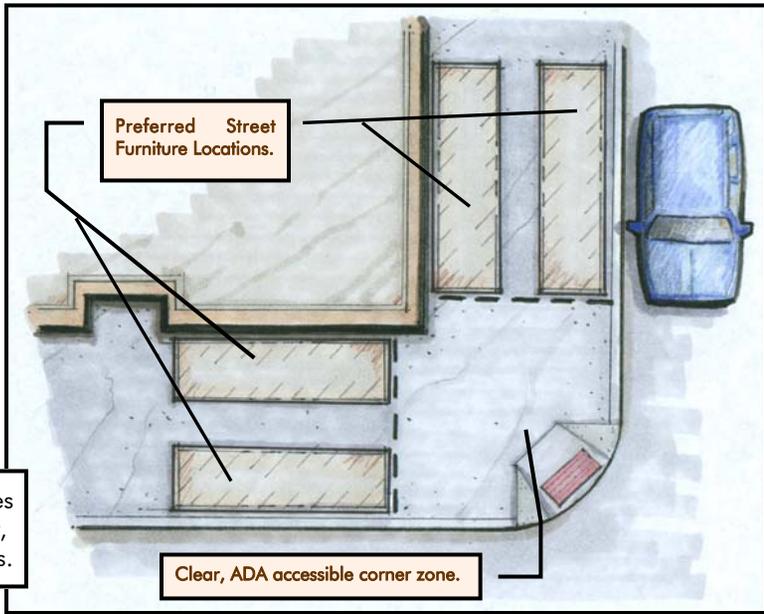
Businesses should consider providing and maintaining such elements within their Building or Parking Lot Zones to enhance service to their patrons.





Sturdy outdoor seating that will stand up to weather and wind is critical for a café.

Umbrellas should be a solid color or advertise the name of the café only, not products sold at the café.



Proper location of cafes is important for safety, success, and aesthetics.

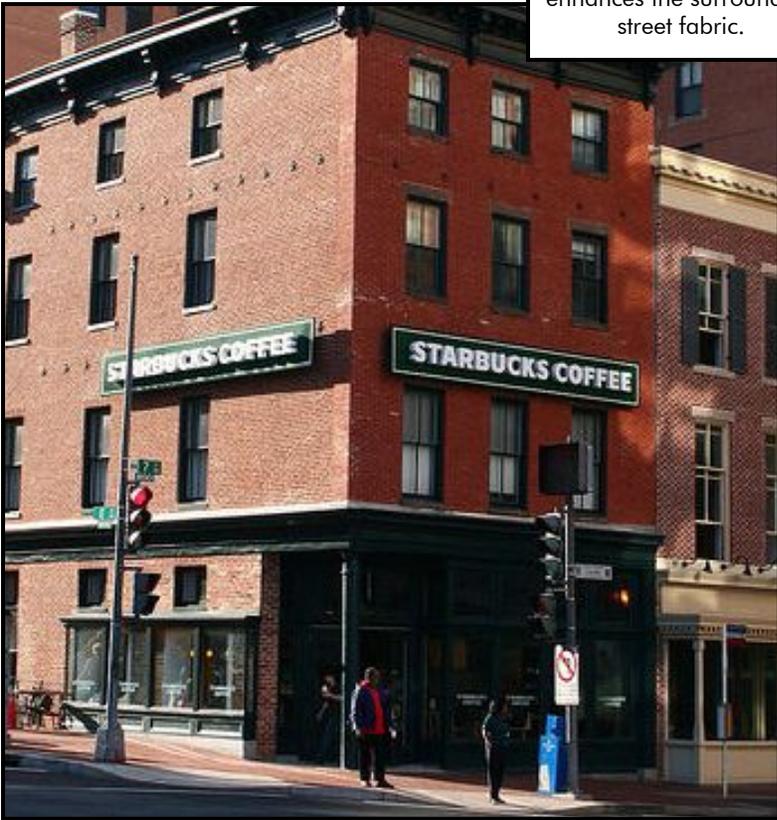
Franchise Architecture -

To establish a unique atmosphere in the *Kennett Square District*, branding the downtown buildings in the style of a company should not be allowed. Large franchises and national chains typically have a 'downtown style' in addition to their trademarked brand. Kennett should insist the company follow these design standards and adapt their brand to create a complementary downtown building.

A franchise façade that does not complement the surrounding street fabric.



A franchise façade that enhances the surrounding street fabric.



Maintenance of Facades -

Facades, particularly restorations, may need extra care and maintenance. The city should encourage proper maintenance by firm code and nuisance enforcement. In addition, if the city has implemented any sort of incentive for façade work a requirement should be proper maintenance according to city standards. An example would be the ability to call a revolving loan should the façade fall into disrepair.



Maintenance of all parts of a building is critical.



Intricate façade in poor condition.



Metal awning can be removed and replaced with canvas. Old sign either removed or repainted for nostalgia. Windows should be uncovered.

Historic Buildings

Original Elements -

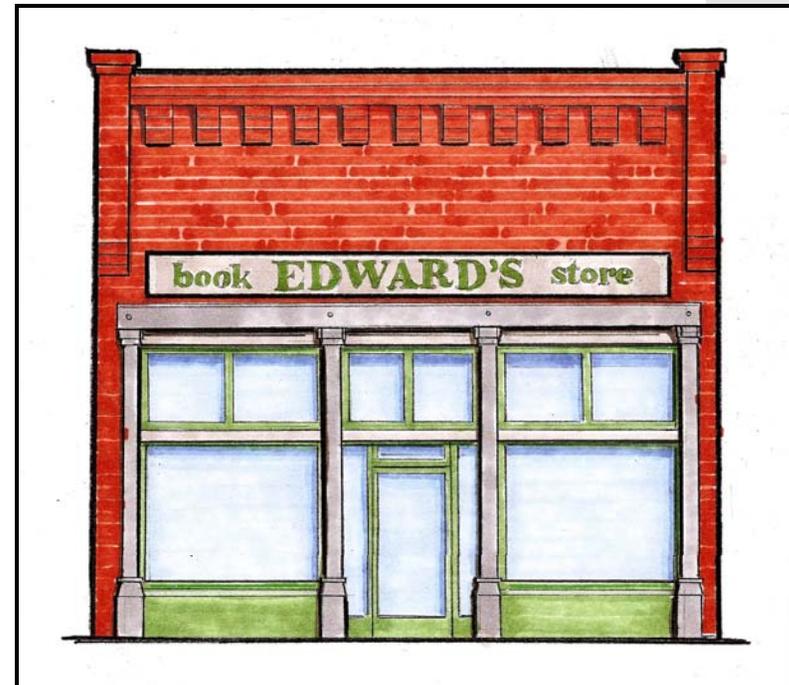
Any original element or material that still exists, particularly on the storefront, should be retained if possible. Original elements provide a historic value that can not be replaced. Prisms glass in transom windows or a decorative wooden door with beveled glass would be an examples of original materials.

Replacement of missing architectural elements should be based on accurate duplications of original features. When an entire detail must be reconstructed the new material should match the original in design, color, texture, and other visual qualities. Where reconstruction of an element is impossible because of a lack of historical evidence, then a new design that relates to the building in general size, scale and material may be considered. Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be acceptable.

Storefront -

Entrance:

- The entrance door should be recessed to emphasized the entry, provide a bit of shelter and remove the open door from the path of pedestrians on the sidewalk. These areas also repeat rhythm of shaded areas along the street helps to identify business entrances.
- If the original recessed entry has been removed, consider establishing a new one.
- The recessed entrance door should also be ADA compliant.
- The door should provide a view into the building as well as a sense of openness. Solid doors should be avoided.
- Consider using an accent color on the door.



Storefront Continued -

Windows:

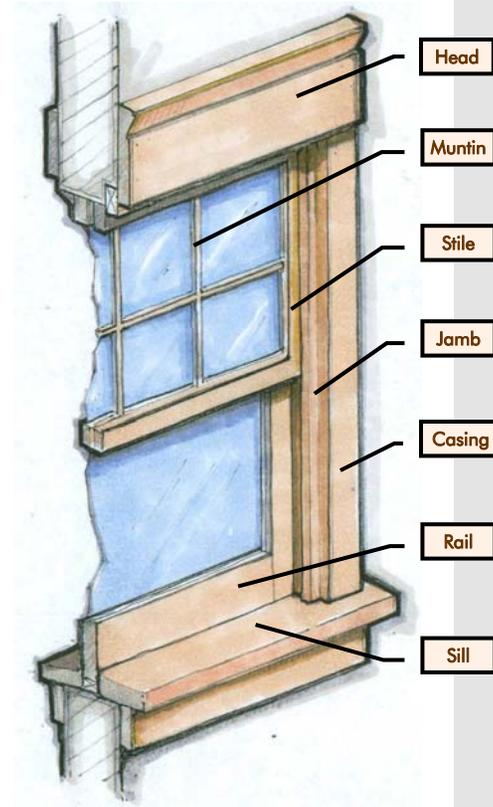
- Preserve any of the large panes of glass that make-up the original storefront if they still exist. These transparent surfaces allow pedestrians to see goods and activities inside.
- Any new or replacement storefront should be built of similar materials compatible with the original façade design and craftsmanship.
- Wood framing similar to the original is preferred but metal framing with the appropriate historic profile is acceptable.
- Clear insulated glass with low 'E' coating is a good choice for replacement storefronts.
- Tinted or reflective glass and interior reflective films should not be used on the storefront.

Never allow window unit AC in front facade. Only in rear.



Spandrel Panels:

- Maintaining the original spandrel panel if it exist is preferable but if the panel is missing, reconstruction using old photographs as a guide is acceptable.
- Coordinate the color scheme of the spandrel panel with other façade elements.
- If original design information is not available, another option is to design a simplified panel using appropriate materials such as painted wood or metal.



Storefront Continued -

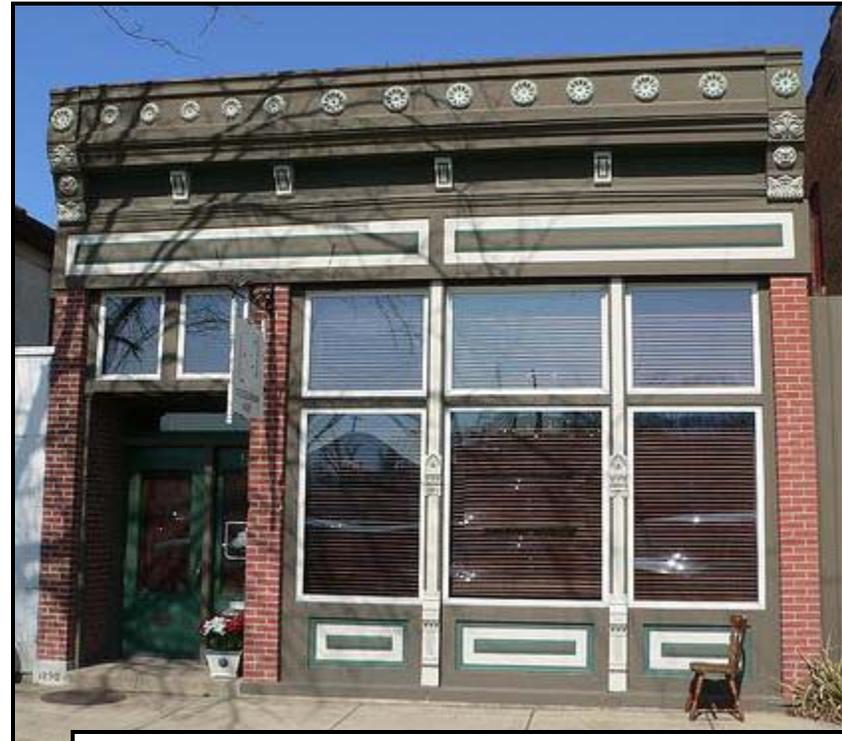
Transoms:

- These bands of glass are found on many buildings and they often align at the same height in a block. Maintaining this line will help to reinforce a sense of visual continuity for the street.
- When transoms are covered and original moldings and window frame proportions are concealed, the impact of the storefront is weakened. If the interior ceiling is now lower than this glass line, move the dropped ceiling back from the window to maintain its historical dimensions.
- Some transoms have hinged panels to allow natural ventilation. Restore these to working order where feasible. Used in combination with ceiling fans these operable transoms can be very effective in improving comfort levels when full air-conditioning is not as necessary.

Upper Facade -

Windows:

- Typical upper windows are vertically oriented and uniformly spaced across the building front. This rhythm of upper story windows is an important unifying feature of downtown, because it is repeated on most buildings.
- Any window opening covers, masonry infill or mismatched windows should be removed.
- If the original window still exists, it should be restored to serviceable condition when possible.
- Do not allow Window AC units. If they must be used, use only on the rear façade.



An example of a Mesker Brothers Iron Works façade with intricate details well restored and maintained. Mesker Buildings can be found throughout Missouri. Kennett should research www.gotmesker.com

Upper Facade Continued -

Windows:

- Replace only missing portions of original elements where feasible. Sometimes trim elements and other materials must be removed in order to repair or refinish them. Always devise methods of replacing the disassembled materials in their original configuration. Code trim pieces, for example, so you can replace them accurately.
- Installation of interior storm windows should be considered.
- Keep window unit AC in the rear façade, not in front.
- If the existing window is beyond repair an appropriate replacement window of the same size and profile should be installed.
- Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be considered.
- Window shades or curtains in colors that coordinate with accent trim should be encouraged.
- If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the window.

Cornice and Architectural Details:

- Replacement of missing cornices or architectural elements should be based on accurate duplications of original features. In some cases, an entire detail must be reconstructed. In the event replacement is necessary, the new material should match the original in design, color, texture, and other visual qualities. Photographic evidence is a good source for research.
- If the cornice is missing, a similar cornice of like size and scale should be installed.
- If no evidence exists as to form and detail, the reconstructed cornice should be as simple and non-intrusive as possible.
- If the cornice is intact it should be repaired and maintained as required.
- Where architectural details have been removed, look at photos for details to use as patterns for new designs.
- Where suitable reference materials are not available, consider basing the cornice details on adjacent buildings.
- Where exact reconstruction of details is not feasible, consider developing a simplified interpretation of the original, in which its major form and line is retained.

Rear Façade -

The rear facade typically faces an alley and provides access for deliveries and pick-up. In some cases customer parking is provided behind a building and entry to the business through the rear elevation is desirable. Attention to the appearance of the rear elevation can be extremely important to the quality of the customers' shopping experience. Consider how image can be improved here, while accommodating service functions. If window AC units must be used, this is the only façade acceptable.

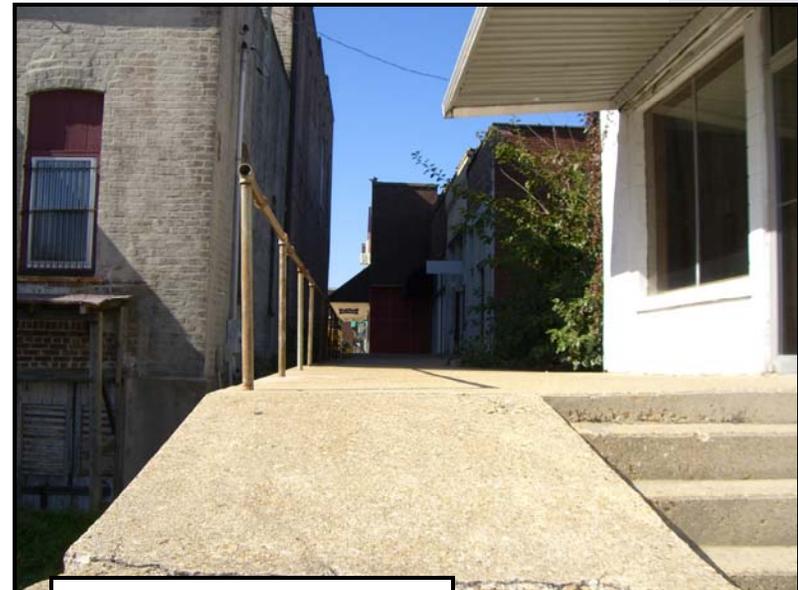
Entry Door:

- The rear door will no longer just be for service but should project a sense of openness and welcome.
- Customers might also feel a loyalty or sense of 'special access' by using this door and the business can build on this loyalty by catering to that customer and improving that experience.
- A new door and hardware with a large area of glass may be considered.
- A small canopy or awning may provide some of shelter.

Upper Rear Façade:

The rear upper facade elements should be treated similar to the front. Too often this is a façade that is neglected and allowed to deteriorate.

- Windows should be restored or replaced and have no covering except an awning.
- Gutters and downspouts should be in good repair and painted.
- Use materials and colors that coordinate with the main façade so customers will learn to recognize both entrances are related to the same business.
- Use a smaller version of the front sign to identify this entrance.
- Exit stairs and balconies can enhance the marketability of second story space, especially when these lead out onto parking lots located on the back side of the building. Encourage installing new stairs that comply with current building codes.



An example of an uninviting rear entrance from a parking area.

Fences:

- Fences should be designed to harmonize with the surrounding structures in both scale and color.
- Some materials which may be appropriate include masonry, wood and wrought-iron.
- Chain-link should not be a permitted material.

Trash, Ancillary Structures, & Utilities:

Sensible, yet firm enforcement of the city's building and nuisance codes will be required and should be a priority throughout the *Kennett Square District*.

- Trash containers should be placed in an enclosure or behind a screen.
- Central trash collection locations should be considered to minimize the quantity of enclosures.
- Recycling collection centers should also be considered in downtown as residential and retail use increases.
- Enclosures and screens should harmonize with the surrounding buildings in scale and color.
- Landscaping can also be used to screen air-conditioning condensers and utility transformers. All landscaping for such use should be of hardy plant goods with relatively low maintenance needs.
- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Any ancillary structures should match the surrounding buildings style and scale. These structures must be maintained well.
- Keep electrical service boxes and conduits in good repair and painted.
- Encourage using a color scheme on these screens that matches that of the rest of the building.



Regulations for refuse haulers can be established and enforced to prevent situations like this.

Existing Buildings and New Construction

Some buildings in the *Kennett Square District* do not have historic features or ornamentation. Many were built as simple fronts. These buildings and any new construction should implement the following standards:

- New construction should develop a design using the traditional storefront elements described in these guidelines or on nearby historic buildings that contributes to the fabric of downtown.
- Use a simple design, complementary to the downtown, with these three basic elements; a unified paint and color scheme, an awning, and non-intrusive signage.
- Emphasize horizontal features that can align with other buildings.
- Encourage highlighting a simple cornice, a band of color, a sign panel or an awning edge that can line up with similar elements on the street nearby.
- Some newer buildings downtown are set back from the street, with space in front for parking. This should not be allowed to continue. This building situation relates more to cars than pedestrians. Landscaping elements can enhance the existing sites.

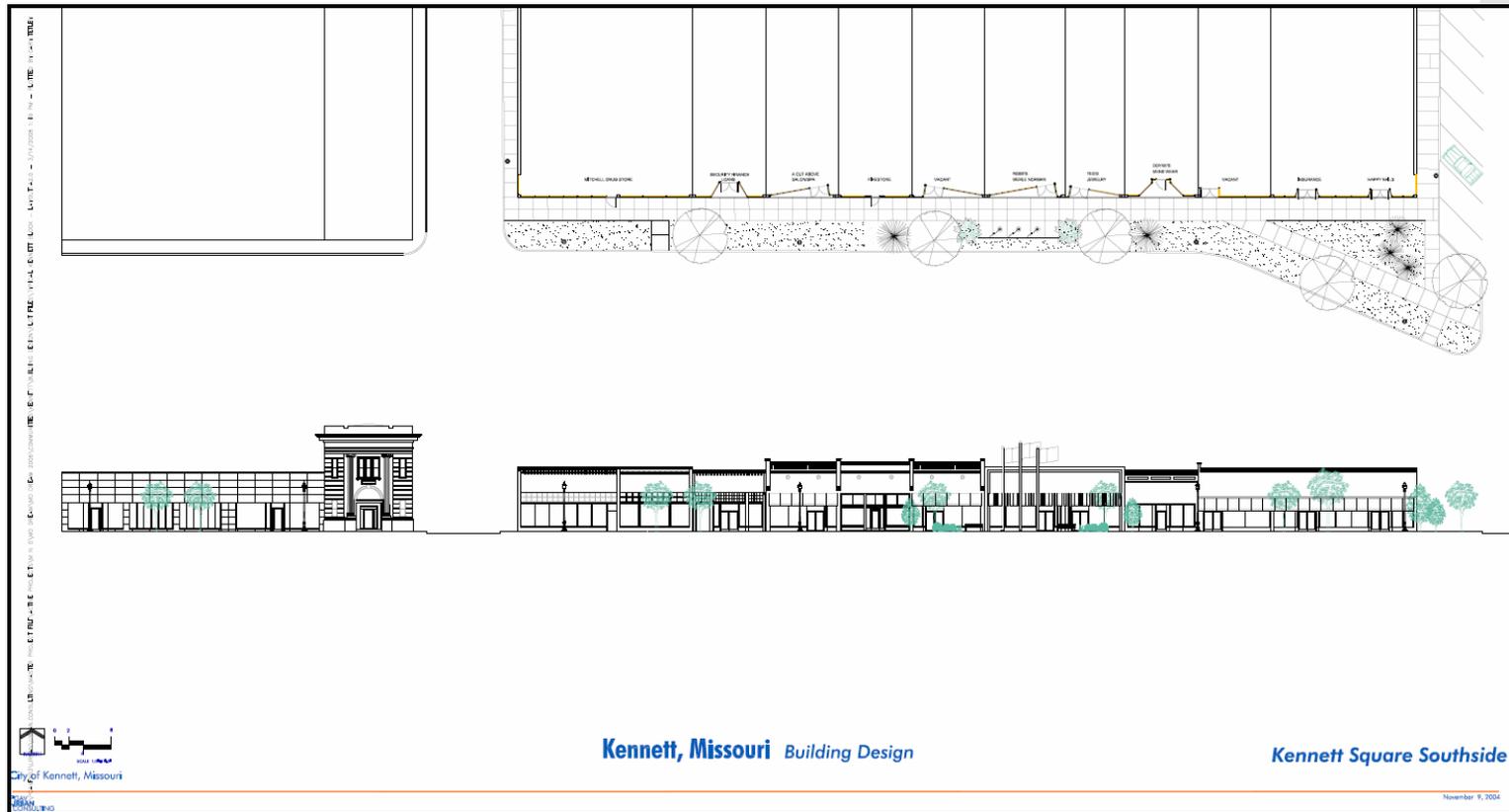
Color Guidelines

Use color to your advantage. Some of the most noticeable results are achieved with a fresh paint job. The most effective and economical schemes often start with the natural colors of the building materials themselves as a base, such as the native red of many brick buildings. The following techniques should be encouraged:

- Use only one base color for the majority of the background wall surface, but use a different color for accents. Do not paint a building entirely one color.
- Base colors should be muted earth tones or pastels.
- Look for “built-in” features of the façade that can be highlighted with an accent color.
- Window frames, sills, moldings, and cornices are potential elements to dramatize with a contrasting color.
- Use bright colors only in small amounts. Place them at the first floor level to direct the customer’s eyes to the business.
- Consider accent colors for signs, awnings, and entrance doors.
- Earth tones will hold their color well, as will darker pastels. Check for color stability in ultra-violet light; some colors, such as red, tend to be unstable and will shift in hue over time.

BUILDING DESIGN EXAMPLES

This report focuses on revitalizing the South side of First Street from Kennett Street on the East to Main Street on the West. The concepts presented for this block can be used on all of the blocks surrounding the Courthouse. All of the buildings on the study block are one-story



The metal canopies are to be removed for a more open feel to the street. With the enhancements and added green space from the streetscape plans, First Street could allow for outdoor dining areas, making some of the current vacancies ideal for restaurants, cafes, or coffee shops.

The following pages discuss changes to the buildings housing Mitchell Drug Store, Security Finance, A cut Above Salon, Firestone Tires, Rose's Boutique, Ted's Jewelry, and Doyne's Men's Wear, and various other vacancies in this block.

Mitchell Drug Store / Security Finance

This store appears to be the old Kohn's Men's Wear building. The aluminum siding on the upper façade should be removed to expose the original brick which may require some repair or restoration. This may also expose a row of windows above the existing storefront. Ideally the original storefront configuration will be restored following these design guidelines.

The removal of the aluminum siding on the Security Finance building may reveal the historic Blakemore's Grocery and should also be restored to its original configuration.



Cornice should be uncovered or recreated if possible. If no cornice remains, a simple unobtrusive design will complement the building.

Architectural details exposed and restored.

Metal siding or other inappropriate materials should be removed to expose the original façade.



Repairs and restoration to lintels and columns maintain the integrity of the façade.

Transom windows exposed and restored.

A Cut Above / Firestone / Vacant

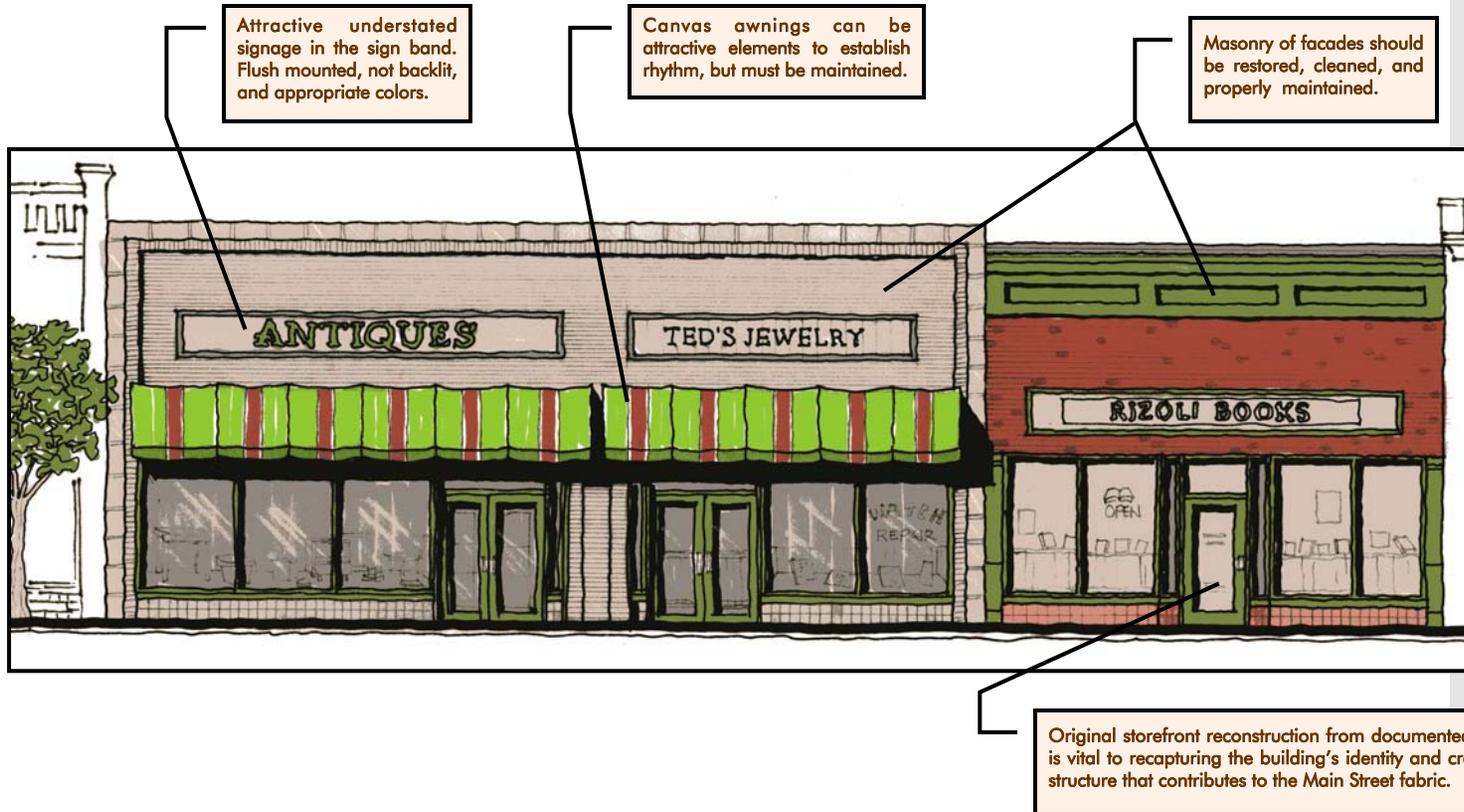
Based on historic photographs, these buildings appear to be part of the same brick building and that look should be restored. Removal of the paint from the masonry with a gentle method is recommended. The salon portion may require rebuilding some of the upper façade and cornice. The Firestone Store retains its original storefront configuration, but has an aluminum panel above the windows which should be removed.



Rose's Boutique / Ted's Jewelry / Doyne's

This building appears to be more recently added, but retains the proper scale and portion for the area. Removal of signage and cleaning of the façade will help this building integrate with the block.

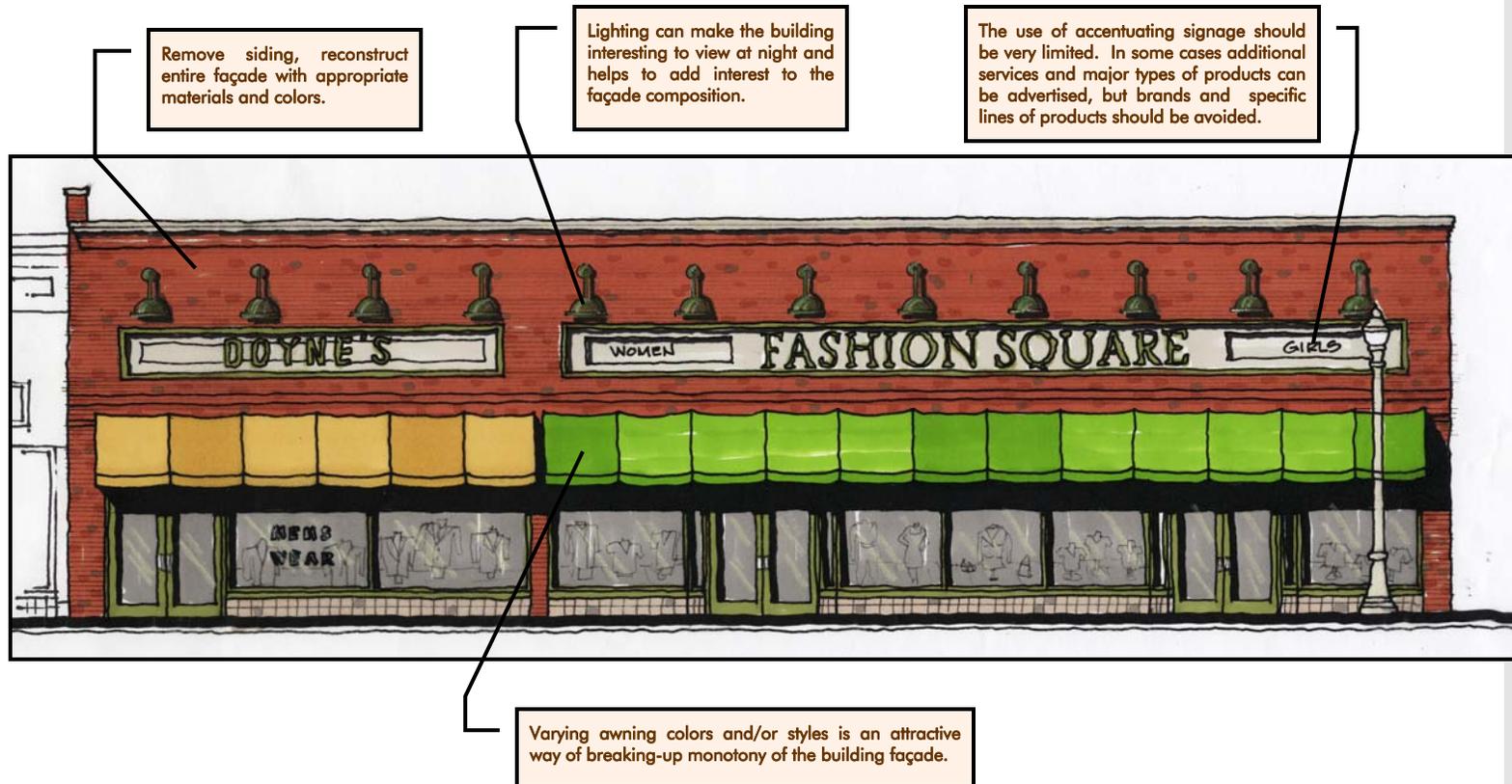
Doyne's appears to be the old Wheelers Cash Grocery. The aluminum siding should be removed and the masonry restored. The original storefront could be recreated based on photographs.



Vacant / Insurance / Nail Salon

The nail salon and vacant shop appears to be a newer addition to the block. Removal of the upper façade siding should occur. If there is no masonry underneath, another appropriate material should be used in a complementary scale and style.

Although the drawing depicts current uses, there are others choices that would more appropriately benefit from the added landscaping and streetscape improvements, such as retail food shops or cafes. This corner could be a very vibrant social gathering place for Kennett and would merit bold improvements like the exciting lighting array.



STREETSCAPE DESIGN GUIDELINES

The term Streetscape refers to exterior public spaces located between the building facades on one side of the street and the building facades on the other side. An organized streetscape with combined lighting and way-finding signage is more efficient and user-friendly for visitors to the ***Kennett Square District***. The existing and recently installed features of lighting, landscaping, and sidewalk and street projects centered around the Sheryl Crow Aquatic Center and a recent expansion of those elements serve as the beginning of an emerging streetscape for Kennett.

Three distinct geographical areas include the Kennett Square, the Sheryl Crow Aquatic Center, and the First Street Approach into the Kennett Square. These areas have the most potential and effect on visitors.

Kennett Square:

One of the problems with the current square is the abundance of hard paved surfaces and the lack of green space and trees. This situation can be improved by reworking the current parking areas around the Courthouse. This would also likely require additional parking at city owned lots and on adjacent side streets. The result would be an aesthetic view of the square and be more respectful of the beautiful Courthouse building.

Starting on First Street, this proposal would eliminate all curb side parking from N. Main Street to College Avenue on both sides of the street. This will allow for the expansion of a strip of green space running the entire length of the south block incorporating the triangle shaped piece of land at First & Main Streets. This will not only provide additional landscaping on the square, but will also allow room for outdoor dining and seating. The large traffic signal fixture needs definite improvement and this area should be a focus for the streetscape in the square area. The removal of parking should also improve the movement of traffic along First Street (Hwy 84).

Then, along College Street, between First and N. Court Square, the angled curb side parking would be changed to double loaded perpendicular parking and the double row of angled parking next to the Courthouse would be eliminated. That would allow for the expansion of green space from the Courthouse to the edge of the parking area. College Street would remain one way heading north.

N. Court Square would remain unchanged with the exception of additional landscaping on islands at both ends of the block. This will also likely slow down the one-way traffic as this will be the main parking area on the square.

Continuing on N. Main Street, between N. Court Square and First Streets, the angled curb side parking would be changed to double loaded perpendicular parking and the double row of angled parking next to the Courthouse would again be eliminated. This would allow for the expansion of green space from the Courthouse to the edge of the parking area. N. Main Street would remain one-way heading south.

Along S. Main Street, between First and Second Streets, the parallel parking on the east side of the street would be changed to angled parking.

There are currently approximately 106 parking spaces around the square. With these proposed changes there will be approximately 86 spaces with an additional 16 spaces one block away. This is a loss of only 4 parking spaces and the addition of the green space will be invaluable in creating a better atmosphere for the square. The benefit to the parking changes may require constant education for the merchants and the county employees. The parking lot at the Northwest corner of Kennett and Second Streets has approximately 48 parking spaces, the lot

on Main Street just south of Spence Avenue has over 30 parking spaces, and the lot at the Southeast corner of College Street and Cedar Street has 20 spaces. In addition the parking lot at the Sheryl Crow Aquatic Center has approximately 48 parking spaces. This is a total of approximately 250 parking spaces within the ***Kennett Square District***, not more than a block away from the Courthouse. There is ample parking in Downtown Kennett at this time.

One of the potential projects to help give the square identity is the compass rose located at the intersection of First and College Ave / Kennett Streets. During the DREAM Focus Groups several participants remarked upon Kennett being a crossroads and related its location by noting various distances to several major metropolitan areas. This can be an effective branding effort.

Another possible project is the idea of the cotton boll sculpture at the south east corner of the Courthouse property. Upon entering the square the most prominent feature is the Courthouse and the eye would be drawn to this corner, making it an ideal location to reinforce the brand and name of Kennett. Nearby is the veterans memorial that would be enhance by providing the area a park setting. The installation of inviting street furnishings and an informational kiosk will encourage the traveler to look for a place to park.

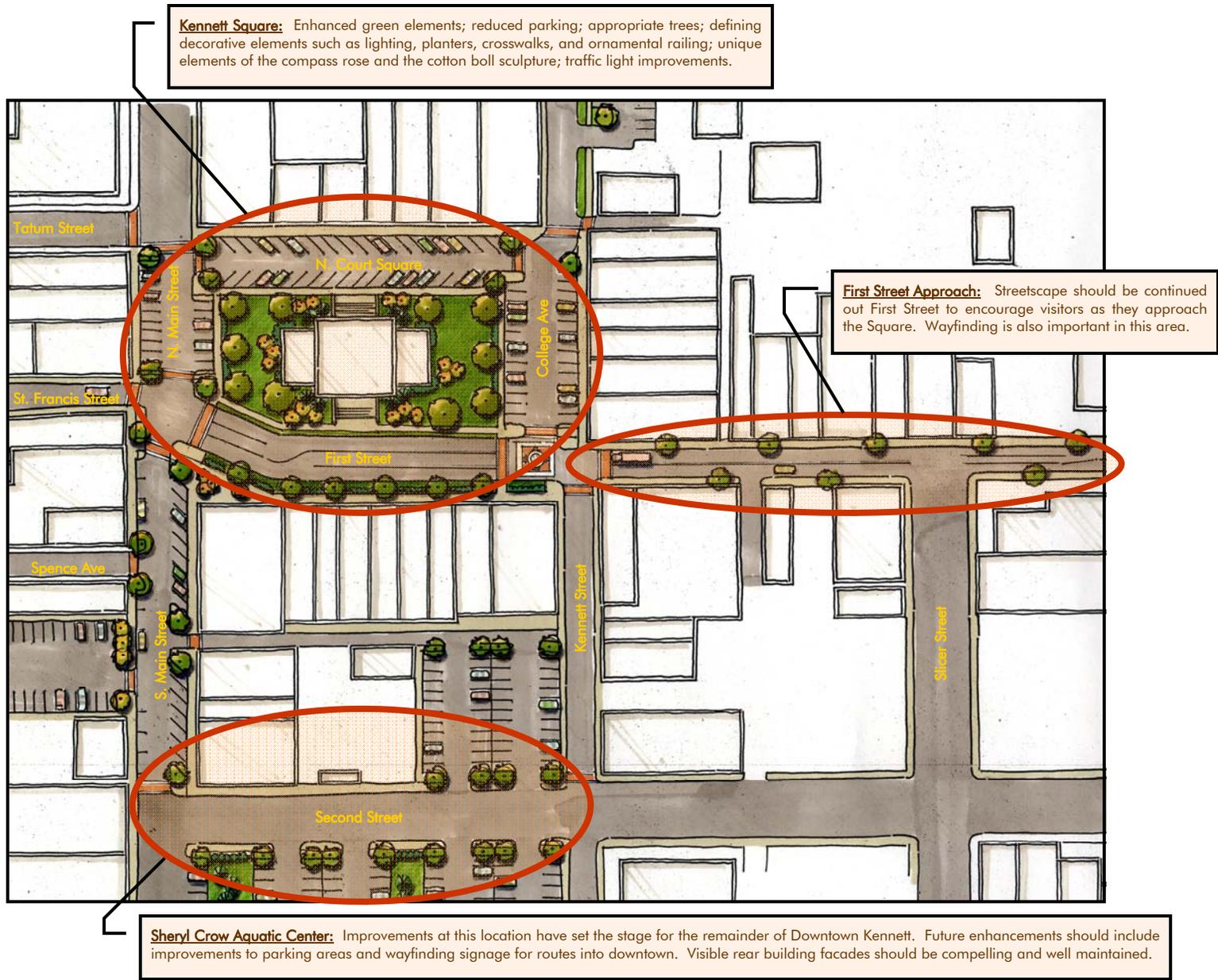
First Street Approach:

Along First Street to the East is the main entryway into the square. This corridor needs attention and a similar theme as the square. As discussed in the Wayfinding section on page 52, attention should be on wayfinding as there will be a need for some traffic to be routed toward the Aquatic Center.

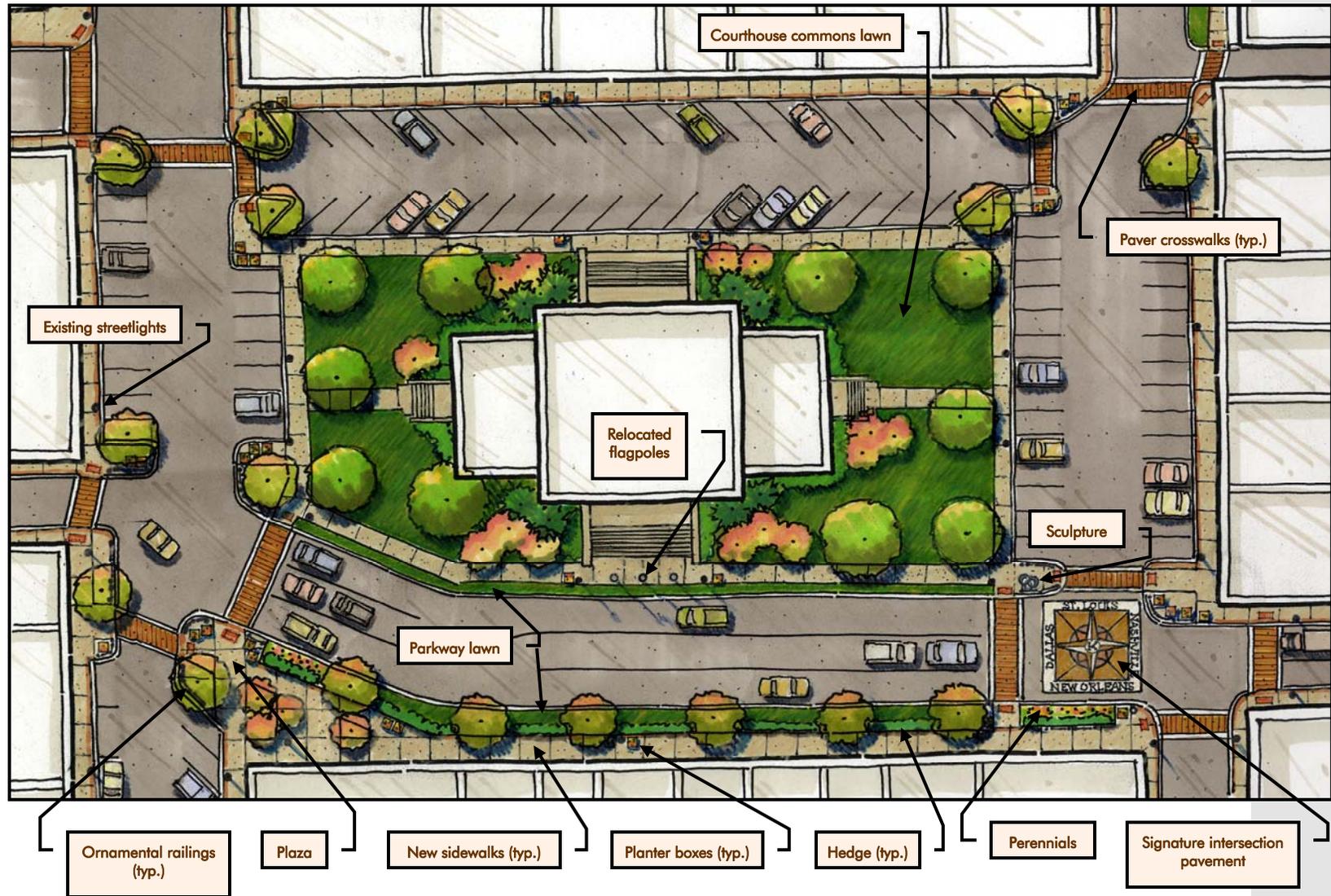
Another important aspect to this approach corridor is business sign and building code enforcement. The image this area presents will help to keep people in the square longer and to walk more. If the visitor has the impression that the **ONLY** warm and inviting area is around the Courthouse, they will not want to wander for a parking spot nor will they seek out other businesses. The approach will be critical to establishing the atmosphere downtown.

Sheryl Crow Aquatic Center:

Also discussed in the Wayfinding section, the goal of improvements to this area is to capitalize upon the visitors attending the various functions at the Aquatic Center. As critical as the vehicular approach described above, will be the pedestrian attraction to the square as the Aquatic Center sits a block south. A pedestrian will need the reassurances of a safe and direct walk to an area of greater activity. This will mean signage, lighting, good parking lot conditions, and nice buildings.



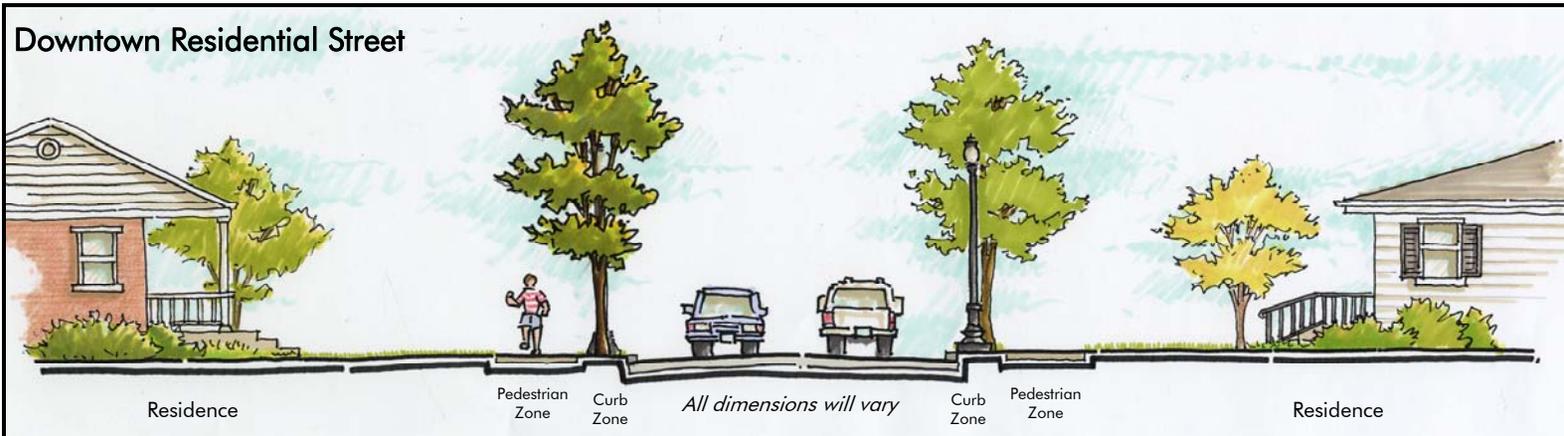
A more detailed view of the proposed elements and changes for the *Kennett Square District*, is seen below.



A representative cross-section of commercial-oriented streets in the DREAM boundary are indicated below. As the City seeks to extend streetscape improvements out Saint Francis, N. Main Street, S. Main Street, College Avenue, Kennett Street, and 1st Street, consideration could be given to a long-term plan for 2nd, 3rd, 4th, Slicer, Depot, Cedar, Washington, and Jackson.



A representative cross-section of residential-oriented streets in the DREAM boundary is indicated below. Streets within the DREAM boundary that this might be representative of include Harvey, Tatum, and Harrison streets. In addition, this could also be representative of the immediate streets around the downtown where the residential has not yet converted to commercial uses.



Design Coordination

An overall design approach to the *Kennett Square District* will result in a successful project. A district can display a sense of order and rhythm through the repetition of design elements on buildings and street furnishings. A sense of arrival should exist upon entering the *Kennett Square District*.

- The district should have well lighted and maintained streets and walks.
- Particular care should be devoted to the main entries and corridors as these will be the first impressions of the downtown area.
- Good, clear signage provides an invitation to enter and spend some time.
- The themes of the recent Streetscape work around the Aquatic Center should be maintained and expanded to include other streets.

Infrastructure

- Overhead electrical and telephone lines may pose a visual distraction from the overall unity of entire *Kennett Square District*. Although costly, relocation of overhead utilities should be considered.
- Curbs should be in good repair and a consistent material along the street. There should be no gaps or areas of uneven elevation along the curb line. At all street intersections there should be ADA compliant curb cuts.
- Poorly working or non-existent storm drains can create an undesirable situation at street intersections when runoff water collects in large pools. This condition makes pedestrian access virtually impossible and must be corrected.
- A public works project should be considered in connection with any other work such as sidewalk, curbs or street pavement to minimize street closings and maximize cost savings through economies of scale.

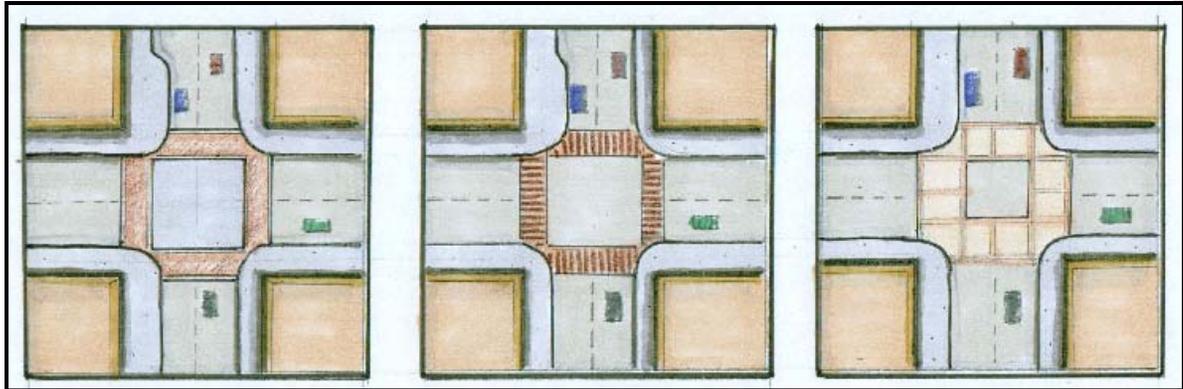
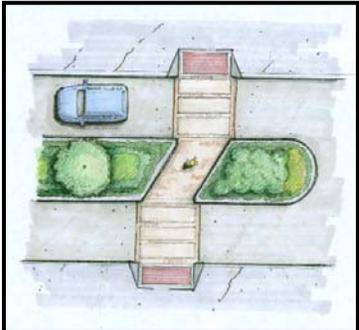
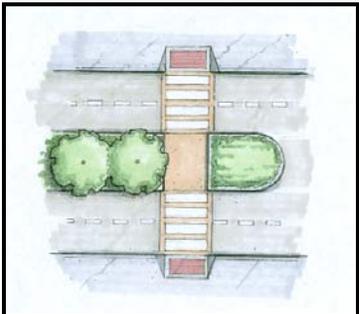


Pedestrian Access and Sidewalks

- A clean, clear and well lit pathway for pedestrians should be provided from any remote parking area to the **Kennett Square District**. This pathway will also need to comply with Federal ADA Accessibility Guidelines.
- Sidewalks should run continuously through an entire block to create a clearly defined pedestrian pathway and minimizing conflicts between people and vehicles.
- All roadway crosswalks should be clearly marked with signage and striping.
- The crosswalk itself can become a downtown design element through the use of decorative pavement or concrete stamping. Such installations require maintenance but are an effective way of providing beautification and safety features.



Example of current sidewalk view in Kennett.

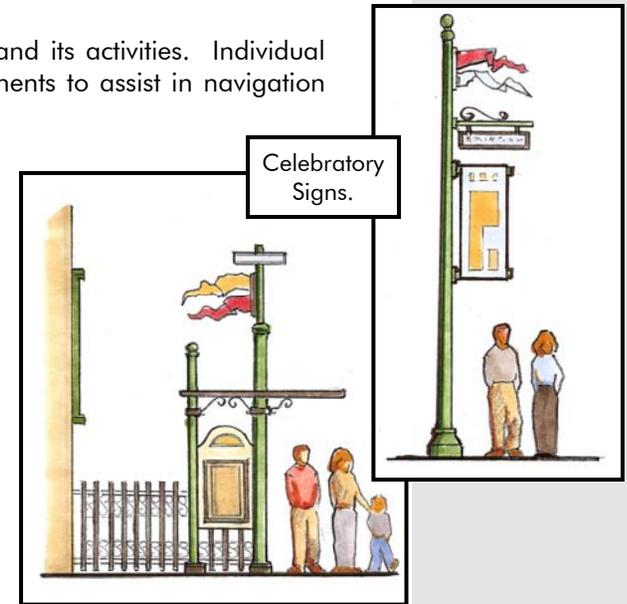


Suggestions for sidewalk alignment and designation.

Signs and Banners

This section is concerned with signage that will identify, define and promote the downtown area and its activities. Individual building and business signage was discussed on page 16 and wayfinding techniques and components to assist in navigation through the area will be addressed in a later section.

- Street name signs should be chosen and installed that are distinctively different from the street name signs located in the rest of the community. This will give a feeling of 'place' to the downtown.
- Street address signs should be uniform throughout the downtown area and prominently displayed. Merchants can be encouraged to adopt the same type of sign.
- The style of the street name and street address signs should complement if not match completely. A 'historic plaque' type can provide a very elegant touch. The style, font, and colors of these signs should be easily read.
- Banners and other temporary signs should be allowed, but restricted as to size, prevalence, and length of display.
- Hung from the light poles, seasonal banners or decorations approved by the City can tie the downtown area together.
- Banners can also add a sense of excitement while providing information about upcoming events or festivals.
- Banners should be well designed and are most effective with a simple design, repeated throughout downtown, minimal lettering, and no sponsor panels.
- The banner brackets used for these banners should be maintained by the City and only for use by the City.
- Banners should be changed on a regular schedule and replaced as needed.
- Balloons, pennants, and other distracting sign novelties should not be allowed in the downtown area.
- Murals must have an artistic component and should be allowable by City approval only.



Wayfinding kiosks can be an easy to spot element of the downtown pedestrian system.

Parking and Service Areas

- Adequate parking to support business and retail tenants must be provided. Street parking will accommodate some but not all of the required parking spaces.
- Nearby well lit and landscaped parking lots on previously vacant property is a good solution for additional parking spaces. Parking lots are the chance for a downtown to make a lasting impression.
- Provide planting buffers at the edges of parking lots or use decorative paving to define the site border.
- Include landscape islands throughout the lot. This will improve the aesthetics as well as the stormwater percolation.
- Side or rear locations off the main street are preferred for parking lots.
- A clear and well lit pathway for pedestrians from any parking area in the *Kennett Square District* should be provided.
- The street and alley and sidewalk pavement should be in very good condition with no tripping hazards for pedestrians.
- Crosswalks should be clearly marked and free of landscaping and other obstacles to provide a clear view for traffic.
- Care must also be taken that lots are policed in the evening as they will tend to become a gathering place for youth. This, in and of itself is not a bad thing, but it will make any visitors or evening patrons nervous about using the lot.

Downtown Merchants may need to be educated that any negative effect from loss of parking due to streetscape enhancements will easily be overcome by added activity due to the atmosphere provided by the enhancements.

Parking Meters

Parking meters can contribute to the overall sense of clutter and so coordination with other elements should be considered. The economic benefits of revenue from parking meters is often offset by the costs to enforce the meter limits and the unseen costs of discouraging customers from parking in downtown. Meters are not recommended, but should they be considered, they should follow the guidelines below:

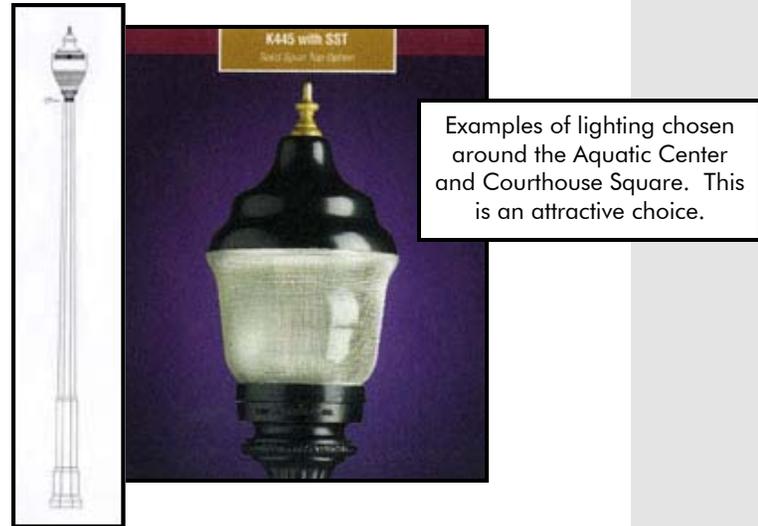
- Meter poles should be of the same design as light and signage poles.
- To reduce the number of poles, two adjacent parking spots should share one pole.



Lighting

Lighting of the *Kennett Square District* should provide the minimum illumination required by the Missouri Department of Transportation for the road surface.

- The sidewalks should be provided with pools of light at a higher level of illumination than the roadway.
- Storefront lighting can add to the pedestrian walkway illumination.
- Street lighting should be on pedestrian height poles and project light down onto the sidewalk not out into second floor windows.
- Lighting should be uniform in style, type, height, and brightness throughout the area.
- The antique replica light poles are not preferred unless it can be shown that they previously existed, however, the style used in the current streetscape plans should be continued as a theme throughout the downtown.
- Lighting poles with brackets for banners or electrical outlets can effectively display temporary or seasonal City approved decorations.
- An overall lighting design strategy should be developed to ensure appropriate lighting levels.
- The lighting plan should not neglect parking areas, rear entrances, and alleys.



Examples of lighting chosen around the Aquatic Center and Courthouse Square. This is an attractive choice.



Complementary vintage lighting can replace utilitarian 'cobra head' lighting and still provide good illumination.

Site Furnishings

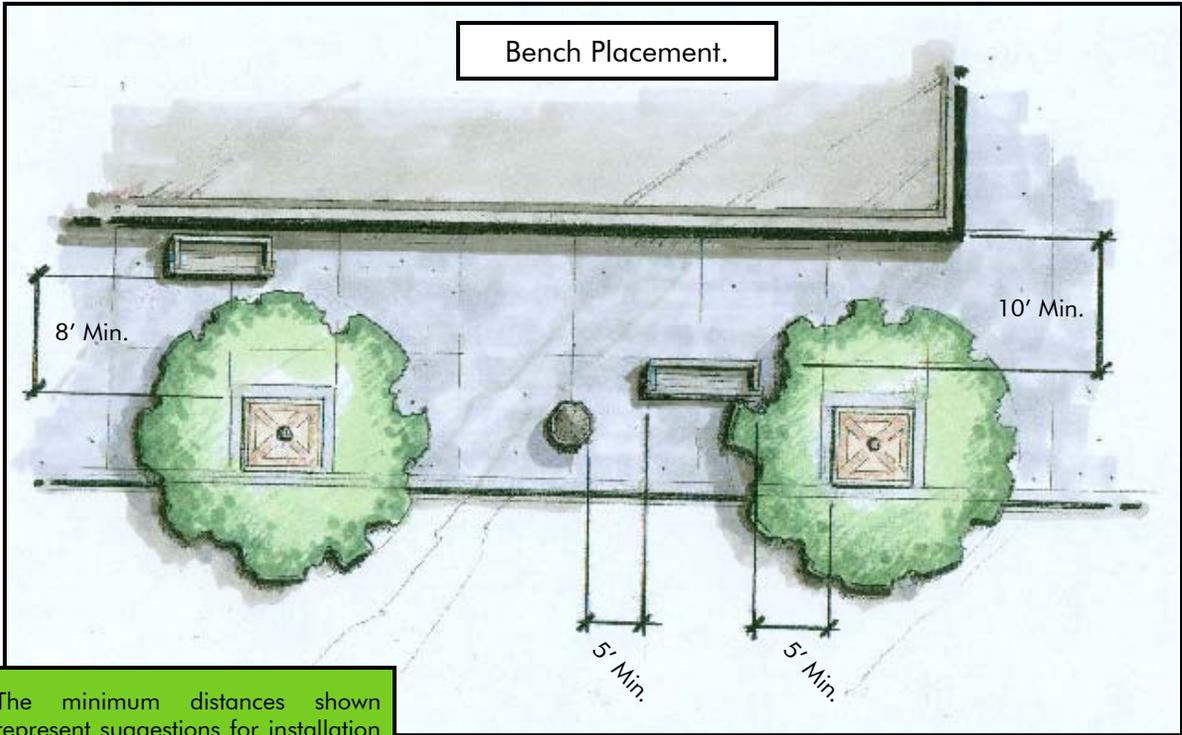
- Benches within the streetscape encourage social interaction which contributes to a successful neighborhood.
- Planters and window boxes provide color and can be a volunteer rallying point if maintained by a local club or organization.
- Public art and sculpture can provide an inspirational atmosphere in which people enjoy lingering.

Site Furnishing Continued -

- Trash receptacles provide a place to dispose of potential litter.
- Grouped together, such furnishings will enhance the downtown and provide a gathering place for pedestrians.
- Furnishings should be coordinated with light and sign poles to present a unified look to the streetscape.



Street furnishings will invite people to get out of their cars, walk around, and linger in downtown Kennett.



The minimum distances shown represent suggestions for installation placement. Actual distances may vary due to site conditions.

These existing planters look out of place and are likely not sturdy enough to last any significant amount of time.



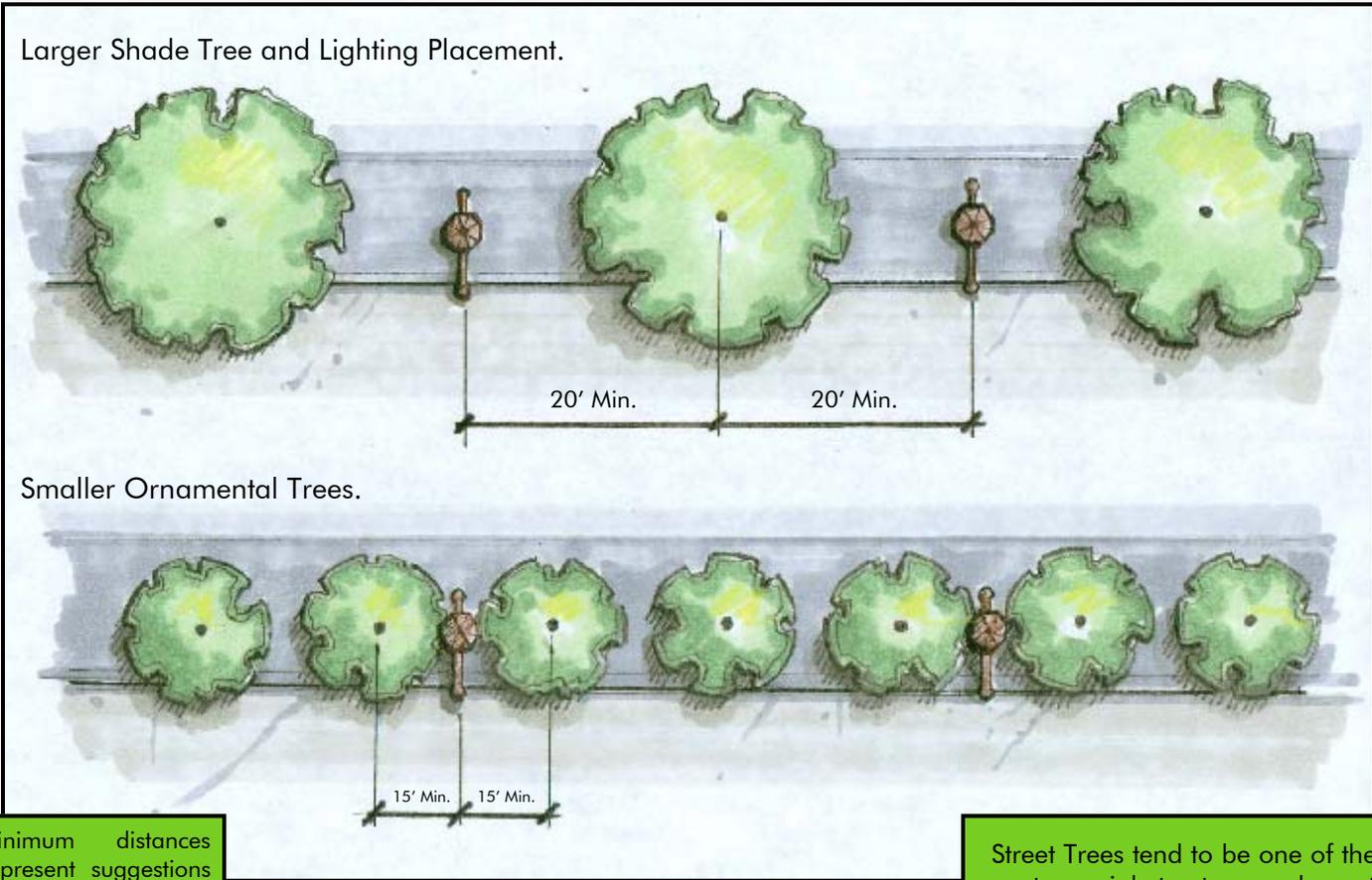
Landscaping

Downtown Kennett currently presents a fairly bare appearance that can be improved markedly by installation of low maintenance landscaping. But any landscaping will require some maintenance and the City should be prepared to care for this valuable asset to its image from the outset. The following guidelines should be followed:

- Street trees in front of main street building facades should be avoided. Instead street trees should be planted in front of parking lots or other property without street edge building facades.
- Landscaping spots can also be identified along side streets to complement, but not obstruct building facades.
- If landscaping in front of a business is desired, plants in movable containers should be considered. Containers should never be place in the pedestrian pathway but rather immediately adjacent to buildings or curbs.
- Trees work best when planted in groups or islands where they can thrive on larger volumes of soil.
- Trees and shrubs should be of any hearty variety common to the region but specified at a size which will allow a minimum of seven feet of clearance before any lateral branching begins.
- The fullness of the tree should be considered to avoid excessive roosting of birds.
- Fruiting berries or other intricacies of a trees annual cycle should be a maintenance and clean-up concern.
- Trees should also be chosen for their root growth structure. Care should be taken to select varieties with downward growing roots, not lateral growth that will damage surrounding concrete features. As trees grow, the surround tree well, brick, or concrete should also be enlarged as appropriate.
- Shrubs should be massed in groupings of five to seven plants with no more than two different species within a planting bed.
- Locate plantings in traditional areas of the site. For residential buildings, plantings along fences, walks, foundations, and at porch edges are good locations.



Examples of poor tree planning and maintenance.



The minimum distances shown represent suggestions for installation placement. Actual distances may vary due to site conditions.

Street Trees tend to be one of the most controversial streetscape elements, but they are well worth the trouble. The natural feeling provided by correctly placed, selected, and cared for trees is critical to connecting with pedestrians.



Main & St. Francis / First Street intersection currently.

The existing stoplight configuration has been maintained as First Street is also Missouri State Highway 84. If it is possible to reconfigure the light, it may be to Kennett's advantage to strive for a more aesthetically pleasing and lower profile structure that would still be acceptable to the Missouri Dept. of Transportation. MODOT may be open to this idea particularly if parking is removed as recommended.

This being the most obvious structure and at this key location, makes this a critical issue for Downtown Kennett.

Potential enhancements to Main & St. Francis / First Street intersection.





Potential view upon entering the Kennett Square District from the East along First Street.

WAYFINDING

Wayfinding Principles

Wayfinding is an indispensable tool for directing travelers to destinations while at the same time creating a positive first impression. The term wayfinding was originally coined by Kevin Lynch in his seminal 1960 book *The Image of the City*. Lynch presented the concept that people use a cognitive map to move through their environment to their destination. Wayfinding develops a system to assist travelers in interpreting the map.

Wayfinding systems have been used for many years in institutions such as colleges and corporate headquarters. As it is commonly recognized today wayfinding helps travelers *find* their *way* to the destination, in this case Downtown. The goal in relation to community planning and specifically to downtown Kennett, is to assist this process in becoming as transparent and seamless as possible. Furthermore, by taking a comprehensive approach in developing the wayfinding system it can reinforce the community's unique identity and sense of place. This can be accomplished through four main aspects that can be controlled and enhanced through appropriate design and building codes:

Architecture:

- Visual clues of buildings and other features of a street aid people in knowing their location and the direction of their destination without the use of signage.
- Strong architecture, such as the Dunklin County Courthouse, serve as landmarks and orientation points. These points are often destinations as well as starting points and other wayfinding techniques should exploit this aspect.
- Buildings themselves have visual aids that draw our eyes to where we expect an entrance or a shop window to be located.

Sight Lines:

- The motorist will feel most comfortable in maintaining visual contact with his or her destination and will want to make as few direction changes as possible.
- Clean, clear lines down streets at key intersections should be maintained.
- Avoid allowing buildings to encroach or block these lines.
- Repetitive landscaping and furnishings can enhance and draw the eye down these streets, but care must be taken that these items do not obstruct important navigational landmarks.

Lighting:

- Lighting can be used to encourage routes and pathways.
- Warmly lit sidewalks and streets draw the customer onward. Warmly lit storefronts and entrances draw the eye and provide the customer with the information needed to get to the business.
- A repetitive line of lighting can be a very effective navigation tool.
- Poor lighting causes missed information and leaves an unsafe impression.

Signage:

- Uniform signage at important decision points is a critical element of downtown wayfinding.
- Excessive signage will lessen the effectiveness of individual signs. Fewer, easy to read, appropriately placed signs are preferred.

Specific Issues for Kennett will include the motorists' need for clear direction to reach the Sheryl Crow Aquatic Center.

Wayfinding Components

Wayfinding systems are made up of components that create an arrival sequence to the Downtown. The system consists of common themed signs, of various types that direct travelers to attractions. All too frequently existing wayfinding systems are inadequate. Typical problems with existing wayfinding systems include:

- Lack of accuracy, with arbitrary sign location
- Visual clutter from too many signs and foliage
- Lack of focus in directing traffic to Downtown
- Diffuse allocation of signs, across many “entrances”
- Signs that lack charm, or are standard Department of Transportation issue
- Routes actually direct travelers around Downtown
- Signs are too small and consist of different sizes, colors, and types

A successful wayfinding system is made up of many components that can be utilized in a variety of configurations. Some communities may require only a few of the components or can gradually add components into a complete system. Determining how many of the components a wayfinding system needs, depends upon the size, number and type of Traffic Corridors and Key Transportation Nodes within the system.

Key Transportation Nodes are significant sites that require directional signage or other wayfinding mechanisms that will assist in the visitors decision making process. These Nodes are typically located at major road intersections or crossings, and have a high amount of traffic relative to the area. Effective Transportation Nodes will not rely just on signage, but on architecture, sightlines, and lighting as well.

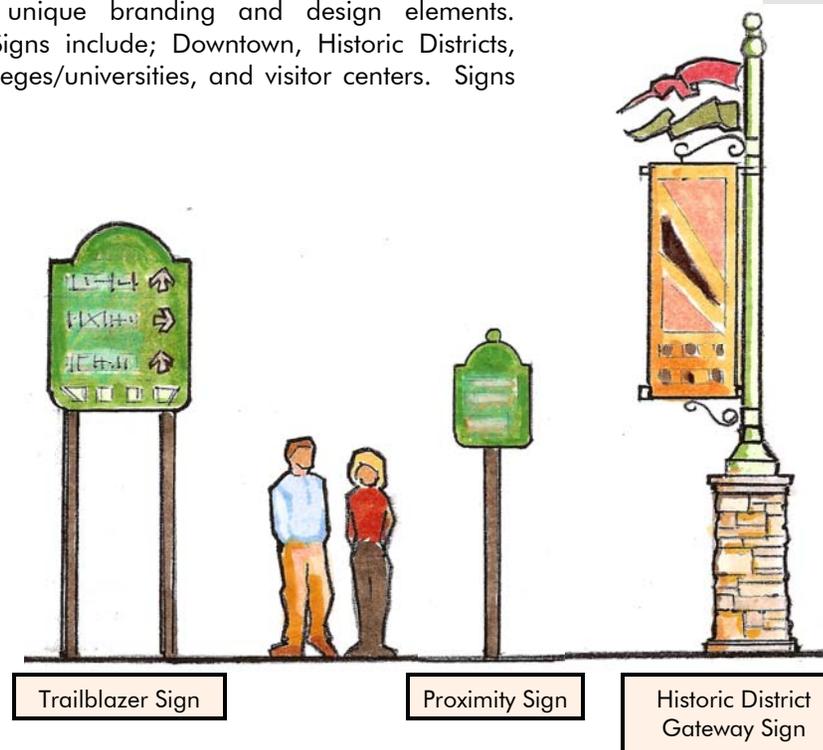
Other components of a successful wayfinding system include, but are not limited to:

- Primary Gateway Sign—Serves as the “Welcome” to a visitor, creating the first impression of the community. The sign should be significant, serving as a landmark.
- Traffic & Directional Signs—As unobtrusive and attractive as possible, while still meeting Department of Transportation guidelines for safety. Amenities like parking and public restrooms can usually be enhanced and improved.



- Trailblazer Sign—Utilitarian purpose combined with unique branding and design elements. Attractions to consider as destinations on Trailblazer Signs include; Downtown, Historic Districts, museums, event areas, government offices, parking, colleges/universities, and visitor centers. Signs should be located at or near a key transportation nodes.
- Proximity Signs—In close proximity of the attractions these signs direct visitors to the destination.
- Historic District Gateway Sign—Creates a boundary for the Historic District, should be used within the Historic District Corridor. These signs should reflect the size, scale and character of the architecture within the district.

All signs and banners included in the wayfinding system should have a simple and uniform design. Above all, the signs need to be brief and easy to read with large type face, appropriate coloring, and adequate character spacing. The wayfinding system signs should be unique and stand out in their surroundings. The automobile perspective should be taken when placing signs. The sign guidelines should be developed using the Uniform traffic Control Devices Standard.



Wayfinding Planning Areas

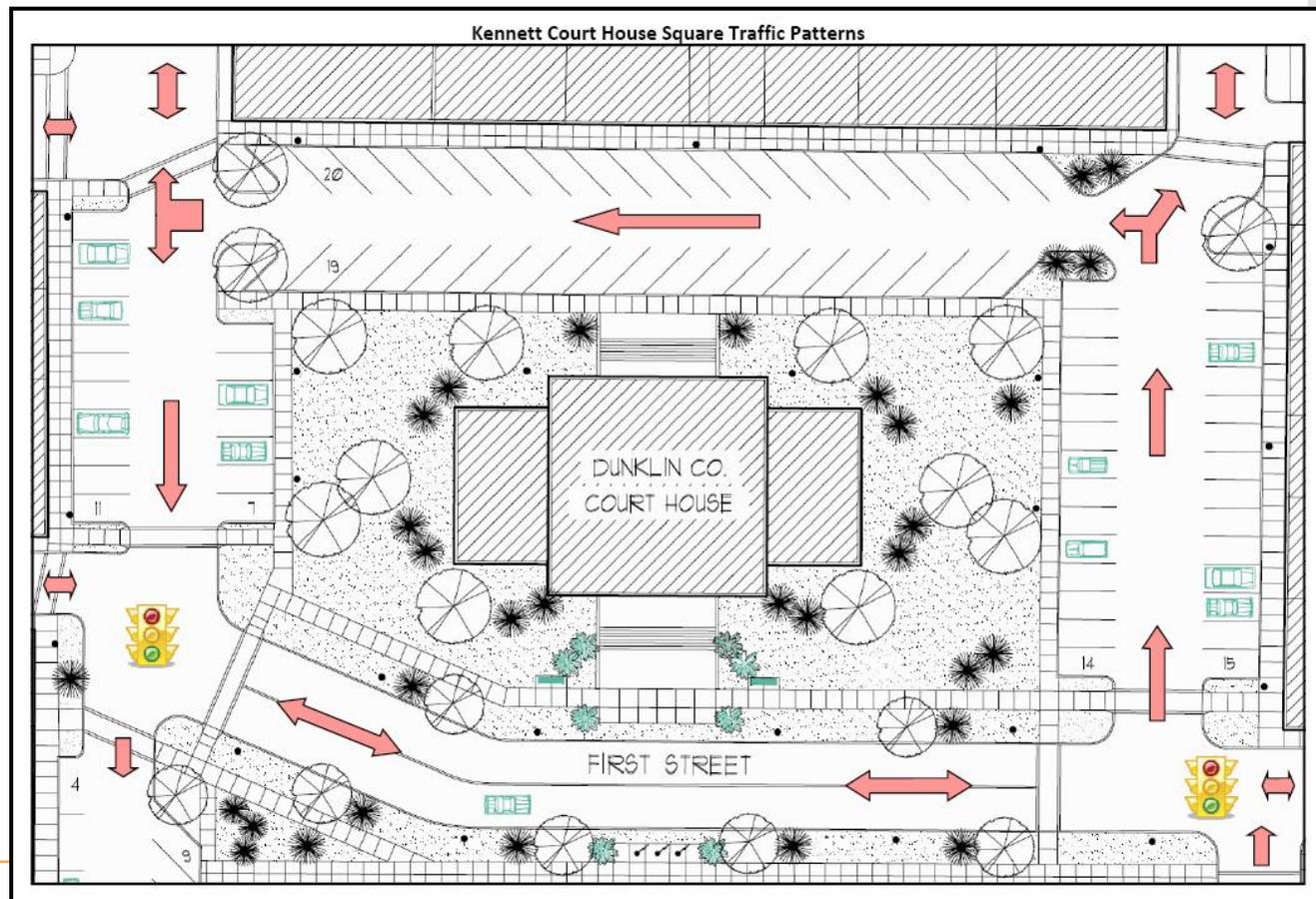
The three key planning areas for wayfinding in Downtown Kennett can be described in the above context as follows:

- Kennett Square—The area adjacent to the Courthouse and primary focus of the downtown activity.
 - Key transportation nodes should have the goal of getting visitors to this area. Trailblazer signs near First Street and Highway 412 and Kennett Street and Highway 412.
 - The goal of the wayfinding within the Square is to efficiently direct visitors to their destinations.
 - Use of Primary Gateway Signs and Historical District Gateway Signs in this area will let the visitor know they have 'arrived' in the heart of the activity.
 - Directional and amenity signage will also be critical components.
- Sheryl Crow Aquatic Center—This is another significant node of activity for Kennett and is located just 2 blocks South of the Square. Visitors to the center are greeted with the uninviting rear facades of the south side block of the Square.
 - Due to the one-way streets in Kennett and the high volumes of people this attraction draws, key transportation nodes should divert traffic quickly to and from this center.
 - Proximity signs, exciting rear facades, sidewalks, and lighting should be used to invite people to visit Kennett Square.

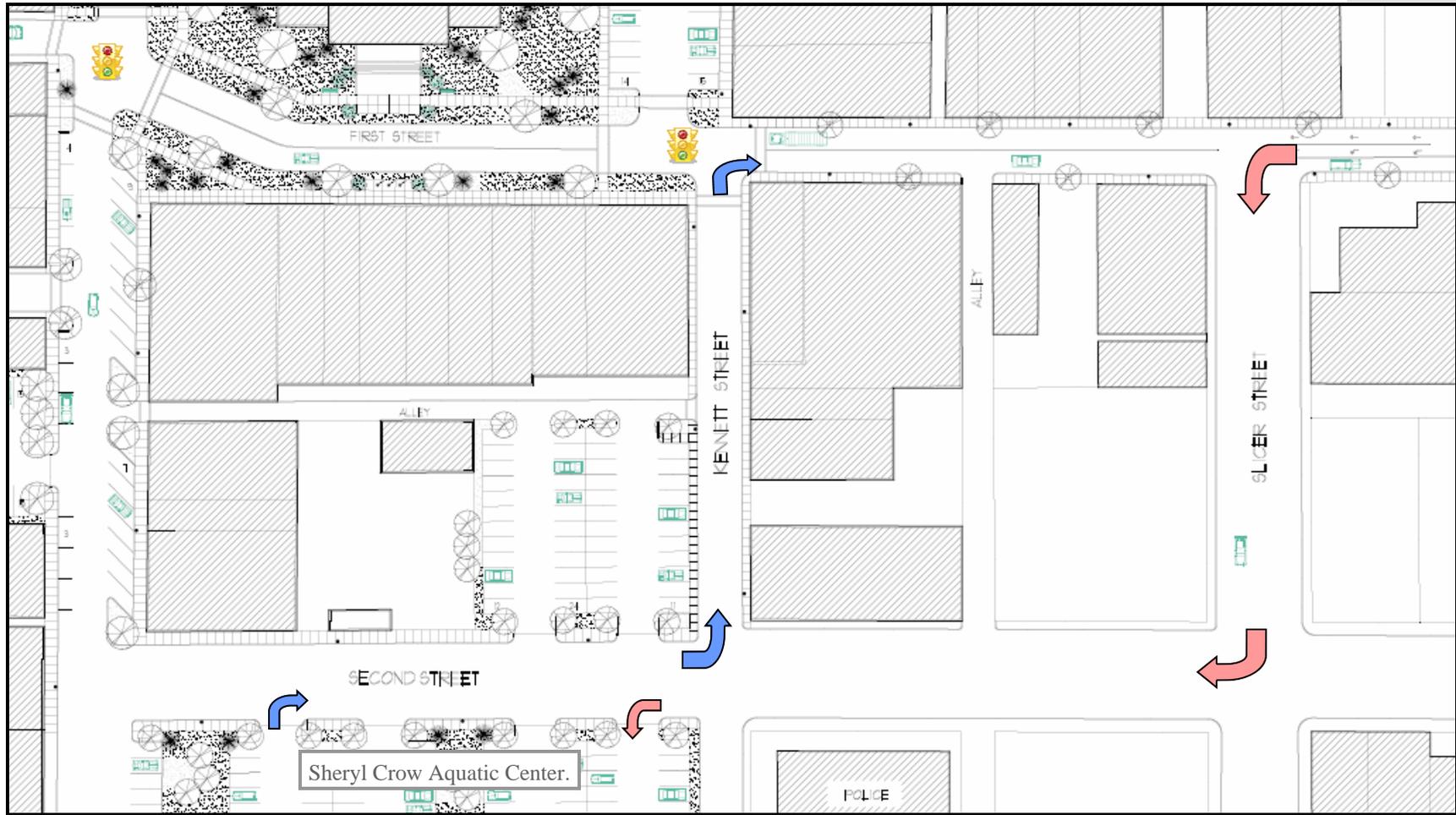
- First Street Approach—Typically the beginning of the wayfinding system for travelers to downtown Kennett will start as they exit Highway 412. Highway 412 connects Kennett most directly with the Interstate system. First Street is the closest and most direct corridor into downtown Kennett. The corridor consists of a variety of commercial uses and should focus on simple design treatments that reflect the scale and traffic. This corridor needs to be inviting. Building codes need to be firmly enforced. Signage types for this corridor should include Primary Gateway and Trailblazer signs, with the Primary Gateway being located closer to the Courthouse Square and the Trailblazers nearer to Highway 412.

The traffic routing in downtown Kennett may need to be studied. The proposed improvement does not contemplate any changes to the patterns, but utilizes the existing network on one-way streets. Current patterns are shown below.

With First Street also being State Highway 84, Kennett is somewhat restricted from making improvements to this roadway, but design enhancements and improvements can occur. A close relationship with the Missouri Department of Transportation should be cultivated.



Potential wayfinding routing to and from Sheryl Crow Aquatic Center utilizing First Street Entry Corridor.



 Approach

 Depart

IMPLEMENTATION

Recommendations

This section is intended to provide direction, financial and otherwise, to assist the community in implementing the desired design guidelines developed through the DREAM initiative. It is very likely that some of the following points have been noted in other aspects of the DREAM documentation, such as the Organizational Structure Review. Although some of these ideas may also apply to other DREAM aspects, the discussions in this document will focus on the improvements noted in these design guidelines only. Sustainability of the downtown organizational structure, while critical, is not considered in this document so that the focus can remain on the physical improvements of downtown.

Another assumption made is that the community wants to implement the suggested design guidelines. The DREAM initiative is a unique planning initiative in that it not only sets out plans and goals, but links those plans with the appropriate agencies that can provide funding. A DREAM community is responsible for choosing, initiating, promoting, and ultimately implementing the ideas provided through this program. These recommendations will assume that the officials, residents, and property owners will 'buy in' to the improvements desired by the community.

Accomplishing this 'buy in' includes:

- ⇒ Demonstrating a thorough understanding of the mechanism being proposed
- ⇒ Effectively communicating the benefits to the downtown revitalization effort
- ⇒ Honestly explaining the impact to all residents and property owners
- ⇒ Reinforcing the link between the health of downtown and the overall health of the community
- ⇒ Strategically maintaining visibility and momentum

To create the resources that will ultimately achieve the goal of a successfully revitalized downtown, it is important to understand that additional taxes, districts, or legislation may be required. The benefits created for downtown by these new mechanisms should far outweigh any new costs to the residents, property owners, or consumers.

In addition, the recommendations that follow are not to be considered "stand alone" elements and the community may wish to implement several on the road to revitalization. This approach diversifies the effort and increases the chances of successful programs; it is encouraged.

Short-Term -

- The City should review its code enforcement practices and ordinances to see if adjustments are necessary. Effective practices will include regulations that:
 - ◇ Address more than just safety concerns. This is an appropriate tool to ensure aesthetics as well.
 - ◇ Secure the condition of the downtown structures for longevity. Quality construction practices must be insisted upon.
 - ◇ Promote the conservation and efficient use of resources. "Green" and other energy efficient, innovative building methods should be considered and can be required through codes.
 - ◇ Are firmly enforced. This will demonstrate to future developers that their development risk will be minimized because Kennett will insist on the same quality of structures surrounding their investment property.
 - ◇ Can be flexible if reasonable variations will not compromise quality or other construction aspects. Any governmental process needs to be able to bend for unusual situations.

Short-Term continued -

- The City has been successful in obtaining grant funding for aspects of downtown. Efforts should be expanded to include foundation grants and programs provided by large companies with locations in Kennett. Wal-mart and Pepsi are two examples.
- The Chamber of Commerce should organize a seminar series aimed at area contractors and downtown property owners concerning appropriate restoration procedures for downtown buildings. The main speaker can be obtained from, or suggested by the State Historic Preservation Office, the City can present these guidelines, and a business, perhaps a bank, could sponsor lunch.
- The City, Chamber, and KCDC should pool some funding and develop a local façade program. This could be a very basic program that provided a 50% match up to a few thousand dollars, but the façade work would have to be approved and fit into these guidelines. Initially the focus might be narrowed to removal of metal awnings or inappropriate siding, but could grow as resources and grants are increased.
- The Chamber and KCDC should work with the City to develop a sponsorship program whereby individuals or businesses can “buy” specific street furnishings for public use. A catalog can be printed that will show the costs of items, which should include installation and a plaque with a message from the sponsor. The City should do all installations and will maintain ownership.
- The Chamber, KCDC, and City should create a formal “Adopt-a-spot” program whereby the burden to maintain landscaping and other maintenance can be adopted by civic groups and clubs throughout the downtown area. The City should supervise, but allow the volunteers freedom to install flora, remove litter, and do other minor repairs and clean-up on a quarterly basis. The City should post a plaque indicating the adopting group at the spot and the Chamber and KCDC should recognize the groups in newsletters and other opportunities.
- The Chamber, KCDC, and City can cooperatively work to identify and recognize outstanding design installations in the Downtown area. If a prize can be offered, a plaque created, and other local recognition and praise provided competition may result which can be a great motivator as property owners revitalize their buildings or grounds. This type of voluntary program should require strict adherence to the guidelines the City develops.

Long-Term -

- In addition to adjusting current codes and ordinances, the City should form a new district that will link these guidelines to the downtown area. This can be a historic district, a downtown zoning district or a special incentive district.
 - ⇒ A historic district overall must meet historic qualifications as well as the improvements made within and approved project may qualify for funding programs and tax credits. A local preservation ordinance will have to be adopted.

Long-Term continued -

- ⇒ A zoning district is implementable by the City and more flexible, but will not have any funding attached.
- ⇒ A special incentive district such as a Tax Increment Financing (TIF), Community Improvement District (CID), or a Neighborhood Improvement District (NID) is a very compelling method to tie in the design guidelines with the funding for the improvements to be made. These districts will require in depth analysis, property owner support, and additional taxes or fees. This method will create sustainable funding.
- The City could form an Architectural Review Board to review all construction and ensure that these guidelines are implemented. Such a procedure will require strict adherence to State Statutes concerning appeals and property rights.
- The façade program could be expanded to be a revolving loan program through a grant or credit funding. The process of these loans should allow for some amount of nominal interest or points to be reinvested back into the program.
- As the various organizational components come into place for downtown Kennett, there will be opportunities for individuals and businesses to make donations to the KCDC, Chamber, or another entity. These donations are encouraged and dynamic programs to solicit for donations should be developed. Kennett has had some experience through the generous donation of Sheryl Crow that made the Aquatic center possible, but Ms. Crow can't provide for all of downtown Kennett's needs. Indeed, broad, sustainable community support is needed.
- The City should include a study of traffic patterns, specifically as they relate to downtown visitors and Highway 84 through traffic. From this study may emerge a plan and implementation steps to be included in the comprehensive plan.
- The City should consider developing and implementing a community-wide wayfinding plan along with any transportation improvements. Major wayfinding components should wait until the transportation plans are decided upon, but others should be implemented as soon as possible to help increase Kennett's downtown appeal.

This Page Intentionally Left Blank

APPENDIX

Attachment 1 - Missouri Financing Mechanism Matrix
Downtown Revitalization and Economic Assistance for Missouri
Kennett, Missouri

| Name | Eligible Uses of Funds | Formation | Powers & Limitations | Governance | Borrowing Authority | Sources of Revenue |
|--|---|---|---|---|---|---|
| Community Improvement District (CID) (Ch. 67, R.S.Mo.) | Public capital improvements; Private capital improvements (located in a blighted area); Special services | By governing body of City, on petition of majority owners by assessed value or number | Petition for district formation specifies: area and duration of district; maximum rate of taxes; method and maximum rate of assessment; types of services; types of improvements; maximum borrowing authority; eligible uses of funds | Program managed by district board, with annual report to City; Board to consist of 5-30 members, appointed by City or elected by "qualified voters" of district, depending on petition; petition may identify original members; annual levy of taxes/assessments set by Board (within petition limits); District may be terminated by City, upon majority of property owners, by value and per capita | Board may issue obligations payable solely from district revenues and assets pledged; District obligations are not general obligations of the district, unless approved by supermajority of voters in district | Special assessments approved by petition or any reasonable method of assessment; Taxes on real property and/or business license or approved by qualified voters; Authorizes different property classes and levy rates for each class based on level of benefit |
| Special Business District (SBD) (Ch. 71, R.S.Mo.) | Public capital improvements; special services | By governing body of City, on petition by one or more property owners | City makes all decisions for district | Advisory board of commissioners, with final authority in City governing body | District may issue general obligation bonds, with approval of supermajority of voters of the district; District may issue revenue bonds to finance revenue-producing facilities, payable from revenue generated by those facilities | Real property taxes with maximum of \$0.85 per \$100 assessed valuation and approved by qualified voters and property owners; Business license taxes if City already imposes tax on businesses licenses, then additional tax cannot exceed 50% of current tax and approved by qualified voters |
| Neighborhood Improvement District (NID) (Ch. 67, R.S.Mo.) | Public capital improvements | By governing body of City, upon either approval of voters in district or petition signed by 2/3 of owners of property in district by area | Election ballot or petition for district formation must specify general nature of improvement, estimated cost, boundaries, method of assessment; City makes all other decision, including classifications and assessment methods | Governed by City | City must issue general obligation bonds to pay for improvements within district if special assessments are inadequate | Special assessments only; Assessment on per lineal foot or square foot or any other reasonable assessment method; Approved by vote of people in district or petition of property owners |
| Tax Increment Financing (99.88-.865 R.S.Mo.) | Public capital improvements; property assembly (including acquisition and demolition of buildings); costs or rehabilitation, reconstruction, or repair or remodeling of existing buildings and fixtures; relocation costs | By governing body of City; requires TIF Commission review and public hearing | Requires adoption of Redevelopment Plan which describes the project, redevelopment project costs, sources of funds to pay costs, type and term of sources of funds to pay costs, type and term of obligations, cost-benefit analysis, eligibility analysis and other findings per the TIF Act; must pass "but for" test | Governed by City; Incremental revenues allocated to Special Allocation Fund | Obligations secured by Special Allocation Fund and may be issued by the City; Obligations shall not be a general obligation of the political subdivision and cannot exceed 23 years; Obligations may only be payable out of any funds or properties other than those specifically pledged as security | Special Allocation Fund collects local incremental revenues generated within the district; 100% PILOTS and 50% EATS are captured by the Special Allocation Fund |
| Community Development Block Grant Funds | Funds a variety of economic development activities, including site acquisition, assessment, demolition, remediation, public works and support the development of affordable housing | Contingent on annual appropriation by HUD to Metropolitan cities and urban counties (entitlement communities) or State for distribution to non-entitlement communities | Submit a Consolidated Plan, annual action plan, and certifications to HUD. If you are a non-entitlement community, an individual or organization, apply to the appropriate city, state or county for funding. Funding restricted by program guidelines and availability of funding. | Entitlement communities or the State receive entitlement funds from HUD that may be used for eligible activities, such as infrastructure. Non-entitlement communities compete for funding via application process to State. | N/A | Funding based on project need, available funding, and program limitations. No match is required, but local in-kind and/or cash matching funds are encouraged. |
| Missouri Downtown Economic Stimulus Act (MODESA) | Certain public improvements; Financing; Relocation costs; Property assembly | Governed by local Downtown Development Authority (DDA) appointed by mayor or chief financial officer of municipality; MODESA application reviewed by Missouri Development Finance Board (MDFB); Combined local and state approval | Applies to "major initiative projects only; must pass "but for" test; project must be located in a "blighted" area; Financial threshold based on new jobs and localities population; local development approvals required; DDA has statutory powers to borrow funds, own property, etc., Need MDFB approval from state participation in funding | Governed by City or Downtown Development Authority | Obligations secured by Special Allocation Fund and may be issued by the City, DDA, or MDFB; Obligations shall not be a general obligation of the political subdivision, DDA, MDFB or the state and cannot exceed 35 years; Obligations may only be payable out of any funds or properties other than those specifically pledged as security | Special Allocation Fund collects local incremental revenues generated within the district; 100% PILOTS and 50% EATS are captured by the Special Allocation Fund; With MDFB approval the Special Allocation Fund may also collect new revenue generated in the form of 3% state sales tax and a 2% portion of the state income tax withholding |

Attachment 1 - Missouri Financing Mechanism Matrix
Downtown Revitalization and Economic Assistance for Missouri
Kennett, Missouri

| Name | Eligible Uses of Funds | Formation | Powers & Limitations | Governance | Borrowing Authority | Sources of Revenue |
|--|---|--|---|--|---|---|
| Transportation Development Districts (TDD) (238.200-238.275 R.S.Mo.) | Transportation infrastructure | By City or County, by voter petition, local transportation authority petition, property owner petition; must be approved by a majority of those voting | Project improvements shall not be under the control and jurisdiction of a local transportation authority (City) while the TDD retains control and jurisdiction. Power to acquire, sell and convey property subject to local authority approval. | Program managed by district board, with MODOT and Local Authority oversight; Board may consist of at least 5 nor more than 15 persons with one MODOT and one or more Local Authority advisors; Election by voters or Owners require approval of special assessment, tax or funding method | District may contract and incur liabilities, may borrow money and issue bonds, notes and other obligations; May issue bonds payable from its revenues but may not exceed 40 years | District may levy a property tax not to exceed \$0.10 per \$100 assessed valuation or sales tax not to exceed 1%; District may levy tolls or special assessments for improvements benefiting the project |
| Missouri Historic Preservation Tax Credits (253.545-.559 R.S.Mo.) (S.B. 1, 1997) (S.B. 827, 1998) | Hard and soft costs of the rehabilitation of buildings on the National Register of Historic Places or in a historic district which is a National Register Certified Historic District | N/A | Any person or entity incurring costs for rehabilitation of eligible property which is a certified historic structure or structure in a certified historic district shall be entitled to a State income tax credit of 25% of the cost of rehabilitation, provided the rehabilitation costs exceed 50% of the total basis in the property and the rehabilitation meets historic standards | Requires project approval by the State Historic Preservation Office and funding approval by the State Department of Economic Development (DED) | N/A | Excess tax credits may be carried back for three years and forward for 10 years or until fully used; Taxpayers eligible for such credits may transfer, sell (monetize) or assign credits |
| Brownfield Redevelopment Program (447.700-447.718 R.S.Mo.) | Voluntary brownfield remediation | N/A | Project must anticipate creating at least 10 jobs or retaining at least twenty-five workers and must be found to focus its redevelopment effort on an eligible site. Completion of remediation subject to the State Department of Natural Resources and the Environmental Protection Agency | Eligibility for an entity is contingent on being accepted into the Missouri Voluntary Cleanup Program; Entity can then apply to State Department of Economic Development (DED) for financial assistance; DED may approve incentive package that is limited to the least amount necessary to achieve remediation or incur a positive net benefit. Local jurisdictional participation may be required. | N/A | Incentives may include tax credits; tax exemptions; grants; loan guarantees, or loans; once appropriated, these funds can be used to purchase or to offset the purchase of materials, supplies, equipment, or other things related to the redevelopment of the site as approved |
| Urban Redevelopment Corporations (Ch. 353, R.S. Mo.) | Clearance, replanning, reconstruction or rehabilitation of blighted areas, and the construction of such structures as may be appropriate | Incorporation with the Secretary of State, Application to the City for authorization of a Redevelopment Plan; Requires public hearing and the granting of rights and powers by City ordinance consistent with Ch. 353 provisions | Corporation can only operate in an area with a Redevelopment Plan. Powers of the corporation must be authorized by the local jurisdiction, including the authorization for tax abatement. The provisions of the general corporation law apply unless in conflict with Ch. 353. | A corporation that has been organized to serve a public purpose. | Any Urban Corporation may borrow funds and secure the repayment thereof by mortgage which shall be a lien upon no other real property except that forming the whole or a part of a single development area. | The City may authorize a 100% abatement of taxes in years 1-10 and a 50% abatement of taxes in years 11-25 on properties owned by the Corporation; the Corporation may accept grants or loans from government agencies |
| Tax Reimbursement Agreements (Ch. 43, R.S. Mo.) | Publicly owned infrastructure | Redevelopment Agreement between the City and Developer; Approved by ordinance | May require all contracts for work to be subject to public bids; Only City's tax revenue is utilized; | City oversees project | N/A | Incremental increase in the City's property taxes and as much as 100% of the City's incremental increase in sales and utility taxes, depending upon municipal charter limitations |