

# BUILDING AND STREETScape DESIGN GUIDELINES AND DEVELOPMENT PLANS

Master Plan - Alternate 1



**D**OWNTOWN  
**R**EVITALIZATION &  
**E**CONOMIC  
**A**SSISTANCE FOR  
**M**ISSOURI



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## Acknowledgements



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## Table of Contents

<b>Acknowledgements</b> .....	<b>I</b>
<b>Table of Contents</b> .....	<b>III</b>
<b>1.0</b> .....	<b>Introduction 1</b>
1. 1 .....	Intent of the Guidelines 1
1. 2 .....	Historic Overview 2
1. 3 .....	Existing Context 5
1. 4 .....	Process and Public Input 11
<b>2.0</b> .....	<b>Building Design Guidelines 15</b>
2. 1 .....	Rehabilitation and Maintenance Guidelines for Historic and Non-Historic Buildings 15
2. 1. 1 .....	Differences Between Rehabilitation, Restoration & Renovation 15
2. 1. 2 .....	The Benefits of Rehabilitation 16
2. 2 .....	Guidelines for All Historic Downtown Buildings 17
2. 2. 1 .....	Building Zones 17
2. 2. 2 .....	Façade Elements 18
2. 2. 3 .....	Rhythm 18
2. 2. 4 .....	Alterations 19
2. 2. 5 .....	Masonry 20
2. 2. 6 .....	Downtown Color Palette 21
2. 2. 7 .....	Windows 22
2. 2. 8 .....	Architectural Details 23
2. 2. 9 .....	Entrances 24
2. 2. 10 .....	Awnings 25
2. 3 .....	Additional Guidelines for Downtown Commercial Buildings 25
2. 3. 1 .....	Storefronts 25
2. 3. 2 .....	Signage 28
2. 3. 3 .....	Lighting 35
2. 3. 4 .....	Rear Elevations 36
2. 4 .....	New Construction Concepts 38
2. 4. 1 .....	What’s Acceptable? 38
2. 4. 2 .....	What’s Not Acceptable? 39
2. 5 .....	Franchise Architecture 40
2. 6 .....	Design Guidelines for New and Existing Residential Buildings 41
2. 6. 1 .....	Building Orientation 41

2. 6. 2.....	Building Materials	41
2. 6. 3.....	Building Design Elements	41
<b>3.0.....</b>	<b>Sustainable Design</b>	<b>43</b>
3. 1 .....	Fundamentals	44
3. 2 .....	Elements	45
<b>4.0.....</b>	<b>Proposed Building Improvement Concepts</b>	<b>47</b>
4. 1 .....	Proposed Building Improvement Concepts	47
4. 1. 1.....	Rear Building Facades (Elm to Church St.)	48
4. 1. 2.....	Rear Building Facades (Church to Brick St.)	51
4. 1. 3.....	Rear Building Facades (Brick St. to Hall)	52
<b>5.0.....</b>	<b>Streetscape Design Concepts</b>	<b>55</b>
5. 1 .....	Design Coordination	56
5. 2 .....	Infrastructure	57
5. 3 .....	Accessibility	58
5. 4 .....	Sidewalk Zones	59
5. 5 .....	Parking and Service Areas	63
5. 6 .....	Outdoor Café Seating	64
5. 7 .....	Street Lighting	65
5. 8 .....	Signs and Banners	66
5. 9 .....	Bicycles	67
5. 10 .....	Public Furnishings	68
5. 11 .....	Landscaping	70
5. 12 .....	Rain Gardens	72
5. 12. 1 .....	List of Appropriate Planting Species	74
5. 13 .....	Fountains, Monuments, and Public Art	76
<b>6.0.....</b>	<b>Development Plans and Concepts</b>	<b>77</b>
6. 1 .....	Finley River Redevelopment	77
6. 1. 1.....	Finley River Redevelopment Property - Alternative One	78
6. 1. 2.....	Finley River Redevelopment Property - Alternative Two	80
6. 1. 3.....	Finley River Redevelopment Property – Building Typology	82
6. 2 .....	Courthouse Square and Finley River Redevelopment Gateway Improvements	84
<b>7.0.....</b>	<b>Wayfinding</b>	<b>87</b>
7. 1 .....	Principles of Wayfinding	87

7.2	Wayfinding Components	88
7.2.1	Architecture	88
7.2.2	Sight Lines	89
7.2.3	Lighting	89
7.3	Hierarchy of a Signage System	90
7.3.1	Gateways	90
7.3.2	Trailblazers	90
7.3.3	Proximity Signs	92
7.3.4	Special Purpose Signs	92
7.3.5	Banners and Decorative Lighting	93
7.4	Special Considerations	94
7.4.1	Corridors and Destinations	94
7.4.2	Public Versus Private Signage	94
7.5	Wayfinding Implementation	96
7.6	Review of Existing Signage in Ozark	97
7.7	Ozark’s Proposed Wayfinding System	98
7.7.1	Gateways to Ozark and Downtown	98
7.7.2	Trailblazer Signs	99
7.7.3	Proximity Signs	99
7.7.4	Pedestrian-Oriented Signage	99
7.7.5	Special Signage	100
<b>8.0</b>	<b>Implementation and Next Steps</b>	<b>105</b>
<b>9.0</b>	<b>Glossary</b>	<b>107</b>

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## 1.0 Introduction

### 1.1 Intent of the Guidelines

While these guidelines are written for Downtown Ozark, the design recommendations are sound advice that might be applicable elsewhere in the community. These guidelines are written primarily for commercial areas, however they also briefly address issues regarding residential buildings and sites. The main intent of the guidelines is to help preserve the architectural character and improve the visual appearance of Downtown Ozark and its adjacent areas.

In America, downtowns traditionally have had a sense of place. Businesses, courthouses, city halls, shops and houses of worship were almost always located in the Downtown. Downtown was the business and civic center of the community. The architectural style, size and materials of the buildings in Downtown often reflected the success and wealth of the community. In Ozark, Missouri, this civic pride was evident among the many historic facades along the courthouse square. While many of the original uses have been replaced and the commercial, the buildings and memories of Downtown Ozark remain.

Downtown Ozark has many buildings which have design merit and character. Collectively these buildings and other structures, some of which lack architectural significance, contribute to the overall character of Downtown. The condition of the vast majority of the historic buildings along the courthouse square is good. The buildings for the most part have been maintained appropriately through the years and rehabilitated with complimentary building materials. Design guidelines provide recommendations to help preserve existing buildings and spaces which still have historic integrity. This report also suggests methods of improvements for buildings which have lost their character due to inappropriate alterations or neglect. Restoration of buildings to the original design is not the goal of the guidelines, unless the building is on the National Register of Historic Places. The guidelines focus on improving the public façade and appearance of structures, streets and public spaces to help create a unique identity for Downtown. There are three types of structures which form the collective whole of Downtown:

- those that contribute to that identity,
- those that detract, and
- those that do neither.

The objective is to maximize contributing elements and minimize those which detract thus creating a stronger, more attractive Downtown Ozark.

This document also contains planning recommendations for the City of Ozark to consider regarding future policy and procedural decisions that affect the public elements of Downtown. Included are streetscape Design Concepts which the City can use in planning future public projects, as well as illustrations regarding specific locations along 3rd Street and the LCRA property. Potential issues noted with the existing streetscape, as well as the installation of new features, included aesthetic design, practicality of use, available rights-of-way, heritage, and compliance with the Americans with Disabilities Act Accessibility Guidelines (ADAAG). The overall intent of this Plan is to help preserve and improve the character and visual appearance of Downtown Ozark's public infrastructure.

The improved identity and appearance of Downtown Ozark will provide an incentive for more investment and interest. To successfully support revitalization, it is vital that property owners, city staff, and other community organizations make a long-term commitment to Downtown Ozark. The guidelines are a resource for local leaders who agree to the commitment of improving the collective visual appearance and activity of Downtown. By investing in the public spaces of Downtown, the City will send a strong message to residents, businesses, visitors, and potential private investors that Downtown is a positive and progressive place to be.

## 1.2 Historic Overview

Ozark Missouri, incorporated as a city in 1888, and its surrounding area has a rich history with deep roots in the westward expansion of the United States. The area surrounding Ozark is rich in natural sources of fresh water, hickory, oak and pine forests, fertile farmland, caves, and man-made lakes. Christian county, which Ozark was named county seat, encompasses most of the land directly surrounded Ozark.

Christian County, originally inhabited by the Osage Native American tribe, was opened for exploration via the Louisiana Purchase and other legislative acts. Exploration and settlement of Ozark was led by Henry R. Schoolcraft, the Pettijohn family, William Friend, George Yoachum, James Kimberling, George Wells, and Thomas Patterson. Smallin Cave located North of Downtown Ozark has extensive history attached to it with many of these early settlers. The early settlement days of Ozark spanned approximately between the 1820s and 30s.

The Civil War played a major part in the early history of Ozark. Christian county played a role in the nearby battlefield at Wilson's Creek through small skirmishes throughout surrounding rural areas. Like many of the nearby Southern Missouri communities, residents of Ozark were directly affected by the



Smallin Cave is an excellent example one of the many historic and natural features surrounding the Downtown area.



Ozark mill on the Finley River.

events of the Civil War. The effects of the Civil War lasted beyond the surrender of Confederate Army at Appomattox into the late 1800s. During this period vigilante bands such as the “Bushwackers” and “Bald Knobbers” brought violence and instability to the area until order was restored.

The late 1800s led to a period of stability, which was aided by the coming of the railroad system. The railroad brought prosperity and growth which has continued to present day. For much of this time, Downtown was integral to the town’s growth, serving as the commercial and civic hub for Ozark. Similar to many towns, Downtown served the local community as the place to go for trade, religious services, education, and socializing. Downtown grew and developed many prominent buildings that showcased the prosperity and quality of life in Ozark. In the 1950’s, American lifestyles changed with the rise of highway construction and the affordable automobile. Neighborhoods and commercial areas moved further away from the traditional downtown business district. Downtowns, while still the center of much community life, started to experience a loss of commercial viability. Consumers expected easy access and parking for their cars. New and modern design was preferred over traditional and old. As a result, by the mid 1970’s many American downtowns suffered from a lack of investment. Downtown Ozark was no exception.

Currently the city of Ozark lies along Highway 65, which serves as a major route for tourism related travel. In recent years Ozark has had a boom of growth which has resulted in the development of a new Community Center and Ozark Technical College. The city grew from a small town of 9,243 in 1990 to a burgeoning bedroom community of 18,082 in 2010. Ozark has immense potential for sustained growth for decades to come.

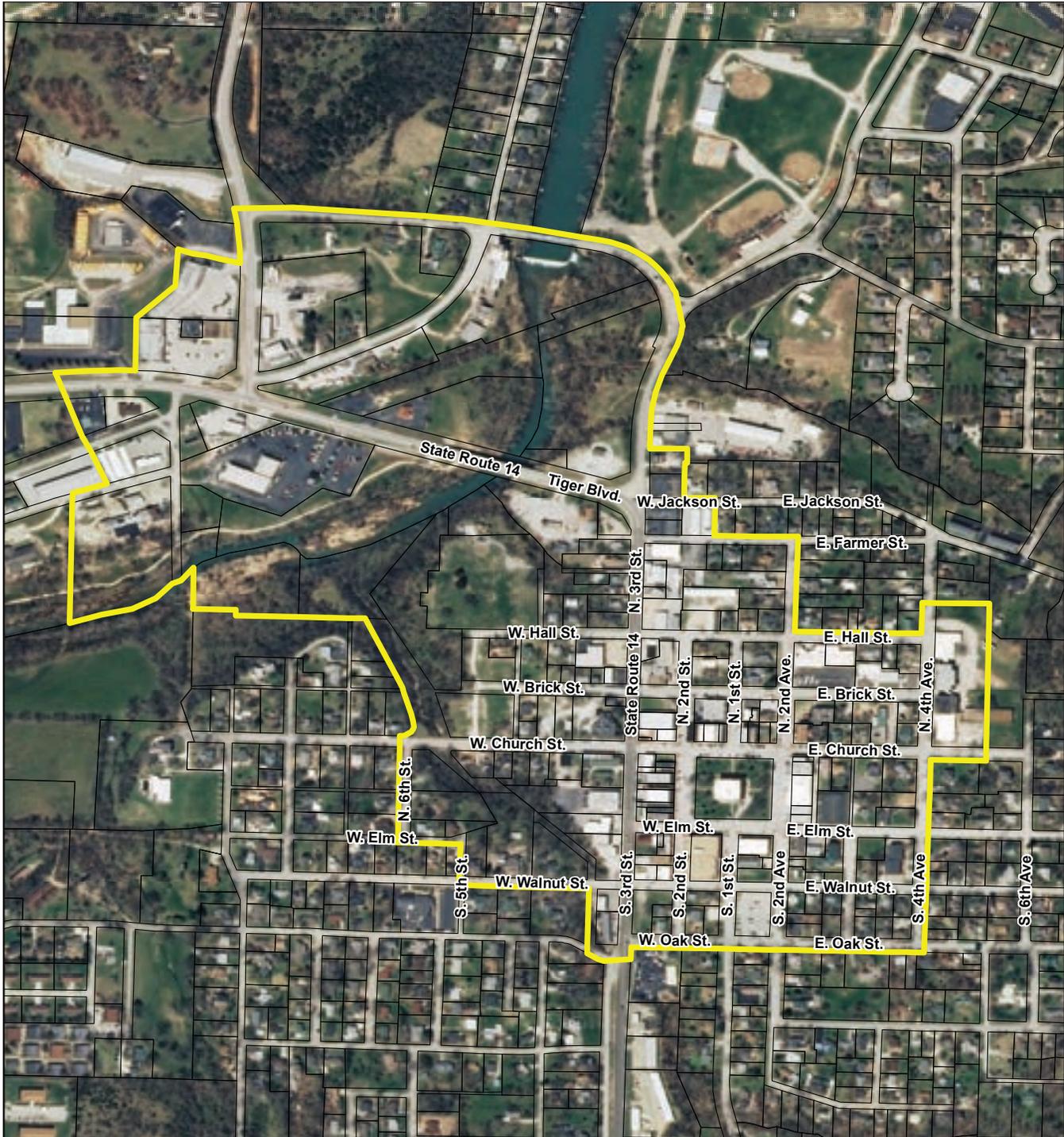
While Downtown Ozark experienced numerous changes throughout its history, many positive attributes still exist. Downtown reflects a long history with beautiful and intriguing natural features, picturesque buildings, storefronts, and homes. Although the district requires infrastructure improvements the overall area maintains its historic charm. Downtown Ozark retains many positive qualities and a unique built environment.



Image of the Ozark Community Center. This establishment along with many others signifies the economic growth of the Ozark community.



Image of a building located on the Ozark Courthouse Square. This building is characteristic of a majority of the Courthouse Square buildings.



Aerial Map showing the extent of the DREAM boundary.

### 1.3 Existing Context

Downtown Ozark has a traditional grid pattern street layout, including a central Courthouse Square filled with stores, civic institutions and offices. The buildings surrounding the Courthouse Square comprise a dense, well-defined central business district. Although there are a number of cross streets and access points, the primary access route into Downtown Ozark is from 3rd Street along the backside of buildings on the west side of the Courthouse Square. The intersection of 3rd Street and Church serves as the main entry to Downtown. The high levels of congestion, lack of signage, lack of sense of place, and limited sense of entry along 3rd Street inhibits the ease of access to Downtown.

Most of the buildings along the Courthouse Square retain their architecturally significant elements and are well-maintained, few show signs of deferred maintenance and cosmetic wear. In addition, there are newer buildings within Downtown that do not meet the historical context of the surrounding older building stock. These more recent buildings demonstrate a lack of connectivity in terms of scale, height, as well as windows, doors and siding materials. Examples include the Ozark Bank building along the east side of the Courthouse Square. Off the square and along 3rd Street buildings show more signs of wear and buildings that were altered negatively by the addition of elements which are inappropriate in scale, material, and finish. The existing streetscape in Downtown Ozark is in good condition and has recently been updated in recent years included irrigated landscape beds, street trees, period style lighting, and street corner bumpouts.

The LCRA owned Finley River Redevelopment property to the west of Downtown along the West side of 3rd Street contains a vast amount of vacant land with immense potential for redevelopment. Portion of this property have been cleared for future development, but the property is still fragmented by the presence of housing on Brick and Hall Street, and the Ozark Cemetery on the Northwest portion of the site.



View of the Courthouse Square and the Historic Christian County Courthouse building.



Image of Ozark Bank building. This building shows the use of a non-complimentary building material. The dark metal siding contrasts from the brick and masonry facades throughout Downtown.



Image of the Christian County Justice Center. This provides a good example of a newer building which compliments the historic context of Downtown.



The vast majority of buildings in Downtown are in good or excellent condition.



Overall the streetscape throughout Downtown is attractive and well-maintained. The street corners, shown here, define the parking areas and soften the streetscape.



An example of a period style light fixture in Downtown. The lighting is in good condition and is uniform throughout Downtown.



There are large areas of vacant space located along the west side of 3rd, adjacent to Downtown Ozark. Halted development plans have left exposed foundations and unattractive vacant lots.



A good example of outdoor dining in Downtown Ozark.



Many of the buildings in Downtown have attractive awnings and business signage that does not overpower building facades.



Image of a poorly-maintained automotive building located along 3rd Street.



An image of the brick sidewalk and relatively new streetscape present around the Courthouse Square.



An image of buildings located along 3rd Street. Many of the building facades along this stretch are deteriorated. Excessive signage and overhead utilities present visual and aesthetic challenges.



An image of Christian County Courthouse located in the center on the historic Downtown square.



The entry to Finley River Park.



Image of the right-of-way conditions along 3rd Street.



Image of an open lot near the main entry to Downtown Ozark. A faded mural is shown on the facade of this building.



A deteriorated parking lot entrance located along 3rd Street is shown here.



A well-maintained building located in Historic Downtown Ozark.



A view down a sidewalk in Historic Downtown Ozark.

## 1. 4 Process and Public Input

The Downtown Revitalization and Economic Assistance for Missouri (DREAM) Initiative helps Missouri communities improve efforts regarding downtown revitalization through an intense three-year planning process tailored to the needs of each city. The City of Ozark applied to be a DREAM community in 2010 and this Building and Streetscape Design Guidelines is the result of one identified program task.

Along with specific planning tasks, the DREAM Initiative provides opportunities to gather public input and examine recommendations with local leaders and residents. This Building and Streetscape Design Guidelines task included a series of ongoing interviews with City staff and community stakeholders and an open house showing of the proposed design concepts at the Ozark Birthday Bash on August 2, 2012. The public comments provided at the various meetings included a wide variety of topics.

### 1. 4. 1 Comments from the General Public regarding Downtown included the following:

- ◇ Stoplight needed at Chruch and 3rd Street.
- ◇ There is a lot of traffic from nearby school.
- ◇ Enhance 'entry' into Downtown.
- ◇ Signage needed to tell people how to get to the Square from 3rd Street.
- ◇ Need stop signs on the corners of the Square.
- ◇ Consolidate business entrances at the intersection of Route 14 and NN.
- ◇ Need a 55+ community, Not rent subsidized [In the Finley River Redevelopment Property].
- ◇ Need for playground, sculpture parks, and water graden for kids in a safe location.
- ◇ I think signage would improve the Downtown District.
- ◇ Create an incentive for building owners in the district to create the historical look due to this great little town.
- ◇ Perhaps loosening or easing some of the codes for upgrading the buildings would be of help.
- ◇ As a business owner myself, I have dealt with building codes in order to update the store and it can be very frustrating and discouraging to any new businesses planning on relocating or opening in Ozark.

### 1. 4. 2 Comments from the Downtown Business Owners regarding Downtown included the following:

- ◇ Emphasize where the front of the building is.
- ◇ Designate Parking areas clearly and show where it's at.
- ◇ We are a "Historical" Downtown – On the National Register.
- ◇ Tie in River and City Park with Downtown.
- ◇ Need office space in Finley River Redevelopment Property, provide space for Downtown attorneys to move there and free up space for retail on the Square.
- ◇ Need for traffic calming on 3rd Street.
- ◇ Need a greenway system connecting to Downtown.

- ◇ Use an archway as a gateway entry to Downtown.
- ◇ Get sign sponsors to offset cost.
- ◇ Like historic feel of iron work, brick, and turn of the century feel.
- ◇ Make sure Finley river Redevelopment site design is complementary to Historic Downtown.
- ◇ We have too many lawyers and jails in Downtown.
- ◇ Need more retail.
- ◇ Incorrect mix of businesses.
- ◇ Need signs North and South to let people know what businesses are on the Square.
- ◇ Parking is an issue.
- ◇ Tourists are currently coming to Ozark for antiques.
- ◇ Install a sign for Downtown at Lamberts Exit (CC).
- ◇ I am skeptical of an increase in bus traffic being a positive; large groups could enable shoplifting to happen by overwhelming shop owners.
- ◇ Attract motorcycle tourists.
- ◇ Need kid friendly things Downtown.
- ◇ Build a sidewalk to the Park.
- ◇ We need a park amenity Downtown.



Ozark residents inspecting development plan presentation boards at the Ozark Birthday Bash on August 2, 2012.

**1. 4. 3 Comments from the Elected Officials regarding Downtown included the following:**

- ◇ Stay with the historical context
- ◇ Need more historical looking signs as you get closer Downtown
- ◇ Downtown needs an event sign that is able to be updated regularly.
- ◇ Install brick crosswalks along 3rd St. at various entries.
- ◇ Can we attract more retail?
- ◇ Can we use the third story? Maybe only in portions of buildings.
- ◇ Getting a destination restaurant in the area sets the tone!
- ◇ Please produce 3rd Street (side of Square) rear renderings.
- ◇ We could use a view of possible cemetery landscape treatments.
- ◇ Install an archway? Create streetlights and banners along 3rd Street.
- ◇ Watch for historic cemetery grants

The recommendations within this Plan have also been discussed at numerous City meetings and will be included in other DREAM tasks, such as the Downtown Strategic Plan. Thi report and other Ozark DREAM reports are available to the public via the internet at [www.modream.org](http://www.modream.org).



A gathering of Ozark community stakeholders at the DREAM kick-off meeting.



Ozark residents inspecting streetscape presentation boards at the Ozark Birthday Bash on August 2, 2012.

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## 2.0 Building Design Guidelines

### 2.1 Rehabilitation and Maintenance Guidelines for Historic and Non-Historic Buildings

Original elements on historic buildings provide a historic value that cannot be replaced. Any original element or material that still exists, particularly on the facade, should be retained if possible. Prism glass in transom windows or a decorative wooden door with beveled glass are examples of original materials that should be retained.

Efforts should be made to accurately duplicate original features during the replacement of missing architectural elements. When an entire architectural element is missing, the replacement should match the original in design, color, texture, and other visual qualities. Where reconstruction of a missing architectural element is impossible due to a lack of historical evidence, the new design should complement the subject building, as well as surrounding buildings, in size, scale and material. Architectural design elements should reflect the building's style, but replication of similar features on comparable buildings may be acceptable.

#### 2.1.1 Differences Between Rehabilitation, Restoration & Renovation

The Secretary of Interior's Standards for Rehabilitation define rehabilitation as:

"the act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values."



A view of Historic Building located on the Ozark Courthouse Square.

Rehabilitation should be distinguished from restoration, which is:

“the act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work or by the replacement of missing earlier work.”

As opposed to rehabilitation and restoration, renovation seeks to modernize a building. Little attention is paid to retaining historically significant architectural features of a building. Renovation, by its very nature, destroys the historic integrity of a building. Once a building is renovated it may no longer be eligible for rehabilitation tax credits or listing on national or local historic registers.

### **2. 1. 2 The Benefits of Rehabilitation**

Proper building rehabilitation provides significant benefits to property owners, tenants, and contributes to the collective well-being of Downtown Ozark. Building rehabilitation may include façade improvements, updating mechanical, electrical and plumbing systems, and new interior finishes. These improvements represent a significant investment and results in a positive economic impact for the community. For a building owner, rehabilitation not only increases the property value but typically lowers ongoing maintenance and operating costs, translating to an additional return on investment. More than just an economic impact, the rehabilitation of each individual building adds to the overall improvement of Downtown.

A building façade provides the first impression to the public, which is the most lasting impression. This reinforces the importance of the proper façade rehabilitation. An attractive façade and positive first impression is critical not only to the business inside, but also to the overall impression of Downtown. Well done rehabilitation of each building will create a higher standard in regards to the public façades in Downtown. A high standard for the design and rehabilitation of public façades will maintain a collective of high visual quality for Downtown Ozark

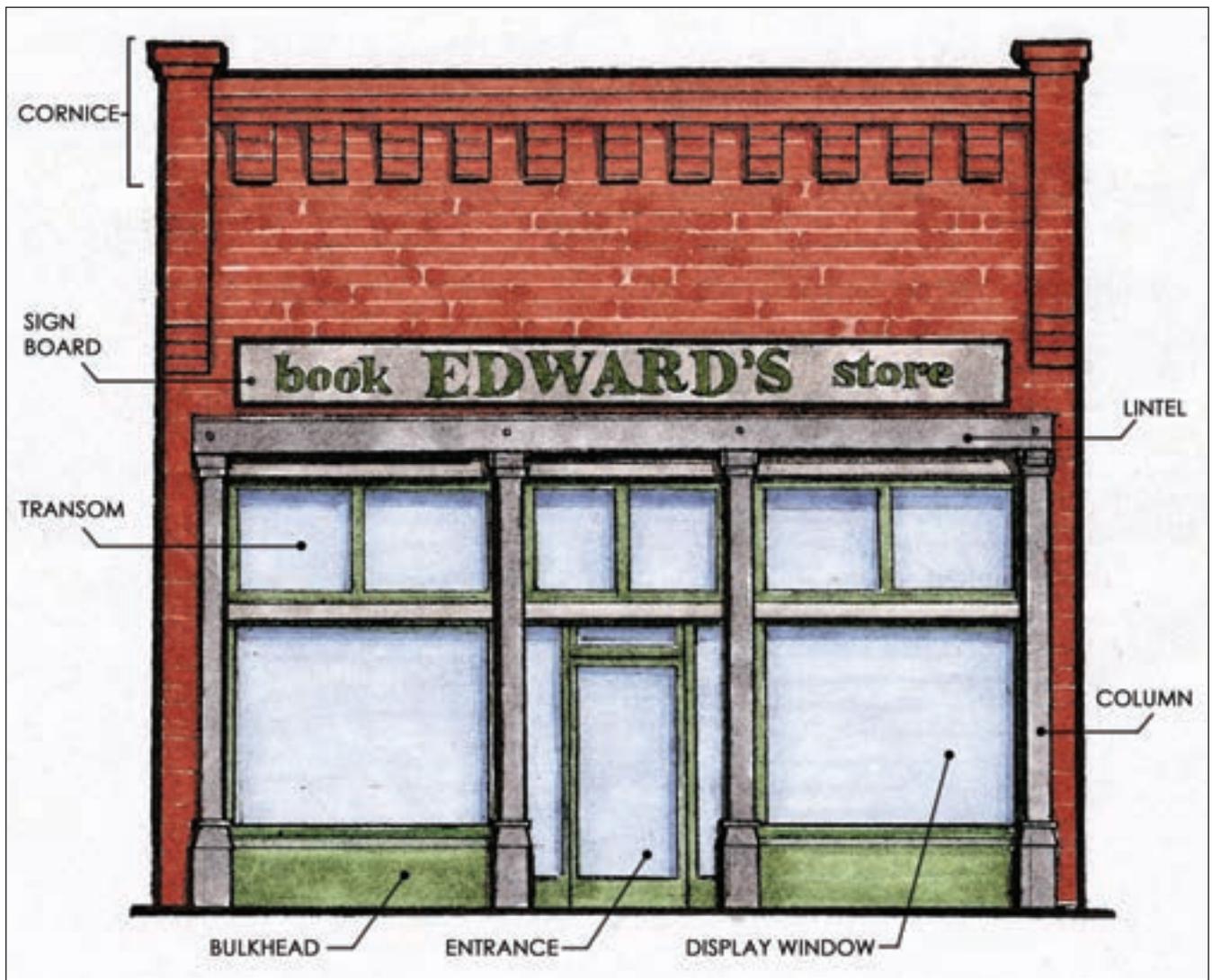


An example of Historic buildings which show some cosmetic wear and a lack of proper maintenance.

## 2.2 Guidelines for All Historic Downtown Buildings

### 2.2.1 Building Zones

Improvements to individual buildings will be discussed in the context of three distinct 'zones'; the Storefront, the Upper Façade, and the Rear Elevation. The elements of the Storefront/Façade zones are depicted in this diagram.



This diagram shows the various components of a standard historic storefront.

### 2. 2. 2 Façade Elements

The various elements of a façade must be balanced. Appropriate massing, building and floor heights, proportions, roof lines, materials, and setbacks are critical considerations in new construction.

Any future development should be encouraged to implement a design that contributes to the fabric of Downtown. Any future design that will detract from the fabric should be denied.

Other aspects like architectural details, colors, and cornices are more important to the restoration of historic buildings, but can be used effectively in new construction as well. Developing balance between all elements allows a building to be individual in its character, but at the same time complementary to the overall fabric and feel of Downtown.

### 2. 2. 3 Rhythm

The defined rhythm of Downtown Ozark should be maintained along the street frontage by adhering to uniform lot widths, building widths, and window spacing.

- New infill buildings and structures should maintain the rhythm through proper repetition of details and orientation to the street.
- Vertical elements, entrances, lighting, and other street furnishings can also develop the rhythm of a specific block.



Architectural elements such as the windows, awnings, and cornice detailing provides visual contrast and creates rhythm among the building blocks.



Large variations in building height, shown here, can detract from the rhythm of Downtown buildings.

## 2.2.4 Alterations

Encourage removal of inappropriate alterations or additions that disrupt the fabric of the Storefront Zone. It is possible that non-historic and new construction can complement the building fabric that has developed; therefore some alterations may not need to be removed. Decks, ADA structures, and other 'detachable' alterations can be utilized, but should be as unobtrusive as possible and located on the rear or sides of the building.

As a rule, during rehabilitation all alterations to the Upper Façade zone should be removed. Alterations in this zone can significantly change the appearance of the face of the building. This includes all signs and lighting as these should be restricted to the Storefront Zone. Avoid removing or altering any historic material or significant architectural features. Care should be taken during the removal process due to the possibility of damaging original elements hidden behind the alterations. When disassembly of a historic element is necessary, use methods that minimize damage to the original materials.



### 2.2.5 Masonry

Masonry is typically the preferred façade material for downtowns. Most existing construction will utilize some masonry. In most instances metal and wood siding is not appropriate choices for the downtown building fabric. These types of siding provide: harsh lines, stark contrast, and no relief or depth to the buildings. If wood was the historic material, it may be restored.

- Maintain the original color and texture of masonry walls. Stucco or paint should not be removed from historically painted or stucco masonry walls. Likewise, paint or stucco should not be applied to historical masonry walls. Unpainted masonry should remain natural, not painted or sealed.
- Clean masonry and mortar only when necessary to limit deterioration or to remove heavy soiling. Sandblasting, caustic solutions, and high-pressure water blasting should not be used. These methods erode the surface, accelerate deterioration and will permanently damage the brick.
- Masonry restoration, particularly on historic structures, should be done with great care.
- If the masonry has been painted or stained, a minimally intrusive removal process should be used.
- Damaged masonry should be repaired or replaced with similar color, texture, and style masonry products. Re-point mortar walls when there is evidence of disintegrating mortar, cracks in mortar joints, loose bricks, or moisture retention in the walls. The new mortar should duplicate the old mortar in composition, bonding strength, profile, color, and texture. Do not use cement mortar in brick construction; cement is far too hard and will cause spalling and cracking of the softer bricks.
- Re-pointing should be done with an appropriate mortar material with a consistent color across the entire façade and all elevations.
- Masonry replacement and/or repair should only be done with appropriate materials.



A worker shown here repairs the tuckpointing on a brick building facade.



This image shows mortar used to cover up brick on a building facade. This improper type of “repair” is not recommended.



This building has siding used as a dominant building material. Although the siding is in good condition it does not compliment the brick and masonry facade characteristic of Downtown.



The rear facade of buildings along 3rd Street, shown here, are generally deteriorated.

- Portland cement as a patch for masonry is unacceptable.
- If a historic façade has been covered with metal or wood siding it should be removed. Exposing the underlying brick masonry will help re-establish the character of the building and contribute to the visual continuity of the block. Metal cladding or siding also hides interesting details that can enhance building identity. If, after removing the covering material, portions of the original must be replaced, use a material that is similar to the original in color and texture.

### 2. 2. 6 Downtown Color Palette

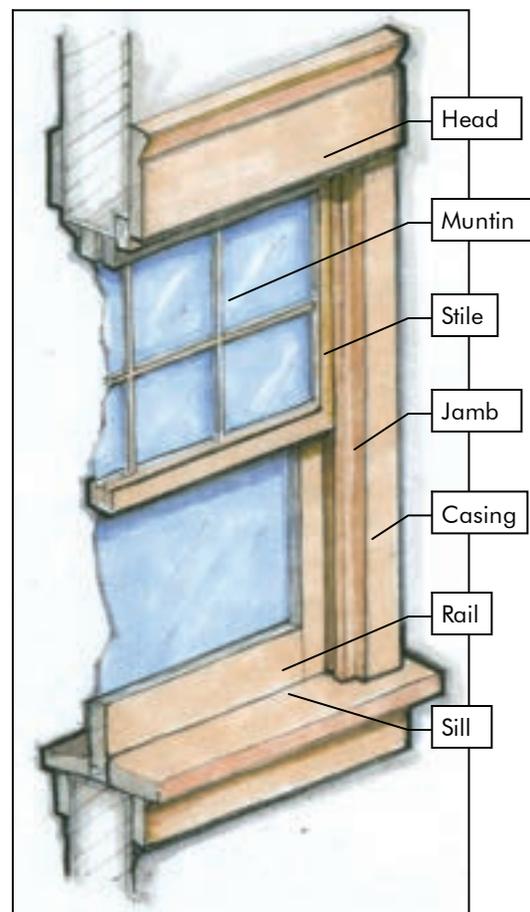
As noted in this plan previously it is important to complement the building materials which are historically present throughout Downtown, mostly being earthtone colors. Below is a sample color palette of colors which could be used for replacement or new building materials to complement the character of Downtown.



## 2.2.7 Windows

Windows are a major feature of the building exterior and vary with each building style. Windows have a proportional relationship to the structure as a whole, and they also have a decorative function. The shape and glazing pattern of windows on a building may be one of the principle characteristics in identifying its historic period and style. Thus, if original windows are removed and replaced with incompatible modern windows, the basic character of the building will be altered substantially.

- The number, size and locations of existing window openings should be retained. Do not “block-in” windows to reduce the size of the window opening or to fit stock window sizes. New window openings should not be added on elevations that are subject to view from a public street.
- Retain and repair window frames, sash, decorative glass, panes, sills, heads, hoodmolds, moldings, and exterior shutters and blinds whenever possible. If replacement of any window part is necessary due to deterioration, the replacement should duplicate the material and design of the older window. Replacement sash of wooden windows, for example, should be made of wood. If duplication of the original window or window part is not technically or economically feasible, a simplified version of the original may be acceptable as long as it has the same size and proportion.
- Modern window types that are inappropriate include large picture windows, casements and bow windows, unless they are original to the building.
- Do not install shutters on windows that did not originally have shutters. Replacement shutters or blinds should be sized to cover the entire window when closed. In other words, the shutter should measure the full height of the window and half its width. Fasten shutters to the window frame and not to the siding.
- Inappropriate modern window features such as plastic and metal awnings or fake, non-operable, synthetic shutters and blinds distract from the historic appearance of a building and should not be used.
- Storm windows should have wooden frames, or if metal, should be anodized or painted to blend with the trim. Interior, rather than exterior, storm windows are recommended.
- Typical upper windows are vertically oriented and uniformly spaced across the building



This diagram shows the main components of a standard window.

front. This rhythm of upper story windows is an important unifying feature of Downtown.

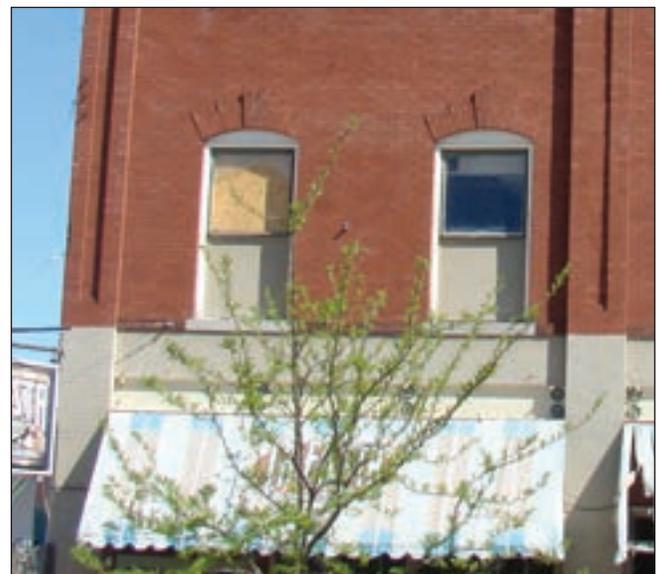
- Masonry infill, wood panels, or mismatched windows should be removed and replaced with appropriate materials.
- If the original window still exists, it should be restored to serviceable condition when possible.
- Replace only missing portions of original elements where feasible. Sometimes trim elements and other materials must be removed for repair. Always devise methods of replacing the disassembled materials in their original configuration.
- Installation of interior storm windows should be considered.
- If the existing window is beyond repair an appropriate replacement window of the same size and profile should be installed.
- If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the window. Any windows covered by masonry infill, wood panels, or mismatched windows should be removed.
- Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be considered.
- Window shades or curtains in colors that coordinate with accent trim should be encouraged.

### 2. 2. 8 Architectural Details

- Replacement of missing cornices or architectural elements should use accurate duplications of original features. In some cases, an entire detail must be reconstructed. In the event that



This building shows a good example of well-maintained and unique second story windows.



This building shows boarded-up second story windows. This can create a feel of vacancy in Downtown and should be repaired properly.

replacement is necessary, the new material should match the original in design, color, texture, and other visual qualities. Photographic evidence is a good source for research.

- If the cornice is missing, a similar cornice of like size and scale should be installed.
- If no evidence exists as to form and detail, the reconstructed cornice should be as simple and non-intrusive as possible.
- If the cornice is intact it should be repaired and maintained as required.
- Where architectural details have been removed, refer to historic photos for details to use as patterns for new designs.
- Where exact reconstruction of details is not feasible, consider developing a simplified interpretation of the original, in which its major forms and lines are retained.

### 2.2.9 Entrances

- Recessed entries help invite customers into the store.
- Maintain recessed entries where they exist. These areas provide protection from the weather, and the repeated rhythm of these shaded areas along the street helps to identify business entrances.
- Avoid entrances that are flush with the sidewalk.
- If the original recessed entry has been removed, consider establishing a new one. Use doors with large panes of glass where feasible, these will improve the visibility of the business to outside viewers.
- Consider using an accent color on the door.
- Center signs over door.



Unique and attractive architectural details found on the Historic Courthouse building.



Business entrance with a sign to highlight entry. Problems may arise from the door opening onto the sidewalk.

## 2.2.10 Awnings

Awnings used in the storefront zone provide shade for merchandise, shelter for pedestrians, and bring a colorful accent to the building front that can be changed frequently and without great expense. The following suggestions enhance appropriate use of awnings and improve Downtown aesthetics:

- Mount the top edge to align with the top of the transom, or to align with the framing that separates the transom from the main display window. This will help strengthen the visual continuity of store fronts.
- Roll-up awnings were a common site on historic storefronts and can be used following a similar approach to the original application. If a roll-up awning is not operable, the awning should follow the shape of an operable awning.
- Like the storefront, awnings should be confined to the extent of the original storefront opening.
- Awning colors should coordinate with the color scheme for the entire building.
- Awning signage or lettering should not be allowed where another flush-faced sign exists.
- Awnings will wear and should be acknowledged as an operating cost of doing business which can be changed every few years for a fresh look.

## 2.3 Additional Guidelines for Downtown Commercial Buildings

### 2.3.1 Storefronts

#### Entrance:

- The entrance door should be recessed to emphasize the entry, provide a bit of shelter and remove the open door from the path of pedestrians on the sidewalk. These areas also create a rhythm of shaded areas along the street to help to identify business entrances.
- If the original recessed entry has been removed, consider establishing a new one.
- The recessed entrance door should also be ADA compliant.



Awnings with deteriorated conditions such as the one shown above can severely detract from a building's visual appearance.

- The door should provide a view into the building as well as a sense of openness. Solid doors should be avoided.
- Consider using an accent color on the door.

**Windows:**

- Preserve any of the large panes of glass that make-up the original store front, if they still exist. These transparent surfaces allow pedestrians to see goods and activities inside.
- Any new or replacement storefront should be built of similar materials compatible with the original façade design and craftsmanship.
- Wood framing similar to the original is preferred, but metal framing with the appropriate historic profile is acceptable.
- Clear insulated glass with 'Low-E' coating is a good choice for replacement storefronts.
- Tinted or reflective glass and interior reflective films should not be used on the storefront.



An attractive sign board in front of a Downtown store.



An attractive storefront with a unique color scheme and good use of transom windows.

**Spandrel Panels:**

- Maintaining the original spandrel panel, if it exists, is preferable, but if the panel is missing, reconstruction using old photographs as a guide is acceptable.
- Coordinate the color scheme of the spandrel panel with other façade elements.
- If original design information is not available, another option is to design a simplified panel using appropriate materials such as painted wood or metal.

**Transoms:**

- These bands of glass are found on many buildings and often align at the same height in a block. Maintaining this line will help to reinforce a sense of visual continuity for the street.
- When transoms are covered and original moldings and window frame proportions are concealed, the impact of the store front is weakened. If the interior ceiling is now lower than this glass line, move the dropped ceiling back from the window to maintain its historical dimensions.
- Some transoms have hinged panels to allow natural ventilation. Restore these to working order where feasible. Used in combination with ceiling fans these operable transoms can be very effective in improving comfort levels when full air-conditioning is not necessary.



This store looks vacant and uninviting. Windows should be open and welcoming; this use of blinds or boards should be avoided.



An attractive and unique Downtown storefront.

### 2.3.2 Signage

For a successful business environment each shop must have its own identity while at the same time maintaining the continuity of the district. Appropriate signage identifies the business without detracting from the architecture of the building and the fabric of Downtown. Sign types and their locations should be kept simple and consistent for ease of public awareness. Signage should be restricted to the storefront or rear entrances of a building. The following guidelines will help enhance this aspect of Downtown Ozark:

#### 2.3.2.1 General Signage Design Issues:

- The sign should be a part of the building design. Do not hide building features. Find an element or space that will naturally accommodate the sign.
- The size of the sign should be of an appropriate scale for the building and street. Large signs should not be needed as the signage in a downtown area is more oriented to the pedestrian than the motorist.
- Flush-mounted signs positioned to fit within architectural features is preferred. This type of signage will help reinforce horizontal lines along the street.
- Locate flush signs so they do not extend beyond the outer edges of the building front.
- The material and color of the sign should complement the building materials and color scheme.
- The message of the sign should be simple and easy to understand. The name of the business and type of business should be sufficient. A logo or symbol of the type of business could substitute for a “type of business” message.



Illuminated and pylon signs such as this should be avoided in the Downtown area.



Signs should be well-maintained and updated.



An example of a unique business sign. It is perceived that the rustic and weathered look is intended, but the sign is still readable.



An example of a faded and unattractive sign and sign support.

- Rooftop, blade, pole, abandoned, neon, electronic message boards, and billboard signage should not be allowed or severely restricted.
- Place signs near the business entrance, to guide a customer's eyes to the door.
- Where several businesses share a building, coordinate the signs by aligning several smaller signs or grouping them onto a single panel as a directory to make them easier to locate. Use similar forms or backgrounds for the signs to tie them together visually and make them easier to read.
- Mount signs so they will not obscure any architectural details.
- Sign materials should be compatible with the façade materials.
- Good craftsmanship will pay off through longer use of a sign, and will convey a stronger image to the public. Select high quality materials. Signs are exposed to extreme weather conditions, and a deteriorating sign presents a poor image to customers.
- Encourage the use of "custom" designs that portray a business as being unique. Mass-produced signs, especially rectangular plastic panels with internal lighting, fail to make a lasting impression.
- Illuminate signs in such a way as to enhance the overall composition of the façade.
- External lighting cast from period style, non-intrusive fixtures is preferable to internal sign lighting.

### 2.3.2.2 Style and Location of Signs:

Projecting Signs: Projecting wall signs that give the name or the logo of the business or product sold, such as a watch for a jeweler or a drug company logo. These signs should have the following characteristics:

- Material: Unframed painted wood or metal panels hung from painted wall brackets. Wood signs with carved or sandblasted designs that are painted are also appropriate.
- Color: Sign colors should complement the paint scheme and masonry color of the building.
- Lighting: Non-illuminated or externally illuminated with spotlights.
- Location: Bottom of sign should be 8'-0" above the sidewalk and below the building parapet or the second floor windows.
- Locate projecting signs along the first floor level of the façade. If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the window.
- Use symbols in projecting signs; these are more easily identified and remembered and will add interest to the building.

Wall Signs: Painted signs on the brick wall above the windows or on the side of the building. The old faded signs on the sides of the buildings are commonly called "ghost signs" and should be preserved wherever possible. Wall signs should have the following characteristics:

- Material: Painted on brick wall or on wood or metal panels. The signs painted on brick were usually white lettering on black backgrounds unless they advertised a product, such as Coca-Cola or Wrigley's, which were multi-colored.
- Lighting: Natural light or externally illuminated with spotlights.



A projecting sign such as this is not complementary to the Historic Downtown.



An example of a painted wall sign.

- Location: Many of these signs were in recessed brick panels above the storefront windows. There are many examples of these in the historic photos and they still exist beneath paint and metal or wood panels.
- Wall signs should not be located above the building parapet.

Window Signs: Painted or foiled lettering on the display window glass. These often advertised a doctor, dentist or attorney. Window signs should have the following characteristics:

- Material: Painted lettering, or gold or silver foil lettering. Lettering colors should complement the paint scheme of the building.
- Lighting: Natural lighting or the inside lights of the building.
- Location: On the glass of the entry door or the display window at eye level. These signs were fairly simple and did not attempt to dominate the window. The merchandise inside is what you are trying to sell. Window signs are also appropriate in second floor windows to identify second floor businesses.

Awning and Canopy Signs: Awning or canopy signs should have the following characteristics:

- Material: Lettering silk-screened on awning fabric or painted on wood or metal sign panels.
- Location: Six to eight inch high lettering on the front valence of a fabric awning or a hung sign panel. These panels should be a maximum of twelve inches high.
- Mount the top edge to align with the top of the transom, or to align with the framing that separates the transom from the main display window. This will help strengthen the visual continuity of store fronts.
- Roll-up awnings were a common sight on historic storefronts and can be used following a similar



An example of a business window sign.



An example of an awning sign in Downtown Chillicothe, MO.

approach to the original application. If a roll-up awning is not operable, the awning should at least follow the shape of an operable awning.

- As with the storefront, awnings should be confined to the extent of the original storefront opening.
- Awnings should be trapezoidal in profile with closed ends, not rounded or curved, and a consistent color.
- Awning colors should coordinate with the color scheme for the entire building.
- Awning signage or lettering should be limited to the hanging vertical flap of the awning and be complementary in color to the building.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Awning signage or lettering should not be allowed where another flush faced sign exists.
- Awnings will wear and should be acknowledged as an operating cost of doing business. They can be changed every few years for a fresh look.
- Aluminum and/or steel awnings and structures are not original building elements and typically detract from the overall appeal of the facade. These awnings should be removed and points of attachment repaired on the building facade.

Sidewalk signage: Symbolic signage, such as barber poles, were often set on the sidewalk. Sidewalk placards were also used to advertise merchandise. Sidewalk signage should have the following characteristics:

- Material: Painted wood or metal.
- Lighting: Natural illumination. Do not internally illuminate.



An example of an attractive street pole banner.



An example of a wall mural in Downtown Chillicothe, MO



An attractive sidewalk sign board.



Illuminated signs such as this should be avoided.

- Location: At the edge of the sidewalk or at the building face. Most signs of this type should be portable so that they can be taken inside at night or during special activities such as parades.
- Signboards under the awning intended to assist pedestrians should be a limited, uniform size and complement the awning and building.

**Signs to Avoid:**

- Flashing or animated signs, or signs with moving parts or the effect of movement
- Internally illuminated signs or awnings
- Signs that make sounds or music

### 2.3.2.3 Number and Area of Signs:

**Principal Business Signs:** Signs that identify the name and nature of the principal business should be limited to two per building storefront. These signs could be any combination of the sign types discussed above.

**Auxiliary Signs:** In addition, each business could have a sign stating hours of business and an “open” sign. These should be limited to two square feet each.

**Side Street Directories:** Side walls of corner buildings could be used for directions to side street locations such as parking, churches and businesses. These signs should be of uniform size and design, and be mounted below a directional arrow. A suggested sign panel size would be 12 inches high by 48 inches long with 6 inch high lettering.

**Sign Area:** The aggregate area of all principal signs should not exceed 100 square feet, except buildings with front wall area of 1000 square feet or more, where the aggregate sign area should not exceed approximately 10% of the front wall area.

**Lettering Size:** The size of lettering or any sign type should not exceed 12 inches high, except for the first letter of each word, which should not exceed 18 inches high.

**Lettering Style:** Because the historic signs spanned a long time period, a variety of lettering styles existed together. Lettering style for new signs could be either simple block letters or more elaborate lettering



A unique sign design and lettering. The business name over the logo may be hard to read for vehicle travelers.



An example of a well-lit business sign in Historic St. Charles, MO.



An example of a lighted sign board. There is too much text on this sign to make it easily read.



An example of lighting to avoid from Downtown Columbia, IL.

### 2.3.3 Lighting

Buildings should be interesting to pedestrians and motorists at night, as well as by day. A well-lit storefront or rear façade creates a positive impression about Downtown. The following lighting conditions can be implemented to enhance the attractiveness and safety of the Downtown:

- Use lighting as a design element to draw attention to the entire building, not just the sign.
- Any lighting at the storefront should be used to accent the entrance, signage, or architectural elements as well as provide light for safety and security.
- Light fixtures should be the lowest wattage possible and of a concealed, simple, and non-intrusive design or a style that is appropriate to the period of the building.
- Sign lighting should be balanced in color and intensity with light in display windows.
- Warm-colored light is preferred for all exterior lighting, since this is more pleasing to the eye, and will more easily draw attention to window displays.
- Neon lights and cool fluorescent lights should not be used.
- Lighting on rear façades should provide illumination at the entry door as well as along the pedestrian path from the parking area. This lighting should be similar to the lighting in the front of the building.



An attractive rear building facade and entrance in Historic St. Charles, MO.



An inviting rear building entrance in Historic St. Charles, MO.

### 2.3.4 Rear Elevations

The rear elevation typically faces an alley or parking lot and provides access for deliveries and maintenance. In some cases customer parking is provided behind a building and entry to the business through the rear is desirable. Attention to the appearance of the rear elevation can be extremely important to the quality of the customers' shopping experience. Consider how image can be improved here, while accommodating service functions.

#### 2.3.4.1 Entry Door:

- The rear door will no longer be just for service but should project a sense of openness and welcome.
- Customers might also feel a loyalty or sense of 'special access' by using this door and the business can build on this loyalty by catering to that customer and improving that experience.
- A new door and hardware with a large area of glass may be considered.
- A small canopy or awning can provide some form of shelter.

#### 2.3.4.2 Upper Rear Façade:

- The upper rear façade elements should be treated similar to the front. Too often this is a façade that is neglected and allowed to deteriorate.
- Windows should be restored or replaced.
- Gutters and downspouts should be in good repair and painted.

- Use materials and colors that coordinate with the main façade so customers will learn to recognize both entrances are related to the same business.
- Use a smaller version of the front sign to identify the rear entrance.
- New exit stairs and balconies can enhance the marketability of second story space, especially when these lead out onto parking lots located on the back side of the building. Encourage installing new stairs that comply with current building codes.

#### **2.3.4.3 Fences:**

- Fences should be designed to harmonize with the surrounding structures in both scale and color.
- Look to city ordinance and zoning code for limitations and regulations on size.
- Some materials which may be appropriate include masonry, wood, ornamental metal and wrought-iron.

#### **2.3.4.4 Trash Dumpsters, Ancillary Structures, and Utilities:**

Sensible, yet firm enforcement of the city's building and nuisance codes will be required and should be a priority throughout the Downtown.

- Waste receptacles should be placed in an enclosure or behind a screen.
- Use landscaping to help screen waste receptacles and dumpsters.
- Enclosures and screens should harmonize with the surrounding buildings in scale and color.
- Landscaping can also be used to screen air-conditioning condensers and utility transformers.



A wood privacy fence used to screen a trash collection/service area.

- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Any ancillary structures should match the surrounding buildings style and scale. These structures must be maintained well.
- Keep electrical service boxes and conduits in good repair and painted.
- Encourage using a color scheme on these screens that matches that of the rest of the building.

## 2.4 New Construction Concepts

The construction of any new structure within Downtown is important because the new structure needs to be compatible with existing buildings. New construction should seek to harmonize with the visual characteristics of the neighborhood and address elements of scale, design quality, and massing; the relationship of the new construction to existing nearby buildings.

### 2.4.1 What's Acceptable?

The following concepts consider additions to existing buildings as well as entirely new infill construction. The concepts are intended to identify a range of design options that will complement existing buildings, not to dictate specific styles or features. The design of new construction should generally focus on massing, rhythm and directional emphasis, materials, and building elements.

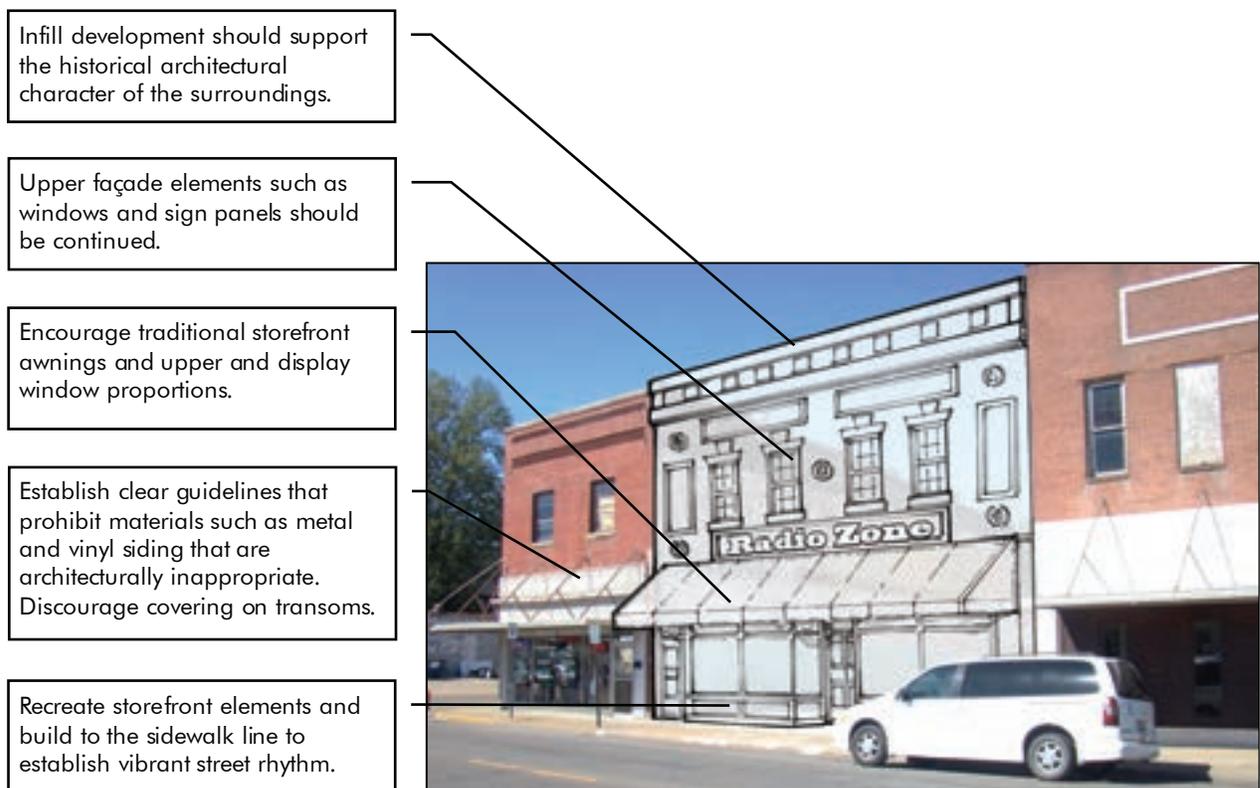
Massing and rhythm are defined by the relationship of a building to the open space along the street frontage, the relationship of solids to voids on building façades, and the relationship of the entrances and porch projections to the street. The directional emphasis (whether vertical or horizontal) of new construction should relate to that of the neighboring buildings. The defined rhythm of Downtown Ozark should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing. Materials and other building elements speak to the style of the building and should be complementary of surrounding structures.

An illustration depicting concepts for designing new Downtown commercial infill buildings is shown on Page 39. The general concepts provided in this illustration include:

- Designs should be considerate of the traditional building elements as described in Section 2.1 or on nearby existing buildings that contribute to the historic context of Downtown Ozark. Often a simple design is best, using three basic elements: a unified paint and color scheme, an awning, and non-intrusive signage.
- Properly orient the building to the street and carefully consider the relationship to nearby buildings.
- Emphasize horizontal features that can align with other buildings to reinforce the rhythm of the block. Vertical elements, entrances, lighting, and other street furnishings can also help develop the block rhythm. Include architectural details sparingly, but properly repeated.
- Restrict off-street parking areas to the side and rear of the building. Typically, a downtown building is built up to the sidewalk and it is important for an infill building to maintain this building line.
- Encourage multi-story construction to maintain the building roof line and to accommodate mixed-use development that reserves the ground floor for retail uses.

## 2. 4. 2 What's Not Acceptable?

Avoid recreating historical styles or themes with new construction. This practice can create a "theme park" type of atmosphere. While new buildings can be inspired by past design, the new building should feel genuine and not a creation of a false past. The building material used for new construction has a tremendous affect on the overall appearance of the building and how it compliments the Downtown context. Building materials that should be avoided in Downtown Ozark includes plastic paneling, shiny metals, mirror glass, metal and vinyl siding.



This diagram shows a before and after look at what can be done for a building infill situation in a Downtown environment.

## 2.5 Franchise Architecture

To maintain the unique atmosphere in Downtown Ozark, branding buildings in the style of a company should not be allowed. Large franchises and national chains typically have a “downtown-style” in addition to their trademarked brand. These styles are more fitting to Downtown, as opposed to a highway corridor.

The company’s “downtown-style” is particularly important if a franchise store is to locate in an attached Downtown building of historical nature. This is an infrequent occurrence as Downtown buildings are typically not suitable or attractive to franchise stores. However, the City should be prepared if a franchise store prospect desires a Downtown location. These stores can still be complementary to the historical fabric of Downtown. The design concepts for new construction provided in Section 2.4 on Page 38 should apply, and other aspects such as parking requirements, pedestrian-oriented signage, building setbacks, and building lighting may require consideration. Store owners should be able to adapt their brand to create a complementary Downtown building.



A good example of franchise architecture altered for a downtown environment.



An example of housing near Historic Downtown Ozark.



An example of housing near Historic Downtown Ozark.

## 2. 6 Design Guidelines for New and Existing Residential Buildings

### 2. 6. 1 Building Orientation

The setback and orientation of new buildings in historic districts should align with neighboring historic buildings. Within the service area, principal elevations of buildings characteristically face the street with a strong sense of entry. New buildings with main façades and entrances oriented to the side yard, or new buildings having a courtyard arrangement are not appropriate.

### 2. 6. 2 Building Materials

The exterior materials used in new construction should be compatible with historically appropriate materials of neighboring buildings or the district as a whole.

### 2. 6. 3 Building Design Elements

The various individual elements of a building—the roof, windows, doors, porches and trim—should be carefully integrated into the overall design of new construction. These elements also should complement those on neighboring buildings. The shape and pitch of the roof should be considered. Window and door proportion, size, design, and pattern of spacing between the openings should be compatible with historic treatments of windows and doors in the district.

The discussion of building materials and elements described in Section 2.2 applies for residential building as well.

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## 3.0 Sustainable Design

The construction of sites and buildings has a significant impact on the natural environment. The operations of a site and/or a building, can also affect the air, land and soil of the Downtown. Sustainable Design measures seek to lessen the impact on the natural and built environment. Such design efforts also aim to increase the efficiency at which buildings operate, in regard to energy use and operating costs. The design process is comprehensive, beginning with site selection and orientation; through specification of sustainable materials to energy efficient operating systems. Sustainable Design properties should be considered with private buildings, as well as with the public streetscape.

Downtown Ozark is a built environment of many historic buildings, modern buildings, public streets, parking lots, a few vacant lots and other open space. Sustainable design measures can be applied to existing buildings, new buildings, and streetscape and site improvements. The U. S. Green Building Council (USGBC) has become the leading organization in developing standards for sustainable design for buildings. The USGBC's certification system is known as Leadership in Energy and Environmental Design (LEED). The majority of LEED designated buildings are new construction projects; however the USGBC has also developed standards for the upgrade of existing buildings.

Sustainable Design is a broad and encompassing initiative which strives to create a built environment which is good for both man and nature. The following recommendations only introduce the basic fundamentals of sustainable design regarding downtown buildings and environments. For additional information beyond these guidelines, numerous resources exist, including:

- USGBC ([www.usgbc.org](http://www.usgbc.org))
- Whole Building Design Guide ([www.wbdg.org](http://www.wbdg.org))
- American Society for Testing and Materials International (ASTM)
  - ASTM E2432— Standard Guide for General Principles of Sustainability Relative to Buildings ([www.astm.Standards.e2432.htm](http://www.astm.Standards.e2432.htm))
  - Sustainable Sites Initiative (<http://www.sustainablesites.org/>)
  - Grow Native (<http://www.grownative.org/>)
  - Smart Growth America (<http://www.smartgrowthamerica.org>)



A good example of native prairie style plantings in an urban environment.

### 3.1 Fundamentals

Sustainable design measures are constantly changing, however there are six fundamental principles which constitute sustainability:

- *Optimal Site Potential:* Consider site selection, building orientation and existing natural features of a site, including topography, drainage, landscape and natural habitats. The rehabilitation and reuse of existing buildings should always be evaluated as an alternative to new construction.
- *Efficient Use of Water:* The design and use of water systems in a building maximize efficiency and recycle water for on-site use when feasible. Site design should seek to reduce storm water run-off from the site. Use Best Management Practices (BMP) to limit storm water run-off, clean storm water, and prevent suspended pollutants from reaching the sewer system.
- *Environmental Materials and Resources:* Utilize building materials with a high percentage of recycled content or contain rapidly renewable materials such as cork flooring, bamboo cabinetry, wool carpeting, etc. Specify or use materials or items which are manufactured within proximity to the project site. Ideally, this proximity is no more than 500 miles.
- *Optimal Energy Use:* The operation of a site and building identify methods for increased energy efficiency or use renewable resources such as solar or geothermal energy.
- *Interior Environmental Quality:* Identify methods for creating a healthy environment, and increasing the comfort of building users. Proper ventilation, use of natural light, and moisture control are a few methods to ensure a quality interior space.
- *Optimal Operations and Maintenance Methods:* Utilize building systems, furnishings and finishes which will have minimal operations and maintenance needs. Such systems will require less energy, less water, and can be maintained with natural cleaners which are non-toxic to the environment or occupants.



An example of permeable paving used for parking areas.



Solar panels can be used to provide an energy source for lighting, building use, signals, and even automobiles.

## 3.2 Elements

Sustainable design elements are extensive. The following list seeks to introduce only a few recommendations which are applicable to Downtown Ozark:

- **Parking and Service Areas:** Minimize storm water run-off by using pervious pavement materials such as pervious paver systems or pervious concrete. Such systems will allow storm water to percolate into the soil and not into the public storm water sewer system.
- **Building Materials:** Utilize materials which are composed of recycled materials or manufactured from rapidly renewable materials, which are made from plants that are typically harvested within a 10 year cycle. Examples include: bamboo flooring, linoleum flooring (made of wheat flour and linseed oil), cotton batt insulation, and wheat board cabinetry. Recycled bricks from demolished buildings should also be used for new building construction or restoration projects.
- **Alternative Transportation:** Promote by providing secure bicycle storage and changing/shower facilities for employees.
- **Solar Energy Alternatives:** Install solar panels to supplement the power system for commercial and residential buildings. Utilize prefabricated solar water heaters to provide the majority of the hot water needs for buildings.
- **Stewardship:** New wood products, including construction lumber, should be certified by the Forest Stewardship Council, which promotes responsible forest management.
- **Lighting:** Utilize dark sky lighting on light poles which minimizes excessive lighting, which affects night sky viewing and the migratory patterns of birds. Flags which require lighting should be lit from the top shining down on the flags instead of being lit from the ground, projecting light into the sky.
- **Operations:** Use timers on public fountains and lights in non-essential areas to shut off lights after 1:00 a.m., in order to reduce energy consumption.
- **Landscaping:** Plant native landscape materials which can survive on natural rainfall once established.
- **Street Furnishings:** Specify site furnishings such as benches, waste receptacles, bollards, and planters which are made from recycled plastic materials.
- **Water Conservation:** Capture rain water runoff from roofs in rain barrels for irrigation use or direct to rain gardens on site. Inside buildings, consider waterless urinals or low flow water closets to limit potable water use.



Bike lanes and sidewalks can be used to create multi-modal road corridors. This kind of design element can help create roadways which are safer for pedestrian and bicyclists.

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## 4.0 Proposed Building Improvement Concepts

### 4.1 Proposed Building Improvement Concepts

Within the DREAM process public input was obtained through interviews and public meetings, as well as previously conducted focus groups and surveys regarding Downtown Ozark. The overall consensus was to build on Ozark's history and maintain the historic character. Whenever possible historic photos were used to emulate this style on the illustrations included in this section. The subject buildings chosen by the community for design examples are located along the east side of 3rd Street between Elm and Brick Street. The building rear elevations were chosen for this exercise. These buildings enjoy mixed occupancy, including a number which are service businesses. Many suffer from deferred maintenance and inappropriate alterations. More retail shops and façade improvements are needed for these buildings. The structures are representative of the rear building elevation of Downtown and the buildings located along 3rd Street, which at one point in Ozark's history was not the main thoroughfare through Downtown. The buildings located along 3rd Street are important for Downtown Ozark because they provide the look and feel that is collected by visitors as they approach the Downtown Square. It is important to create a feeling of welcome, attractiveness, and ownership. Currently the building facades along 3rd Street display neglect, clutter, and vacancy.

Concept illustrations for these structures are depicted on the following pages. The images depict the existing conditions and ideas for the potential rehabilitation with a list of the recommended improvements. The design concepts presented are only suggestions to encourage and assist property owners, the city, and other interested parties with revitalization efforts. While these recommendations are for 3rd Street, they are applicable generally for many other buildings in Downtown.



A view of the intersection of 3rd Street and Church Street, the main entry to Downtown Ozark. This intersection should be improved to attractive visitors to Downtown.

#### **4. 1. 1 Rear Building Facades (Elm to Church St.)**

Below is a list of general potential improvements to the rear building facade

- Remove frame, metal & shingled canopies.
- Restore/insert masonry veneer where possible.
- Restore/insert cornice where applicable.
- Restore/insert storefront windows and entry where public access is available.
- Utilize canvas/fabric awnings whenever possible.
- Use compatible paint colors.
- Match brick/elevation color between first and second floor when possible.
- Paint utility infrastructure with a matching color to adjoining wall to soften negative impact whenever screening is not possible.
- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Utilize ornamental sign lighting.
- Install lights and business signage of appropriate scale and design.
- Consider installation of other appropriate decorative metal ornamentation for upper façades.
- Streetscape of lights, planters & site furnishings.
- Assure sidewalks, curb ramps, and business entries are compliant with ADA accessibility standards.
- Place overhead utilities below ground where and when financially feasible.
- Highlight parking areas with landscaping to soften look.

Rear Building Facades (Elm to Church St.)

**WEST WINERY**

Renovate store front window to increase aesthetic and commercial value.

Place overhead utilities underground.

Appropriately repair brick and masonry facade with tuck pointing. It is important to match brick and mortar style and color to the existing masonry.

Add streetlights that match existing Downtown street lighting along this stretch to show connection to Downtown.

Screen airconditioning units and trash bins with fencing and/or landscaping.

Building facade should be a color which is compatible with surrounding buildings. Installation of a brick or stone veneer would be desirable.

Church Street

Elm Street

It is recommended to replace wood shake mansard style upper facade with a more compatible facade style. Remodeled facade should match existing context of downtown.

Add awnings to building fronts to add color, provide aesthetic appeal, and provide shade.

Remove unattractive signs from building facade that detract from the visual appearance of the buildings.

Replace porch canopy roof covering with new or revitalized material. Material should be compatible with downtown buildings.

Replace and repair second story windows to make building feel and look less vacant.

Add ADA accessible sidewalk along entire stretch of 3rd Street.

Replace deteriorated railroad tie retaining wall with masonry unit retaining wall.

Green space between road and parking should be maintained and could include landscaped beds or potentially rain gardens where applicable.

Rear Building Facades (Church to Brick St.)



Renovate warehouse building to increase aesthetic and commercial value. Retail stores or restaurant could possibly be retrofitted into this warehouse space.



Place overhead utilities underground.



Repair or replace building drainage. Gutters and down spouts should be more decorative and color compatible with the building.



Add streetlights that match existing Downtown street lighting the along this stretch to show connection to Downtown.



Permeable pavement could be added to the parking area to increase aesthetic appeal.



Building facade should be a color which is compatible with surrounding buildings. Installation of a brick or stone veneer would be desirable.



Brick Street

Church Street

This large open space would be served as park space or infill commercial business. Public Parking for Downtown may also be acceptable, contingent on the design of the space. If parking is desired appropriate screening, landscaping, and street trees should be incorporated to soften the visual impact of a open parking lot.

Add awnings to building fronts to add color, provide aesthetic appeal, and provide shade.



Renovate or replace old windows and doors with replacements that are attractive and energy efficient.



Foundation plantings could be added here to soften the building facade.

Building foundation should be repaired and repointed to a compatible color or covered with stone veneer.



Add ADA accessible sidewalk along entire stretch of 3rd Street.



Site furnishings such as trash and recycling receptacles, planters, and benches should be provided near business entries.



This area could serve as a potential location for a gateway monument. Intersection improvements should not encroach on the drivers' vision triangle.



#### 4. 1. 2 Rear Building Facades (Church to Brick St.)

Below is a list of general potential improvements to the rear building facade:

- Remove frame, metal & shingled canopies.
- Restore/insert masonry veneer where possible.
- Restore/insert cornice where applicable.
- Restore/insert storefront windows and entry where public access is available.
- Utilize canvas/fabric awnings whenever possible.
- Use compatible paint colors.
- Match brick/elevation color between first and second floor when possible.
- Paint utility infrastructure with a matching color to adjoining wall to soften negative impact whenever screening is not possible.
- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Utilize ornamental sign lighting.
- Install lights and business signage of appropriate scale and design.
- Consider installation of other appropriate decorative metal ornamentation for upper facades.
- Assure sidewalks, curb ramps, and business entries are compliant with ADA accessibility standards.
- Place overhead utilities below ground where and when financially feasible.
- Continue streetscape elements such as lights, planters & site furnishings which compliment Downtown.
- Replace existing gravel parking lot with permeable paving.
- Highlight entry to Downtown with a gateway monument at Church and 3rd Streets.

#### **4. 1. 3 Rear Building Facades (Brick St. to Hall)**

Below is a list of general potential improvements to the rear building facade:

- Remove frame, metal & shingled canopies.
- Restore/insert masonry veneer where possible.
- Restore/insert cornice where applicable.
- Restore/insert storefront windows and entry where public access is available.
- Utilize canvas/fabric awnings whenever possible.
- Use compatible paint colors.
- Match brick/elevation color between first and second floor when possible.
- Paint utility infrastructure with a matching color to adjoining wall to soften negative impact whenever screening is not possible.
- Use solid wood or masonry partitions, lattice screens, or hedges to screen trash areas.
- Utilize ornamental sign lighting.
- Install lights and business signage of appropriate scale and design.
- Consider installation of other appropriate decorative metal ornamentation for upper façades.
- Assure sidewalks, curb ramps, and business entries are compliant with ADA accessibility standards.
- Place overhead utilities below ground where and when financially feasible.
- Continue streetscape elements such as lights, planters & site furnishings which compliment Downtown.

Rear Building Facades (Brick St. to Hall)



Discourage large unattractive business road signs. Encourage signs that have a more decorative look and fit the feel of the Downtown environment.

Hall Street



Brick Street



Renovate building to increase aesthetic appeal or demolish and infill with a new building that conforms with the existing Downtown context.

Potential infill building should have a more commercial and less industrial feel and look.

Clear overgrown vegetation and add attractive and inviting landscaping. This area can be used as an alcove for Downtown workers. Replace existing fence with decorative fencing.



Place overhead utilities underground. Remove obsolete radio tower.



Make improvements to upper facade. Facade improvements should match the existing characteristics of Historic Downtown Buildings.



Add streetlights that match existing Downtown street lighting the along this stretch to show connection to Downtown.

Where applicable and financially feasible replace old siding with materials such as brick or stone that conform with the existing character of Downtown.



Consolidate business parking entries where possible. Landscaped buffers between the street and business parking will create a more pedestrian friendly environment.

Remove or replace old windows and doors with replacements that are attractive and energy efficient.



Add awnings to building fronts to add color, provide aesthetic appeal, and provide shade.

Add ADA accessible sidewalk along entire stretch of 3rd Street.



Site furnishings such as trash and recycling receptacles, planters, and benches should be provided near business entries.



Improve landscaping at intersections. Intersection landscaping should not encroach on the drivers' vision triangle.

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## 5.0 Streetscape Design Concepts

The term “Streetscape” typically refers to exterior public spaces located between the building façades on each side of the street. DREAM proposes an organized streetscape with coordinated lighting, site furnishings, landscaping, and wayfinding. In some cases, streetscape recommendations can include the development of parks, plazas, or other public gathering areas.

Ozark has already invested an immense amount of resources into the streetscape surrounding the Courthouse. Decorative lighting is include throughout the Downtown streets with other streetscape elements such as pavers, landscaped bump-out planters, trees, and a mixed assortment of site furnishings such as benches and trash receptacles. In general, the Downtown Ozark streetscape design is attractive. However there are areas where it could be extended and components that could be added. Overall, the existing streetscape could coordinate better to help visitors define Downtown. One major area for improvement could be providing site furnishings such as trash receptacle, recycling containers, benches, and bike racks. These site furnishing should have a unified style which compliments the Historic Downtown character.

The various concepts that follow in this section are intended to help guide City staff and Downtown Ozark leaders as they pursue future public improvements.



A view of the streetscape surrounding the Courthouse Square.



Another view of the streetscape surrounding the Courthouse Square.

## 5.1 Design Coordination

An overall design approach will complement the existing streetscape as much as possible. Such an approach will help pull together the buildings, streets, parking areas, public spaces, and pedestrian walkways into a pleasing experience that encourages the visitor to explore. A Downtown should display a sense of order and rhythm through the repetition of building design features and street furnishings. In addition to the recommendations found in this report, there are other streetscape design issues and aspects that the City should address, including:

- Relocation of overhead power lines to underground conduit.
- Sidewalk replacement and updating to current Americans with Disabilities Act Accessibility Guidelines (ADAAG) or the Proposed Right-Of-Way Accessibility Guidelines (PROWAG). Ideally, overhead power lines can be buried as sidewalks are reconstructed.
- Irrigation and procedures that ensure proper maintenance of landscaping.

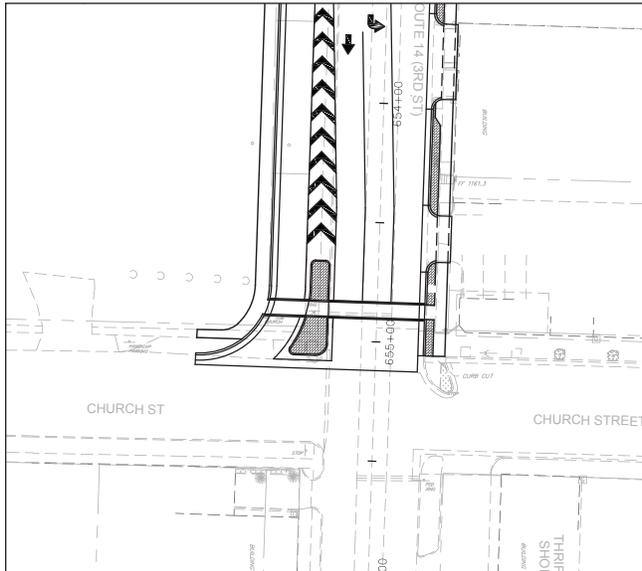
Without exception, all of the physical, public-owned elements of Downtown must be maintained in top condition. Streetscape fixtures should be reviewed on a regular basis and repairs or replacements made as timely as resources will allow. Maintenance costs required by a physically improved Downtown are unavoidable, but Ozark must avoid giving an impression of neglect and decline. The City must commit to enhancing Downtown and strive to keep it that way. Public streetscape enhancements demonstrate that the City is an investment partner in the ongoing improvement of Downtown.



An image of overhead utilities and a defunct radio tower. Coordination with roadway improvement could facilitate the removal and burial of these items.



A well-maintained planted corner bumpout in Downtown. The appearance is helped by the inclusion of an irrigation system at the time of installation.



This image shows engineer drawings for roadway improvements to 3rd Street.



An image of a deteriorated parking lot and business entry. Areas such as this should be repaved.

## 5.2 Infrastructure

Downtown cannot function without intact infrastructure, but this does not imply infrastructure should just be functional. The City should view infrastructure as a design element that can be enhanced aesthetically for the benefit of residents, visitors, merchants, and property owners. Recommendations for effective and attractive infrastructure include:

- Curbs should be in good repair and constructed of a consistent material. There should be no gaps or areas of uneven elevation along the curb line. At street intersections there should be ADAAG or PROWAG compliant ramps as noted in Section 5.3 to follow.
- Poorly working storm drains can create an undesirable situation at street intersections when storm water run-off collects in large pools. This condition makes pedestrian access virtually impossible and must be avoided.
- Street improvements such as pavement, curbs, or sidewalks should coincide with other public works projects to minimize street closings and costs.
- New sidewalks should transition smoothly into the same grade as street surfaces. New or replacement curb and gutter should be vertical curb design.
- Relocating overhead utility lines, although costly, can improve Downtown aesthetics greatly. Above ground utility enclosures should not obstruct the pedestrian walkway.

### 5.3 Accessibility

The U.S. Access Board is an independent Federal agency that has been established to monitor and issue updated accessibility guidelines for new or altered facilities covered by Americans with Disabilities Act (ADA) and the Architectural Barriers Act (ABA). These major civil rights laws prohibit discrimination on the basis of disability and establish design criteria for the construction or alteration of both public sector facilities and private sector facilities for public and commercial use. These guidelines address new construction and alterations and are referred to as the ADA Accessibility Guidelines (ADAAG). A recent addition the Proposed Right-Of-Way Accessibility Guidelines (PROWAG), is meeting acceptance by various federal agencies and will soon expand upon the ADAAG for public improvements.

Without the required curb ramps, sidewalk travel is dangerous, difficult, and in some cases, impossible for people who use wheelchairs, scooters, and other mobility aids. Ramps allow people with mobility impairments to gain access to sidewalks and pass through center islands in streets. Additionally, vision impaired visitors to urban downtowns require detectable warning strips along ramps leading to streets. When streets and roads are newly built or altered, they must have ramps wherever there are curbs or other barriers to entry from a pedestrian walkway. When new sidewalks or walkways are built or altered, they must contain curb ramps or sloped areas wherever they intersect with streets or roads. Resurfacing a street or sidewalk is considered an alteration for these purposes. However, filling-in potholes will not trigger the requirements. ADAAG and PROWAG provide for flexibility in many cases, such as Program Access; where an acceptable alternative route to a building may make use of existing ramps, provided people with disabilities must travel only a marginally longer route.



This image shows curb ramps and sidewalks with the intention of pedestrian crossing. stripping, colored pavers, or a similar treatment should be used to calm traffic and create a safer crossing for pedestrians.



A sidewalk along 3rd Street. This sidewalk is not ADA compliant, and the narrow right-of-way makes it unsafe for pedestrian travel.

## 5.4 Sidewalk Zones

As a streetscape project is contemplated, it is important that adequate zones in front of a building are maintained. The Building Zone, Pedestrian Zone, and Curb Zone all have unique characteristics that should be regulated to ensure that private elements do not adversely impact public improvements.

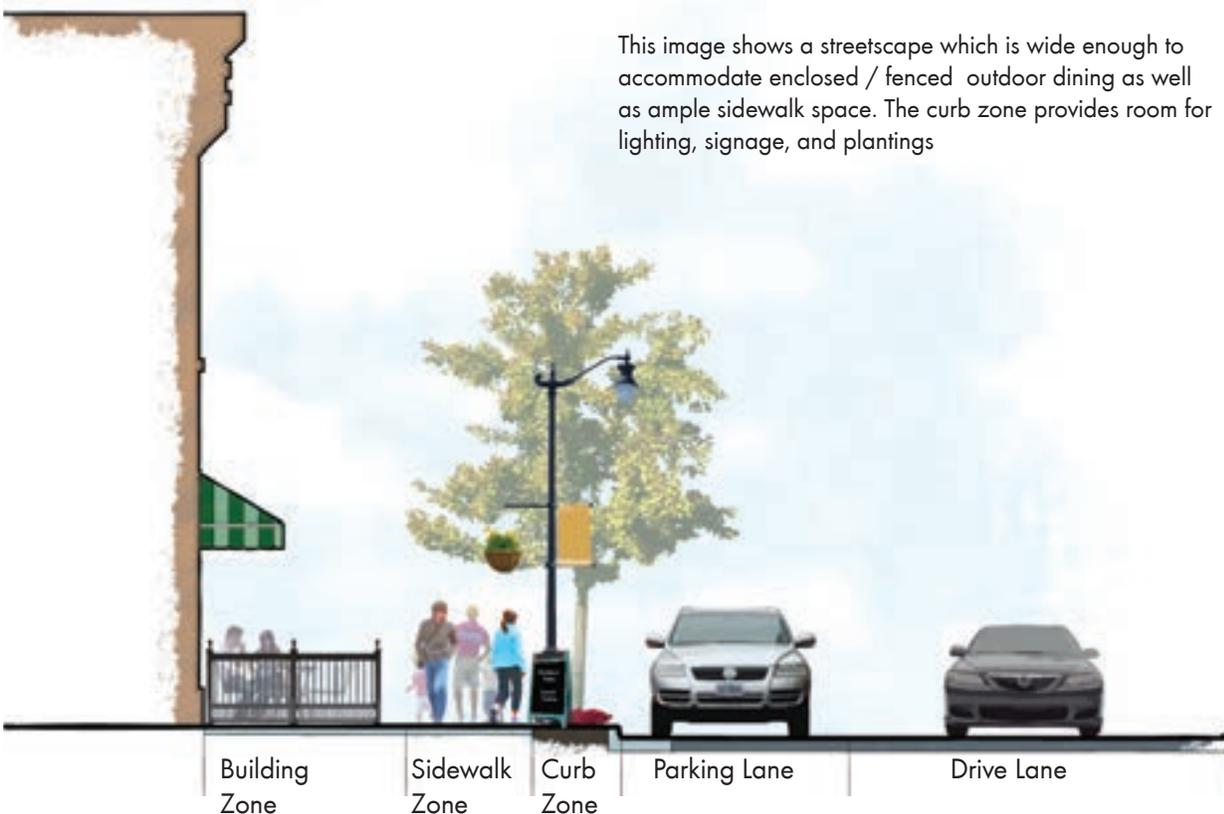
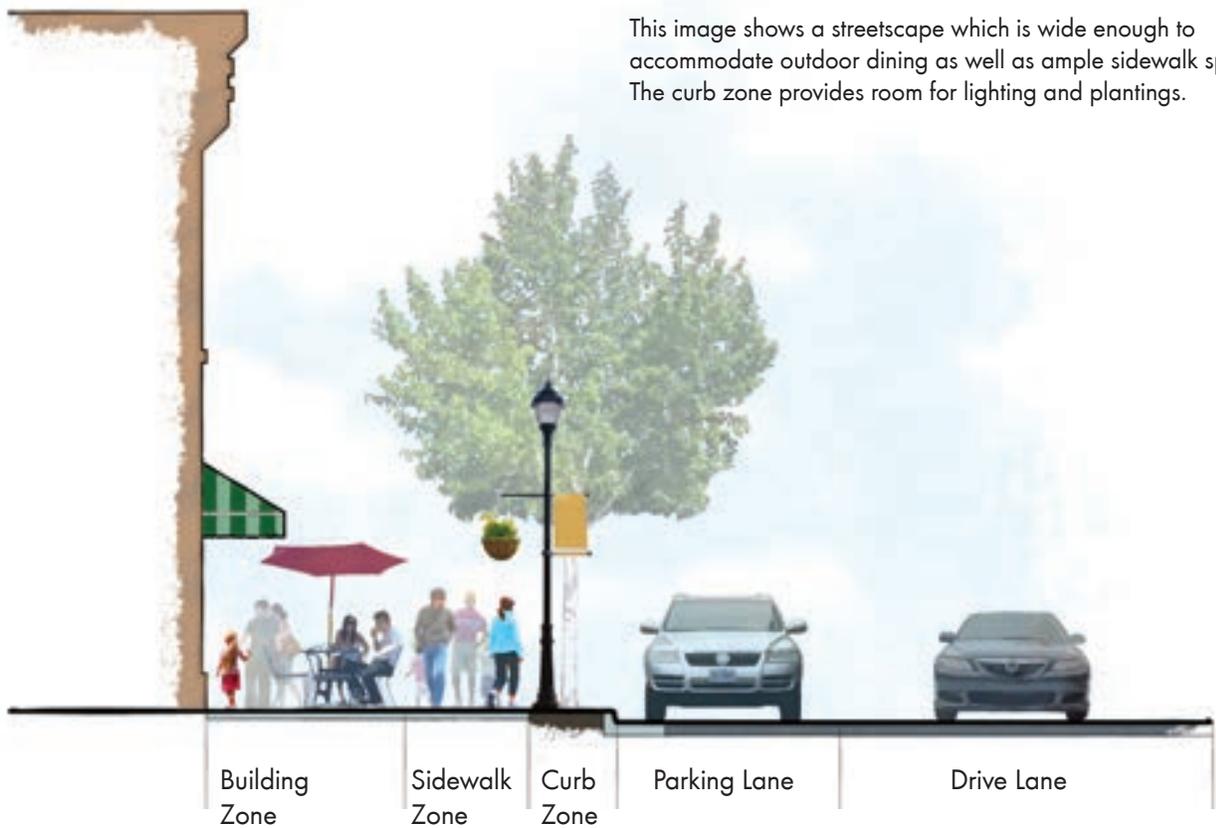
Aside from ADAAG or PROWAG pavement improvements, streetscape amenities should remain clear of the Pedestrian Zone and allow for free movement of pedestrians. Amenities should enhance the pedestrian experience, not be obstructive. Businesses and property owners should be educated on the importance of maintaining Sidewalk Zones. Each business should care for the zones within their building's street frontage.

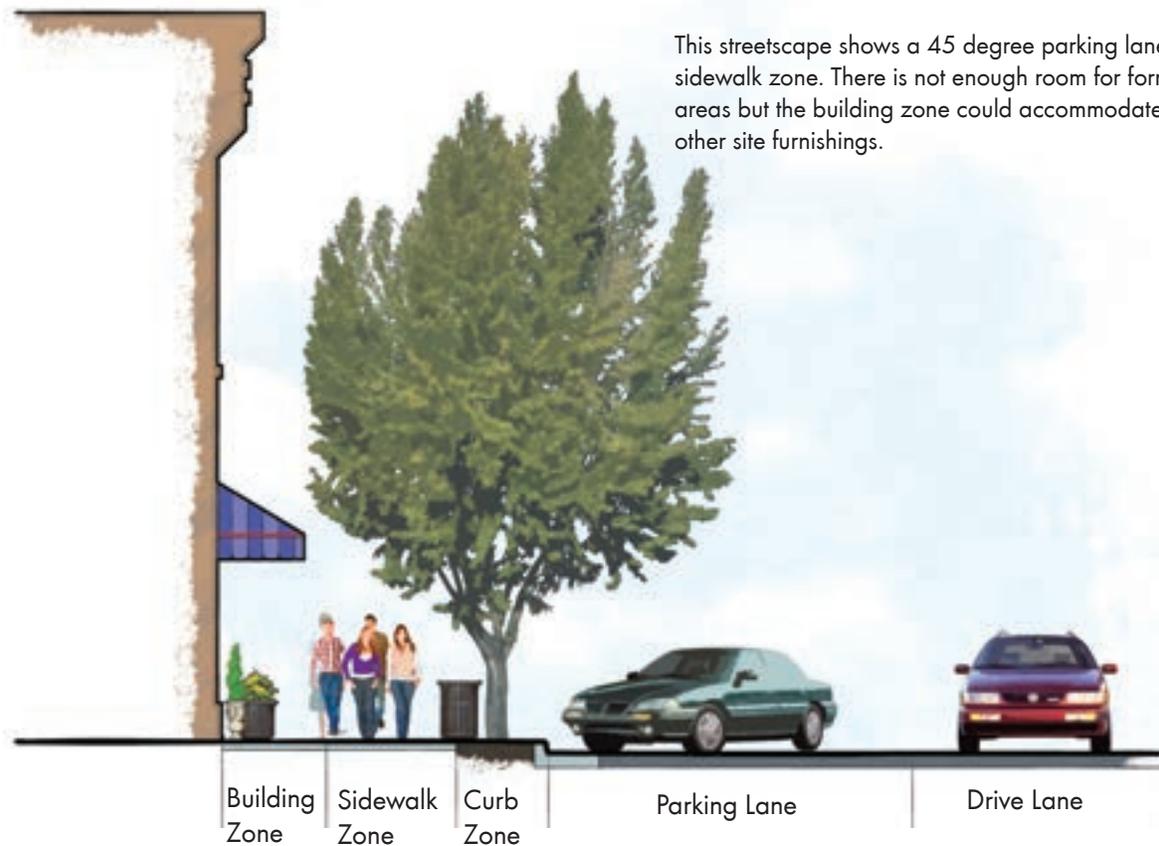
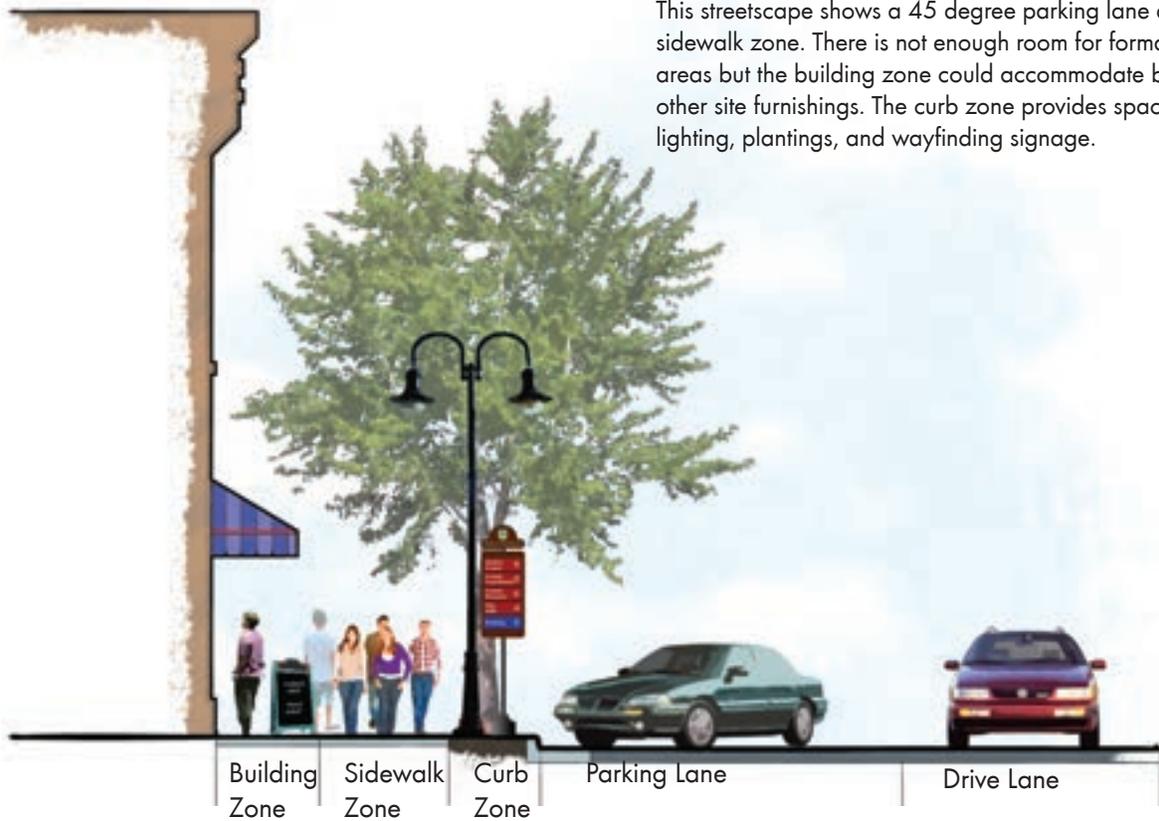
Street furnishings such as bicycle racks, bollards, and benches can add value and functionality to a streetscape, but must be installed in useful locations. Care must also be taken that furnishings are not excessive and do not block on-street parking lanes.

The following pages show various recommendations for sidewalk zone layouts for Downtown streetscape environments and streetscapes adjacent to off-street parking.



A view of a typical Downtown Ozark parking zone, curb zone, sidewalk zone, and building zone.

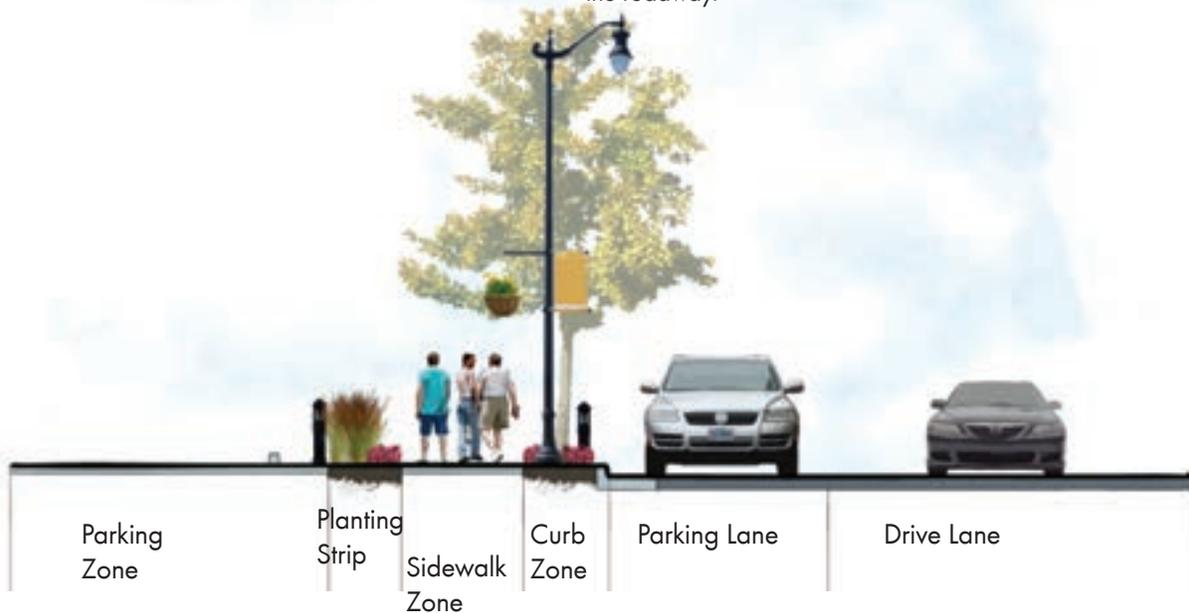




This streetscape shows a streetscape abutting a parking lot. This parking lot is separated from the sidewalk zone an ornamental fence (with an opening providing access to the sidewalk). A bike rack is provided.



This streetscape shows a streetscape abutting a parking lot. This parking lot is separated from the sidewalk zone with a landscaped buffer strip. Added security is provided through the placement of bollards along the edge of the parking area and the roadway.



## 5.5 Parking and Service Areas

Parking lots and service areas are required to support Downtown business and retail tenants. Street parking will accommodate some, but not all of this required parking. Publicly owned parking lots can be improved with streetscape elements to provide pedestrian gathering places. Recommendations for Downtown parking and service areas include:

- Parking and service areas should be well-lit and landscaped. Vacant lots can provide a temporary solution for additional parking, but only on a temporary basis. Planting buffers or decorative paving should be installed at the edges of lots to define the site border. Landscape islands should be included throughout the lot to improve aesthetics as well minimize storm water run-off.
- Parking lots and service areas should be located to the side or rear of the main business area. Parking should be clearly marked as public parking.
- Parking should provide a clear and well lit pathway for pedestrians to reach Downtown activity centers. Lots should also be maintained in good condition, along with all parts of the street, alley, and sidewalk pavement. Parking areas should be connected to clearly-marked crosswalks and may need to be policed. Visitors should feel safe in the parking lot and not have to navigate tripping hazards to reach Downtown destinations.
- To the extent reasonably possible parking areas shall be landscaped along the perimeter of the parking edges to soften the visual mass of the pavement. Landscape should include shrub groupings, grasses, and flower massing. Natural mulch or other pervious ground cover shall be placed between the parking area and the shrubbery and flower massings.



An example of an undefined parking area located off of 3rd Street.

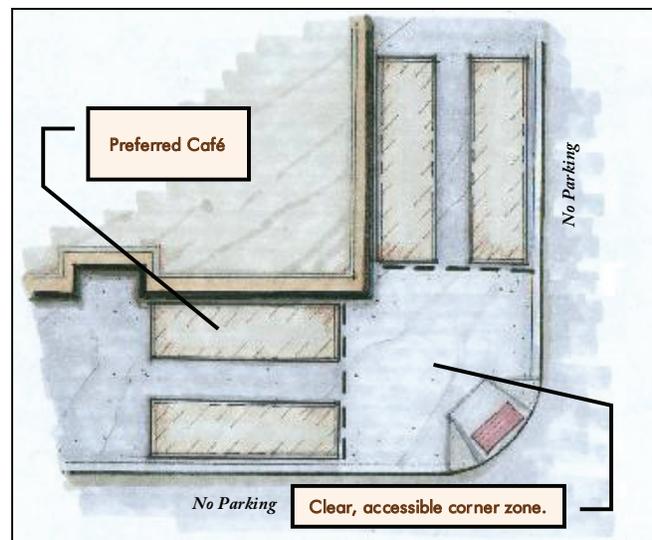


On-street angled parking located in Historic Downtown Ozark. The addition of corner bumpouts helps to define and soften the parking areas.

## 5.6 Outdoor Café Seating

Outdoor Café or sidewalk seating is a common element of a vibrant downtown. Such seating areas can be accommodated in Downtown Ozark, but require special attention. A proper arrangement will:

- Be located in the sidewalk area fronting the restaurant. If located on the side or rear of the building, the seating should not be too near to the parking or street to avoid an unappetizing experience with car exhaust and road grit.
- Allow a clear and unencumbered path along the sidewalk for pedestrian traffic. The sidewalk must maintain accessibility compliance and the restaurant owner should be held responsible for the pathway.
- Not obstruct entrances to the building and provide a clearly defined area connected with the restaurant. Areas adjacent to the building should be ideal.
- Use umbrellas or other patron shelter in a color and style that complements the building. This shelter should only have the businesses name and not advertise beverages or other product brands to promote consistency.
- Consist of furnishings that are durable, weatherproof, windproof, sturdy, and properly maintained. Furnishings should be stored off-site during the winter months.
- Should include sturdy trash receptacles. The restaurant owner should be held responsible for maintaining the cleanliness of the outdoor seating area.



This diagram shows the ideal placement methods for outdoor dining and cafe seating.



A good example of a successful and attractive outdoor dining area in Downtown Ozark.

## 5.7 Street Lighting

Street lighting should enhance the pedestrian experience and nighttime image of Downtown, while also providing an attractive installation during the day. Generally, street lighting should:

- Provide pools of light on the sidewalks at a higher level of illumination than the roadway. Storefront lighting can add to this illumination.
- Be on 12'-14' high poles and project light down onto the sidewalk, not into second floor windows.
- Be uniform in style, type, height, color, type of illumination (LED, compact-fluorescent, etc.) and brightness throughout Downtown.
- Be equipped with brackets for banners and electrical outlets that can display banners and decorations.
- Lighting should not contribute to light pollution, use appropriate lighting systems to ensure dark sky requirements are met.
- Be part of an overall lighting design strategy to ensure desired lighting levels. Street lighting should also illuminate parking areas, rear entrances, and alleys, as well as streets.
- Downtown Ozark has an existing street lighting design that is attractive and meets the above concepts. The lights standards are in good conditions and are currently uniform throughout the Courthouse Square. The City should work to keep the streetscape lighting uniform in future projects, throughout Downtown.



An image of the typical period style light pole located throughout Downtown Ozark.

## 5.8 Signs and Banners

Public signage should be used in the streetscape design to identify, define, and promote Downtown Ozark. Public signage is often only thought of as a functional streetscape element, with no concern for aesthetics. With every Downtown business having at least one sign, public signs can quickly get lost. These signs are critical for wayfinding, traffic flow, and ensuring the safety of Downtown pedestrians. Wayfinding techniques and components to assist in navigation through the area will be addressed in detail in Section 7 starting on page 85.

Coordinated signage for Downtown can also help define the boundaries of Downtown. Concepts to improve the public signage in Downtown Ozark, include:

- Street name signs should be chosen and installed that are distinctively different from the street name signs located in the rest of the City. This will reinforce a feeling of place for Downtown; However, their size, design, typestyle, background color, and lettering color must be readable day or night. Decorative traffic signs and poles can also be used effectively. To reduce visual clutter, regulation and directional signage should be combined where possible.
- Encourage Downtown property owners to install street address signs on their buildings that complement or match the style of the street name signs. A historic plaque-type can provide a very elegant touch. The style, font, and colors of these signs should be easy-to-read and approved by local emergency personnel.
- Temporary banners and other signs for public events and attractions can be allowed, but should be restricted as to size, number in one location, and length of display.
- Seasonal banners or decorations that are approved by the City can be installed to create a festive and vibrant atmosphere. Banners can also add a sense of civic identity, but must be well-designed and are most effective with a simple, repetitive, design. Lettering should be kept to a minimum and sponsor panels should only be allowed within a uniform design panel, if at all. The City should provide all maintenance of public signage, banners, outlets, and brackets. The City should also change the banners on a regular schedule, replacing hardware or faded banners as needed.
- Balloons, pennants, and other distracting sign novelties should be strictly regulated in a downtown environment. These elements can be used on public signage, but this should be uncommon.
- Murals must have an artistic component and should be allowed by City approval only. Murals should be professionally painted. Any mural not approved should be considered in violation of the sign code.

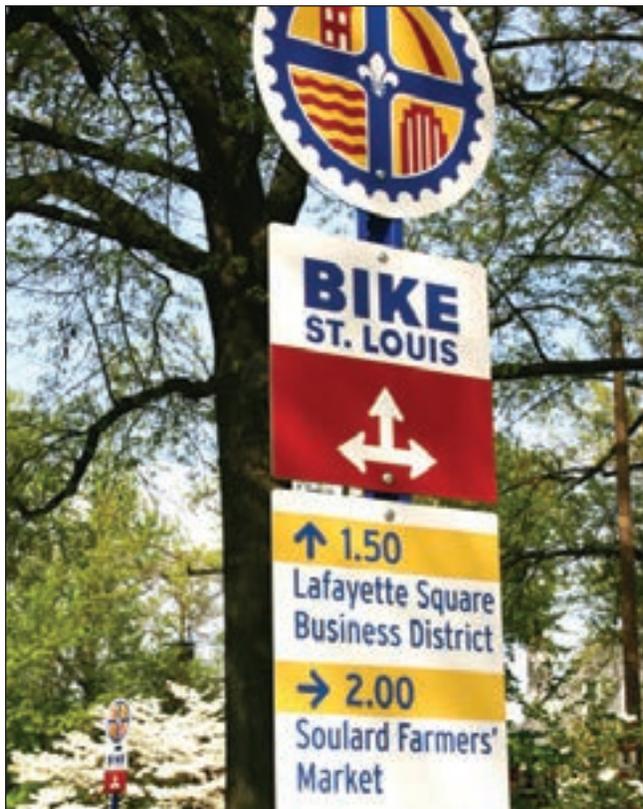


Stops signs should be placed at all corners of the Courthouse Square to calm traffic. Signs should carry a more ornamental appearance than the utilitarian sign shown here.

## 5.9 Bicycles

Downtown Ozark should not only be pedestrian-friendly, but bicycle-friendly as well. Concerns for a bicyclist will include routes of travel, clearance, access to water, type of traffic signals, traffic lanes, signage, drainage grates and curbing obstacles, and parking. Downtown plans should implement bicycle facilities which can be used by local citizens and visitors. The City should identify opportunities for future bicycle facilities in Downtown and throughout the community. Considerations to improve the bicycle friendliness of Downtown Ozark, include:

- Bicycle racks which should be of uniform design of materials, color, and style as other site furnishings. Racks should be located at useful activity nodes throughout Downtown.
- Directional and regulatory signage which identifies bike routes and share-the-road routes. Dedicated bicycle lanes on streets, where feasible.
- Wayfinding signage to various destinations within Downtown.
- Public restrooms and drinking fountains.



An example of bicycle-oriented signage in St. Louis, MO. Signs such as this can alert bicyclists to destinations, distances, and routes.

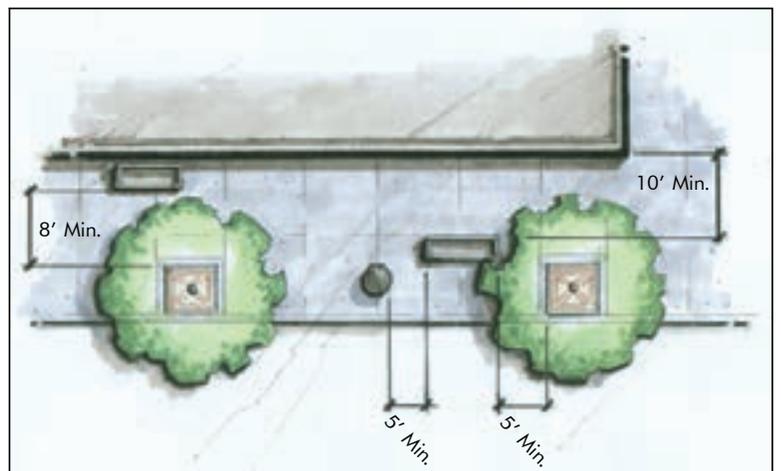


An example of a bicycle repair station in Normal, IL.

## 5.10 Public Furnishings

The furnishings included along a street or in a pedestrian plaza, parking lot, or park should be considered as part of the overall streetscape in terms of design. Elements should complement and introduce users to the theme of Downtown Ozark. Clusters of furnishings will provide gathering places for pedestrians and street furnishings will encourage visitors to park their vehicles and explore. One weakness of the Ozark Courthouse Square is an overall lack of site furnishings. There are a few trash receptacles and a mixed assortment of benches located Downtown. General guidelines for the future installation of public furnishings include:

- Benches within the streetscape encourage social interaction which contributes to a successful Downtown. However, some benches should be oriented so that a pedestrian can sit facing into the storefront. Suggested minimum distances for bench placement are shown at right. Actual distances may vary due to site conditions.
- Planters and window boxes provide color and can be an opportunity to include volunteer service from local clubs and organizations.
- Trash receptacles, bollards, tree grates, and boundary fencing should all be an attractive Downtown accent, not just utilitarian components.
- The styles of furnishings should be simple and not too intricate or flashy.
- Sturdy materials that can be painted are preferred for public furnishings. Wood and soft materials can be vandalized and should be avoided. Concrete is a sturdy material, but is not easily moved, repaired, or replaced. Painted metal is a better choice.
- When grouped together, streetscape furnishings help to enhance Downtown's appearance and create a pedestrian-welcoming environment with functional gathering spaces.
- Furnishings should be coordinated with light and sign posts to present a unified look to the streetscape. The styles of site furnishings should fit with the desired identity and feel of Downtown. They should be of a style that can be easily repaired, replaced or added-to as needed.
- The City may wish to restrict private property owners from installing their own furnishings on the right-of-way in front of their building; instead, providing them with a catalog of approved choices that are complementary to the rest of the streetscape design components.



This diagram shows the appropriate layout and clearances for site furnishings such as benches, trash receptacles, and planters.

- Many site furnishing manufacturers offer customization options for their products, usually via plaques or laser-cut designs (logos, icons, text, etc.) This not only provides a great way to personalize the streetscape elements, but also creates opportunities for community/organizational involvement, dedications, memorials and sponsorship fundraising.



An example of a bike rack and seating area.



An example of an ornamental trash/recycle receptacle.



Ornamental metal benches such as the one shown above provide a durable and comfortable seating option.



Planters shown in front of building entries.

## 5.11 Landscaping

Existing landscaping in Downtown Ozark is abundant and well-maintained. There are street trees installed at regular intervals along the out sidewalks of the Courthouse Square. Although comments from business owners have shown that the trees are seen as blocking their storefronts. Existing planted corner bumpouts provide sufficient greenery but could use more color. There are several opportunities where excess pavement could be removed for landscaping beds. Some building owners have installed their own planters or window boxes and there are a few publicly installed planters.

The City should consider adding landscaping around Downtown intersections. Landscaping zones can also be identified along side streets to complement, but not obstruct, building façades. General landscaping concepts that the City should consider when designing Downtown features, include:

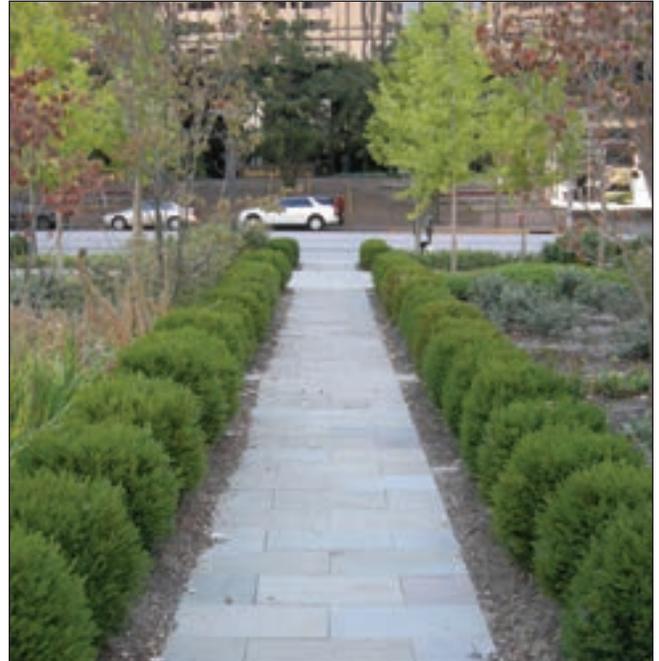
- If landscaping in front of a business is desired, plants in movable containers should be used where no landscape strip is present. Containers should be placed immediately adjacent to buildings or curbs.
- Street trees work best when planted in groups or islands where they can thrive on larger volumes of soil. The trees should be of a hardy variety, common to the region, and at a size which will allow a minimum of seven feet of clearance before any lateral branching begins.
- The canopy of the tree should be considered to avoid excessive roosting of birds.
- Trees that produce fruiting berries should be avoided to reduce maintenance of sidewalks.
- Trees should also be chosen with downward growing roots, not lateral roots that will damage surrounding pavement.
- Shrubs should be massed in groupings of five to seven plants with no more than two different species within a planting bed.
- Suggested minimum placement distances are shown at right. Actual distances may vary due to site conditions.
- Dead plant material shall be replaced in a timely fashion with living plant material, taking into consideration the season of the year, and shall have at least the same quality and quantity of the landscaping initially installed.
- Landscaped areas within and adjacent to off-street parking areas and circulation drives shall be protected from the encroachment of motor vehicles by placing along the entire perimeter of the landscaped area, a six (6) inch concrete curb or other comparable curbing material.



This diagram shows a typical roadway section with street trees, ornamental lighting, landscaped median, and native flower and shrub plantings.



An example of native prairie-style planting area in an urban environment.



An example of a walkway highlighted by a hedge planting.



A sidewalk in St. Louis, MO highlighted with street trees and landscaping.



Nameplates can be added to planting areas for educational purposes and for "plant walks".

## 5.12 Rain Gardens

Rain Gardens are low-lying landscape beds designed to collect rainwater from adjacent impervious areas. A properly designed and installed Rain Garden will ease the load of the existing storm sewer system and reduce erosion and pollution. Rain Gardens also help to facilitate filtration and absorption of rainwater back into the ground. Rain Gardens require proper design and plant selection for maximum efficiency and reduced maintenance costs. Careful consideration to these factors will reward Downtown Ozark with significant environmental impacts for a low cost. Even small Rain Gardens can have a large impact on storm water run-off. General recommendations for Rain Gardens include:

- Low maintenance native plants are recommended due to their greater tolerance for climatic and soil conditions, as well as extreme moisture.
- Design Rain Gardens to also provide aesthetic benefits to the streetscape.
- Rain Gardens should be located so that they do not create an obstacle for street cleaning and maintenance vehicles.
- As with any landscape bed, regular weeding and clearing of litter is required.
- Rain Gardens can typically replace existing landscape beds so as to not require a reduction of parking spaces.
- Species that have trouble thriving should be immediately removed and replaced.
- The City should consider the use of porous pavement in nearby parking areas to help reduce the amount of overall standing water impacting Downtown.
- Rain Gardens should be viewed as a treatment for storm water before it enters the water system.
- Access to conventional drainage structures should be maintained to prevent flooding during heavy storm periods.
- Curb inlets should be provided to allow water to enter and exit the Rain Garden if necessary. The curb inlets should be tapered to minimize damage from maintenance vehicles.
- Refer to page 74 and 75 for appropriate plant species.



An example of a street bumpout rain garden. The Juncus plants are well suited for this environment. Curb cuts and river stone are used to collect stormwater while minimizing erosion to the planting area.



A rain garden used to collect stormwater in a parking lot. Signage is added for educational purposes.



A good example of how to accommodate pedestrian travel and water infiltration into a streetside rain garden.



Plantings can provide substantial amounts of color. Fencing may be required, depending on the depth of the rain garden.



Another example of a lush rain garden.

## 5. 12. 1 List of Appropriate Planting Species

### Ornamental Native Trees

#### Winter

American Holly - *Ilex opaca*  
River Birch - *Betula nigra*  
Sycamore - *Platanus occidentalis*  
Washington Hawthorn - *Crataegus phaenopyrum*

#### Spring

Downy Hawthorn - *Crataegus mollis*  
Eastern Redbud - *Cercis canadensis*  
Flowering Dogwood - *Cornus florida*  
Ohio Buckeye - *Aesculus glabra*  
American Basswood - *Tilia americana*

#### Summer

Fringetree - *Chionanthus virginicus*  
Tulip Poplar - *Liriodendron tulipifera*  
Yellowwood - *Cladrastis kentukea*

#### Fall

Sugar Maple – *Acer saccharum*  
Black Gum - *Nyssa sylvatica*  
Sassafras - *Sassafras albidum*  
Red Oak – *Quercus rubra*  
Persimmon - *Diospyros virginiana*  
Sweet Gum - *Liquidambar styraciflua*

### Ornamental Native Shrubs - by Season of Interest

#### Winter

Black Chokeberry - *Aronia melanocarpa*  
Deciduous Holly - *Ilex decidua*  
Winterberry Holly - *Ilex verticillata*  
Vernal Witchhazel - *Hamamelis vernalis*

#### Spring

Black Haw - *Viburnum prunifolium*  
Spicebush - *Lindera benzoin*  
Golden Currant - *Ribes odoratum*  
Arrowwood - *Viburnum dentatum*  
Roseshell Azalea - *Rhododendron prinophyllum*  
Serviceberry - *Amelanchier arborea*  
Virginia Sweetspire - *Itea virginica*

#### Summer

American Beautyberry - *Callicarpa americana*  
Ninebark - *Physocarpus opulifolius*  
Shrubby St. John's Wort - *Hypericum prolificum*  
Wild Hydrangea - *Hydrangea arborescens*

#### Fall

American Filbert - *Corylus americana*  
Fragrant Sumac - *Rhus aromatica*  
Smooth Sumac – *Rhus glabra*  
Virginia Sweetspire - *Itea virginica*



### Rain Garden Species

Bottlebrush Sedge - *Carex lurida*  
Brown Fox Sedge - *Carex vulpinoidea*  
Virginia Wild Rye - *Elymus virginicus*  
Common Rush - *Juncus effusus*  
Torrey's Rush - *Juncus torreyi*  
Rice Cut Grass - *Leersia oryzoides*  
Dark Green Rush - *Scirpus atrovirens*  
Wool Grass - *Scirpus cyperinus*  
Great Bullrush - *Scirpus validus*  
Swamp Milkweed - *Asclepias incarnata*  
Blue Flag - *Iris virginica*  
Sweet Black-Eyed Susan - *Rudbeckia subtomentosa*

### Native Shade Trees

White Oak - *Quercus alba*  
Shagbark Hickory - *Carya ovata*  
Pin Oak - *Quercus palustris*  
Bur Oak - *Quercus macrocarpa*  
Shingle Oak - *Quercus imbricaria*  
Willow Oak - *Quercus phellos*  
Hackberry - *Celtis occidentalis*  
Kentucky Coffeetree - *Gymnocladus dioica*  
Red Maple - *Acer rubrum*  
Thornless Honey Locust - *Gleditsia triacanthos* var. *inermis*



### Native Grass and Flower Areas

Big Blue Stem - *Andropogon gerardii*  
Indian Grass - *Sorghastrum nutans*  
Switch Grass - *Panicum virgatum*  
Little Blue Stem - *Schizachyrium scoparium*  
Sid- Oats Grama - *Bouteloua curtipendula*  
Canadian Wild Rye - *Elymus canadensis*  
Butterfly Weed - *Asclepias tuberosa*  
Heath Aster - *Aster ericoides*  
Prairie Coreopsis - *Coreopsis palmata*  
Purple Coneflower - *Echinacea purpurea*  
Round-Headed Bush Clover - *Lespedeza capitata*  
Prairie Blazing Star - *Liatris pycnostachya*  
Wild Lupine - *Lupinus perennis*  
Wild Bergamot - *Monarda fistulosa*  
Black-Eyed Susan - *Rudbeckia hirta*  
Showy Goldenrod - *Solidago speciosa*



## 5.13 Fountains, Monuments, and Public Art

Fountains and public art can enhance Downtown and the pedestrian experience greatly. These features will be most effective as simple interactive elements which Downtown visitors can enjoy. Fountains could be simple bubblers that provide a refreshing respite in the summer months, and are lower maintenance than fountains in a pool. Some additional general guidelines concerning fountains, monuments, and public art in Downtown Ozark include:

- Fountain water should be left in its natural state without coloring.
- Commissioned professional public art and sculpture can provide an inspirational atmosphere in which people enjoy lingering. Professional temporary or seasonal art exhibits could also be installed to promote art in Downtown Ozark.
- Fountains and art can also serve as memorial in nature, commemorating City founders or other notable citizens or stories of historical events.
- As with other streetscape elements, fountains and art should not encroach upon the pedestrian walkway.



A historic World War II era Howitzer artillery gun located on the Historic Courthouse lawn.



A monument located on the Historic Courthouse lawn.



A veterans memorial located along side the Historic Ozark Community Building.

## 6.0 Development Plans and Concepts

### 6.1 Finley River Redevelopment

In 2004 the City of Ozark took action to create a Land Clearance for Redevelopment Authority (LCRA) and designate the City's downtown area as blighted for purposes of redeveloping portions of the City's central core. The City also established a tax increment financing (TIF) district. The TIF district includes four redevelopment project areas. One of these is a 47 acre area south of Jackson Street and Finley Creek and east of 3rd Street and the City's historic Central Business District. Referred to as the Finley River Neighborhood District (FRND), this blighted area consisted of a number of single family and mobile homes. Also included were a number of commercial buildings along 3rd Street in various conditions.

The LCRA commissioned the firm Hagerman – New Urbanism from Springfield, Missouri to draft the Ozark Revitalization Plan for this redevelopment area. This plan envisioned a complete redevelopment which included acquisition of properties in the area and demolition of certain existing buildings. The detailed plan included transportation improvements such as a roundabout at Jackson and Third Streets and the construction of a new bridge and connector street called Riverside Drive. Third Street was to be redeveloped as a mixed use commercial main street with a roundabout at Brick Street. A riverwalk was planned to tie the development to the Finley River. The mixed use development included a lifestyle center with retail and entertainment venues, town homes, condominiums, and single-family homes.

Unfortunately, due to a multitude of factors, including the collapse of the real estate credit market, the planned project was never realized. The City now holds a significant amount of financial obligation for the acquisition and demolition costs and seeks to see the site redeveloped. The site is a gateway to the City's central historic area and a prominent commercial location along 3rd Street. PGAV has assessed the site from a market perspective and, in concert with the City, has developed the following development concepts in order to solicit development proposals for the area.

The following pages provides conceptual master plans for Finley River Redevelopment property.

### 6.1.1 Finley River Redevelopment Property - Alternative One

The goal of the Finley River Redevelopment Plan is to create a market feasible development which has real potential for implementation. The concepts alternatives shown on the following pages should serve as a guide for potential development strategies which will best complement the existing Downtown area to the East. Below is a list of ideas and notes for this concept alternative:

- Buildings about the West Side of 3rd Street. This layout is preferred for the continuation of the Downtown feel amongst the buildings.
- A roadway runs through the middle of the property and serves as a central spine for vehicular access. Parking areas are located off this spine and behind the buildings which front 3rd Street.
- Pedestrian access from Historic Downtown and Finley River Park is accommodated at the intersection Church and 3rd Street and the proposed intersection at Route 14 and Jackson Street.
- The majority of the site will be mixed use. This will include first-story retail, second-story and third-story apartments or office, a municipal building, market bistro, a pharmacy, and other related tenants.
- The western edge of the property is intended for retirement age driven housing. These are intended to be two to three-story condos or townhouses. The area of the property has topographical challenges which need to be addressed. This housing can easily be phased development on market demand.
- A destination restaurant or related tenant is desired for the northern area of the property because of the desirable view from the Finley River valley.
- An ornamental fence, a new parking lot, and landscaping are proposed treatments for the cemetery.
- Additionally areas are left as open space for parks, stormwater capture, and parking lot stormwater capture.



Detail of the Finley River Redevelopment Plan - Alternative One

Master Plan - Alternate 1



### 6. 1. 2 Finley River Redevelopment Property - Alternative Two

The goal of the Finley River Redevelopment Plan is to create a market feasible development which has real potential for implementation. The concepts alternatives shown on the following pages should serve as a guide for potential development strategies which will best complement the existing Downtown area to the East. Below is a list of ideas and notes for this concept alternative:

- Buildings about a roadway which serves as the central spine to the development. This layout is preferred for the ease of vehicular access. This layout leaves more open space between the Historic Downtown and the proposed redevelopment.
- A roadway runs through the middle of the property and serves as a central spine for vehicular access. On-street parking is located along this roadway and additional parking lots are located along 3rd street.
- Pedestrian access from Historic Downtown and Finley River Park is accommodated at the intersection Church and 3rd Street and the proposed intersection at Route 14 and Jackson Street.
- The majority of the site will be mixed use. This will include first-story retail, second-story and third-story apartments or office, a municipal building, market bistro, a pharmacy, and other related tenants.
- The western edge of the property is intended for retirement age driven housing. These are intended to be two to three story condos or townhouses. The area of the property has topographical challenges which need to be addressed. This concept shows a potential phase one housing development.
- A destination restaurant or related tenant is desired for the northern area of the property because of the desirable view from the Finley River valley.
- An ornamental fence, a new parking lot, and landscaping are proposed treatments for the cemetery.
- Additionally areas are left as open space for parks, stormwater retention, and parking lot stormwater capture.



A detail of the Finley River Redevelopment Plan - Alternative

Master Plan - Alternate 2

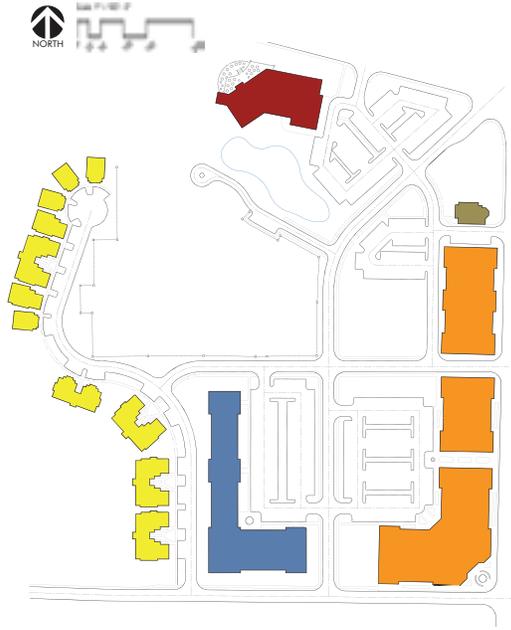


### 6. 1. 3 Finley River Redevelopment Property – Building Typology

This section addresses the intended look, feel, and use of the buildings proposed for this redevelopment. Below are some notes and ideas on building types, which correspond to the images and diagrams shown on the next page:

- Overall the proposed buildings are to have a character which is similar to the Historic Downtown buildings. While they can be constructed with modern materials and constructed materials, the dominant facades should be brick and masonry that complement the Historic Downtown. Similar architectural details should be used as well.
- The desired look, feel, and mix of uses is intended to be similar to that of new urbanism communities, such as New Town in St. Charles, Missouri, Chesterfield Village in Springfield, Missouri, and Winghaven in O'Fallon, Missouri.
- The destination restaurant on the northern portion of the property, shown in red, could be a winery, brewery, or other similar dining and entertainment-related venue. This building has the most flexibility in appearance to accommodate for different possible tenants, but should still have brick and masonry as a dominant façade, complementary to Historic Downtown. There is opportunity for a large patio to capitalize on the view of the Finley River.
- The buildings shown in orange are intended to be mixed-use. These should include first-story retail, second-story and third-story apartments or offices. This building should have a more residential character to the upper stories and commercial storefront look to the first story. The dominant facades should be brick and masonry that complement the Historic Downtown. Similar architectural details should be used as well.
- The building shown in blue is intended to be used for municipal, office, and market type purpose. The building can have more of an office-type look and feel. The dominant facades should be brick and masonry that complement the Historic Downtown. Similar architectural details should be used as well.
- The buildings shown in yellow are proposed to be retirement age-driven housing. These are intended to be two to three-story condos or townhouses. The area of the property has topographical challenges which need to be addressed. The topographical challenges could be minimized by building into the slope, having the top floor at grade with the entry and lower floors built down the slope. Any extra fill generated from excavation and earthwork performed to build foundations should be used for the construction of roadway access.
- The building in green is an existing single family house converted to commercial. This building has historic character and is well-maintained. This can be kept a commercial or could be used for a museum, welcome center, or related purpose.

### Building Type Diagram



## 6.2 Courthouse Square and Finley River Redevelopment Gateway Improvements





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## 7.0 Wayfinding

### 7.1 Principles of Wayfinding

#### What is Wayfinding?

The term wayfinding refers to a mental process of understanding and navigating a given environment—literally “finding your way” between points A, B, and C. To make this mental process as easy as possible, communities can plan a coordinated system of signs, pathways, landmarks and other visual cues to help people understand where they are going.

In addition to directing traffic and conveying practical information, the best wayfinding systems accomplish larger, more subjective goals, such as:

- Creating a unique visual identity,
- Establishing a sense of place, or
- Communicating or reinforcing messages about a place.

To put it simply, wayfinding directs travelers to a destination and creates a positive first impression about the destination. Wayfinding systems have been used for many years on college campuses, office parks and tourist districts. Cities and towns of all sizes are now using wayfinding systems to direct traffic to key destinations throughout the community and help shape their community brand.

Wayfinding is a mental process, and it involves more than just a program of coordinated signage. The most effective wayfinding solutions have well-defined routes or pathways and clear visual cues (such as landmarks or prominent buildings). Of course, printed maps or GPS navigation are part of wayfinding, too.

This report focuses on the value of a coordinated signage program and recommends specific locations for sign types in Ozark. The aim of this wayfinding system is to direct traffic to Downtown Ozark and other prominent destinations and create a positive impression of the community. One of Downtown Ozark’s biggest challenges is alerting those driving along Highway 65 and 3rd Street that Downtown Ozark is there to be seen and enjoyed. A comprehensive wayfinding program can address this challenge.



An example of a pedestrian kiosk.

## 7.2 Wayfinding Components

A comprehensive approach to wayfinding considers signage in light of architecture, lines of sight, and lighting. Below are general principles of effective wayfinding that relate to each of these four components:

Signage:

- Uniform signage at important decision points is a critical element of wayfinding. Locations for signage should be chosen in terms of decision points (“Should I turn or go straight?”) and traffic volume.
- Replace purely functional signs lacking character (e.g. standard MoDOT signs) with attractive uniform signs.
- The size of signs (the sign panel and lettering) should be governed by average vehicle speed and distance from the roadway.
- Excessive signage diminishes the effectiveness of individual signs. Fewer, easy to read, appropriately placed signs are preferred.
- Avoid signs that are too small and are of varying sizes, colors, and types. Uniform design helps users find the next sign and verifies that they are “on the right track.”

### 7.2.1 Architecture

- Buildings, landscape features and other elements of a street can serve as visual cues to help people understand their location and the route to their destination. For instance, seeing buildings spaced closer together is a cue that one may be entering a traditional downtown area.



An example of a Downtown Gateway.



An example of signage with hints to architectural character.

- Strong architectural features serve as landmarks and orientation points. These points are often destinations as much as they can be starting points. The wayfinding system should exploit these types of features.

### 7. 2. 2 Sight Lines

- Clean, clear lines down the streets at key intersections should be maintained. Avoid allowing buildings or private signage to encroach or block these lines.
- Motorists feel most comfortable maintaining visual contact with his or her destination and will want to make as few direction changes as possible.
- Repetitive landscaping, decorative light poles or street furnishings can draw the eye down streets, but care must be taken that these items do not obstruct views or navigational landmarks.

### 7. 2. 3 Lighting

- A repetitive line of lighting can be a very effective navigation tool. Lighting can be used to encourage routes and pathways.
- Poor lighting causes missed information and leaves an unsafe impression.
- Warmly lit sidewalks and streets draw the visitor onward. Warmly lit storefronts and entrances draw the eye and help the visitor get to the business district.



A unique and architecturally-intricate gateway arch.



A good example of light used to highlight a street corner in Fulton, MO.

## 7.3 Hierarchy of a Signage System

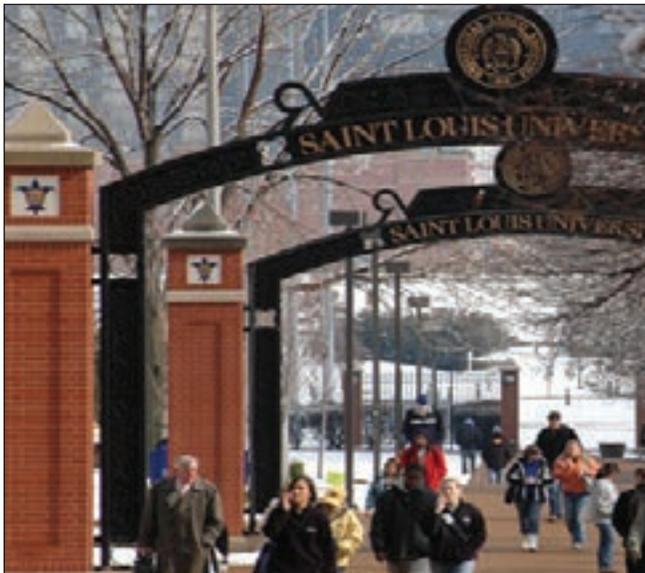
In a wayfinding signage system, several types of signs are designed and placed at various locations around the community. Each sign type has a slightly different function and size but uses a common design theme. A typical wayfinding sign program can be described as a hierarchy because the sign types can be arranged as “levels” ranked by the number of signs needed.

The levels of sign types are pyramid-shaped, with the fewest number of signs occupying the top level, working downward to sign types installed with the most frequency. The top-level signs would be the largest and most costly, and the more common smaller signs lower on the pyramid are less costly. All the signs use consistent fonts, logos, and color patterns to create a uniform theme.

Each of the most common wayfinding sign types are explained and illustrated below.

### 7.3.1 Gateways

Gateways can have an important practical purpose (announcing entry or drawing attention to a key route), and they help establish the all-important first impression. Gateways can be purely signage, but are often incorporated into a monument. In some cases, the monument (such as a fountain, column or archway) is the larger element, with a simple sign component included.



Gateway arches on Saint Louis University campus.



To the visitor, the community gateway will establish the design theme that is repeated in some fashion on signage throughout the system. Some wayfinding systems will also incorporate “district gateways” that establish entry points to a district (the historic downtown, for instance, or a distinct neighborhood).

### 7.3.2 Trailblazers

Trailblazer signs have a utilitarian function, pointing the way to key destinations or districts, but they also incorporate the community brand and the wayfinding system’s design theme. Trailblazer signs can point the way to districts (using simple labels such as “Historic Downtown Ozark”, “Theater District”, etc.) or to specific destinations (e.g. “Finley River Park”, “Smallin Cave” or “High School”).



An example of a gateway monument.



An example of a trailblazer sign in Warrensburg, MO.

Trailblazers should be located at or near key intersections that serve as a “decision point” for travelers — a point where the motorist must decide to turn or continue straight to follow the route toward the destination. These decision points will often be near intersections of heavily travelled streets and highways. The other obvious factor in placement of the signs is the location of the destinations and clearest routes from main highways.

Trailblazer signs vary in size, but since they tend to be at key intersections on streets of relatively higher traffic volume and faster vehicle speeds, the size of the sign and the text printed on it may need to be relatively large. A typical trailblazer sign would be in a range of 5 to 6 feet wide by 3 to 4 feet high, installed on two poles with the bottom of the sign elevated 6 to 7 feet above grade.

Once locations for trailblazers are chosen, the typical sign dimensions and text size should be determined by the speed of traffic and roadway width. Because of the varying size requirements, trailblazers of two different sizes might be needed—one for highways, one for local streets.

### 7.3.3 Proximity Signs

As the most frequently used signs, proximity signs comprise the base of the wayfinding pyramid. Proximity signs are installed as the traveler gets closer to the destination or at the final turn to reach a destination. Proximity signs let users know they are “on the right track.” These signs can point the way to a single destination with a simple “straight-ahead” arrow showing that the traveler is approaching the destination.

A proximity sign might also be a smaller version of trailblazers, pointing the way to multiple destinations.

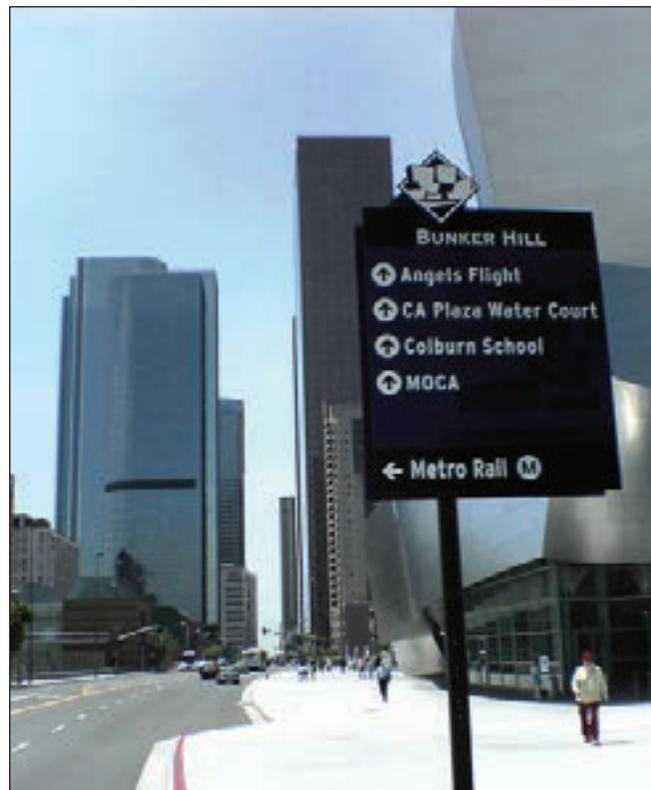
Intended for slower traffic on local streets, proximity signs are smaller and installed on single poles or on existing light poles, if properly situated. Proximity signs can be effective with dimensions as small as 36 inches square and usually still need to be installed at a height of 6 to 7 feet above grade.

### 7.3.4 Special Purpose Signs

Depending on the needs for the area, a series of special purpose signs, designed to reflect the uniform style of the overall signage program can be an important part of the wayfinding system. For pedestrian oriented areas like an historic downtown, informational kiosks can be helpful to point out attractions and options for shopping, dining or public restrooms. The use of historical information markers, which can include historic photographs or community history, are attractive to both visitors and residents alike. Signs pointing the way to public parking are often necessary for a downtown district or other area of dense development. All such signs should be designed with the consistent color scheme and font type of the wayfinding system, which will make the signs stand out.



An example of a historical information sign in Moline, IL.



An example of a proximity sign.

Portable or other temporary signage is often overlooked in wayfinding programs, but these can be among the most useful for special events such as community festivals, sports tournaments, conventions or other events that draw visitors to the community. Examples include portable “sandwich-board” signs that can be placed at key roadside locations or point the way to overflow parking.

### 7.3.5 Banners and Decorative Lighting

Street lighting, whether on standard or decorative poles, plays a role in wayfinding, and a series of attractive, decorative light poles can signal entry into historic areas or a downtown district. Banners that incorporate the consistent design of the wayfinding system can beautify an area and reinforce the community brand.

Ideally, banners will be designed with color schemes and font types consistent with the wayfinding program. Banners of different sizes can be used so they are of a scale appropriate for the pole height and street width. The community can use seasonal banners to celebrate community festivals; these temporary banners can be unique yet still reflect the consistent wayfinding design theme.



Parking signage attached to a stop sign, observed in Downtown Ozark.



A streetpole, banner, and parking sign in Downtown Ozark.

## 7.4 Special Considerations

### 7.4.1 Corridors and Destinations

Before designing attractive signage, an effective wayfinding program begins with identifying the primary destinations and understanding traffic patterns of visitors. Currently, Ozark has identified “entry points” at which to place gateway signs, and trailblazer signs to help visitors along the way to destinations in Downtown Ozark or elsewhere. The maps located on page 100 and 101 is the product of discussions between City staff, DREAM Committee members and PGAV.

### 7.4.2 Public Versus Private Signage

Community wayfinding programs focus on designing signs intended for installation on public right-of-way, which is typically a varying amount of land on either side of the roadway. As part of the street and highway system, installation of signs on public right-of-way requires permission from the appropriate jurisdictional authority (MoDOT, county highway department, or City Public Works Department).

The appropriate jurisdiction should be identified for each desired sign. As the entity coordinating the wayfinding program, the locations under City jurisdiction usually present the least difficulties in permitting. County or MoDOT approval may require submittal of formal applications. Establishing early communications with the appropriate review agency is important, as is keeping in mind the following considerations:

- A “clear zone” along the edge of pavement in which no signage may be installed will be required, in certain locations, to make way for errant vehicles. The width of the clear zone may depend on the traffic volume, average vehicle speed and layout of the particular roadside location.
- To further account for errant vehicles, the permitting agency may require that signs be installed with



A pedestrian directional sign located in Downtown Webster Groves, MO.



An example of a parking destination sign in Downtown Warrensburg, MO.

“break-away” brackets that would allow the sign pole to give way if a vehicle strikes the pole.

- Sign installation will need to meet specific wind load requirements.
- As part of the public right-of-way, the content of the sign will be restricted to giving direction to general districts or public destinations (as opposed to directing to private businesses or attractions).
- Maintain flexibility in working with MoDOT, and keep in mind that the primary goal of a transportation agency is to promote safe, efficient travel of motorists. Wayfinding does promote safe, efficient travel, but the aesthetic and branding goals of a wayfinding program will be subordinate to safety and consistency when it comes to permitting.
- A community may also use the private property adjacent to the street or highway for installation of gateways and other components of the wayfinding system. Private property is usually less preferable since it is farther away from the right-of-way, but depending on the width and design of the roadway and availability of adjacent right-of-way, it may, in certain circumstances, be the only choice.
- If permitting is problematic on right-of-way or if a particularly large sign is desired, then private property might be the best location. Just keep in mind that the farther away from the roadway, the less visible the sign. Elevating the sign or monument with a landscape berm, installing lighting to improve nighttime visibility and increasing the size of the sign can overcome this problem.



Proximity signage attached to an existing light pole.

- On private property, the wayfinding monument or signage has to compete with other privately owned signage. As with any other signs on private property, wayfinding signs are subject to city or county sign regulations, which are typically part of the zoning ordinance. Sign regulations will dictate maximum height, overall size, illumination and the number of signs per parcel. However, as signage with a unique public purpose, local ordinances often exempt signs installed or sanctioned by the local government.

## 7.5 Wayfinding Implementation

After considering the information and recommendations provided in this Wayfinding Plan, the City should confer directly with the Ozark Chamber of Commerce, members of the DREAM Committee, City staff, and members of Downtown Ozark Organization (when formed) with respect to funding and implementing the wayfinding plan:

- Study the Wayfinding Plan and make any desired modifications to destinations and sign placement locations. Begin consultation with MoDOT officials regarding permitting along state routes and highways.
- Develop a budget (including funding sources) and installation timeline for the program.
- Using the recommendations in the Plan, evaluate and photograph each proposed location to determine any conflicting signage or installation problems.
- Obtain permits from MoDOT for signage on state highways.
- Evaluate proposals and enter into contracts to purchase and install signs.
- Because of the anticipated cost of the wayfinding projects, a phased approach is expected, and implementation of portions of the Wayfinding Signage System may take a few years to complete. However, with community buy-in and funding support, much of the program is achievable within a short timeframe.



Installation of a trailblazing sign.



Installation of a proximity sign.

## 7.6 Review of Existing Signage in Ozark

As part of the process of preparing this report, PGAV reviewed existing welcome signage and directional signage to evaluate the ability of motorists to find their way to Downtown Ozark. The following is a list of some of the observations and findings:

- A monumental gateway sign is located on a bluff along Highway 65. This sign is made of masonry and is very attractive and well built. Although this sign is attractive it is located far off the roadside, it is too small for the speed of travel along 65, and is overwhelmed with conflicting signage (Billboards).
- Brown MODOT signs for Historic Downtown Ozark are located along Highway 65 and at the off-ramp for Jackson Street and Route 14. The signage puts you in the right direction of travel but gives no notion of distance or guidance after leaving the highway.
- At the intersection of Church and 3rd Street, the main entry to Historic Downtown, there is no signage to direct visitors to the Downtown Square.
- Lights poles on the courthouse square are very attractive and have banners and parking signage installed.
- No signage to public parking lots were observed, and destination signage was minimal.
- Excessive signage and overhead utilities seemed to be a common issue throughout the City of Ozark. This issue will need to be address in further detail during the wayfinding implementation process.



Ozark gateway monument located on Highway 65.



Intersection of Church and 3rd Street. Note there is no signage to orient drivers toward Downtown.

## 7.7 Ozark's Proposed Wayfinding System

The overall goals of Ozark's wayfinding program are:

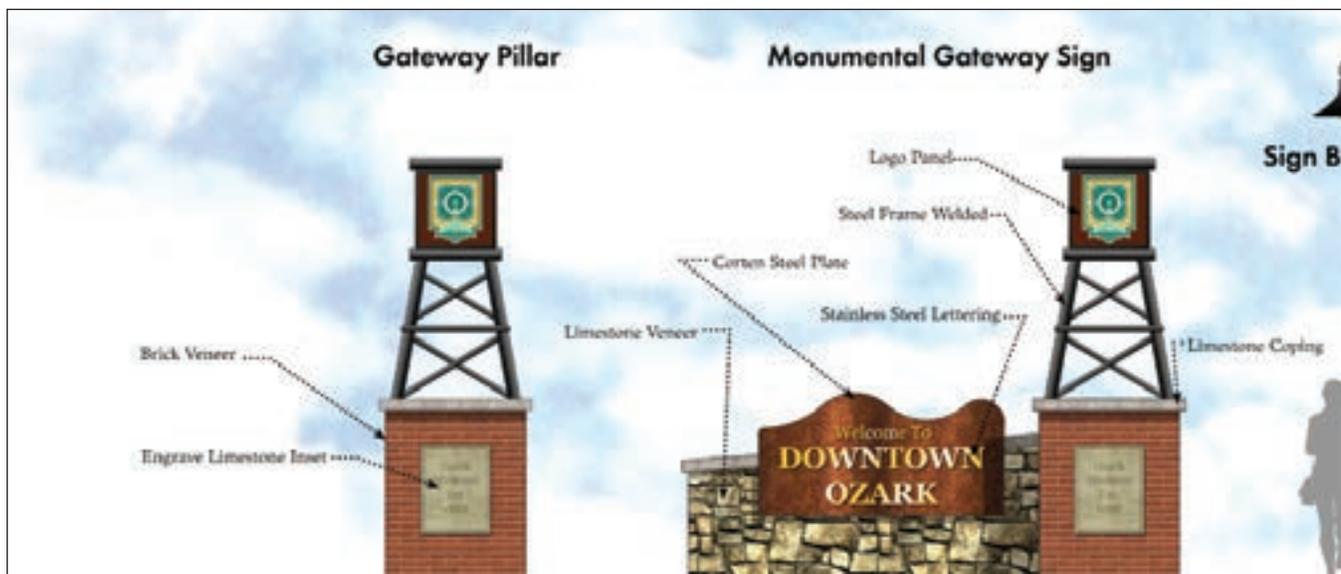
- Alert Highway 65 travelers to Downtown Ozark.
- Help these travelers find Downtown Ozark after leaving the Highway.
- Provide direction to the many destinations and attraction located in the City of Ozark and its surrounding areas.
- Establish a positive first-impression.

A uniform system of wayfinding for Ozark will communicate Ozark's identity as an attractive, vibrant community and direct visitors from the highway to Downtown and other destinations. The components of the wayfinding program are explained in the narrative below and illustrated on the two map exhibits labeled Wayfinding Sign System and Downtown Detail.

### 7.7.1 Gateways to Ozark and Downtown

Gateway signage is proposed at three locations, near the proposed intersection of 3rd Street and Jackson Street, at the intersection of 3rd Street and Church Street, and south of the intersection of 3rd Street and Walnut.

The proposed gateways are intended to be constructed of masonry and ornamental metals for the sign area and lettering. Because of the construction materials of these signs right-of-way placement may not be allowed by MODOT, therefore sign easements or similar agreement may be necessary for gateway placement along 3rd Street.



Monumental gateway sign concepts.

There are two gateway options presented a gateway pillar and a more horizontally oriented gateway sign. The gateway pillar was designed for use at a tight intersection such as the intersection of 3rd and Church Street.

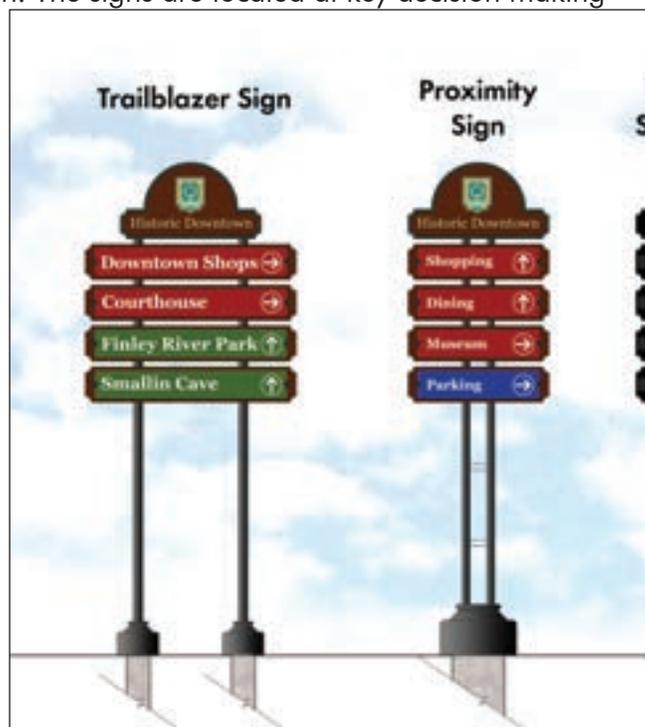
### 7.7.2 Trailblazer Signs

There are eight trailblazing signs proposed on this plan. The signs are located at key decision making intersections for travel to Historic Downtown Ozark and Other locations.

The trailblazers are designed to display three to four travel options for destinations at the most, any more will confuse the traveler and decrease the effectiveness of the signage. Letter heights should be approximately 6 inches to accommodate readability at higher vehicle speeds.

### 7.7.3 Proximity Signs

The proposed proximity sign locations are closer to particular destinations in and around Downtown Ozark. Depending on the location, they can list a single destination, assuring travelers that they are headed in the right direction, or they can point out the location to two or more destinations with arrows. As with the trailblazer signs the proximity signs should only display three to four travel options for destinations at the most to maximize readability and effectiveness.



Trailblazer and proximity sign concepts.

12 proximity signs are noted on the proposed wayfinding plan, additional destination markers may be desired. The majority of these would be on the downtown square, to point out key attractions and businesses or business areas. The remainder will be along key routes, such as 3rd Street and Route 14.

Ozark's proximity signs will have the same coordinated design as trailblazers but would be installed on single posts. Some of these signs, which are designed to be as small as 3 feet by 2 feet, could be installed on existing light poles as a cost-saving measure.

### 7.7.4 Pedestrian-Oriented Signage

Additional sign designs were created to accommodate foot traffic in and around Downtown Ozark. These designs include pedestrian directional signs, pedestrian kiosks, sign banners, and other light pole retrofits.

This signage can have smaller text size and contain more information than the vehicular-oriented trailblazing and proximity signs. The signs should carry the same look and design characteristics of the vehicular signage.

These signs are to be located around the courthouse square, where most of the foot traffic will be taking place. The pedestrian kiosks are placed what is perceived to be the main entry points to the courthouse square by foot traffic. The kiosks are placed between the two parking lots, and the intersections of 3rd and Church Street.

### 7.7.5 Special Signage

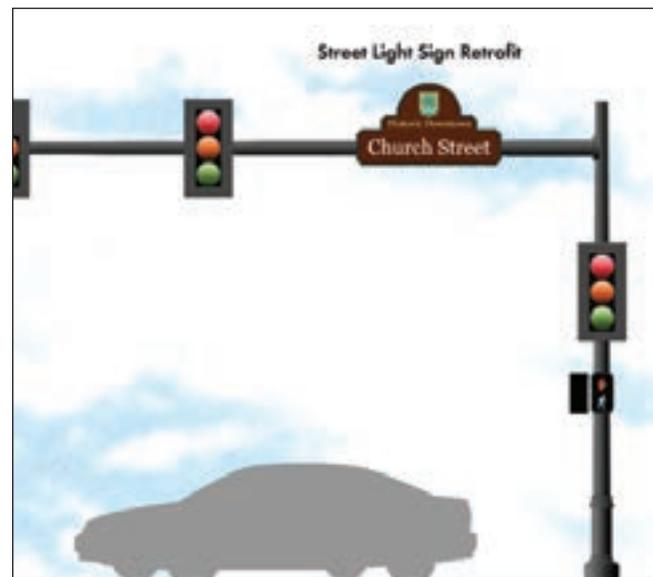
Additional signage could include ornamental traffic signal signs and special events signs. The traffic signal signs could be used around the entire city as well as the Downtown area. The proposed special event sign is designed to be interchangeable and used for differing events such as the farmer market and birthday bash.

The following page shows design concepts for the overall signage system designs.

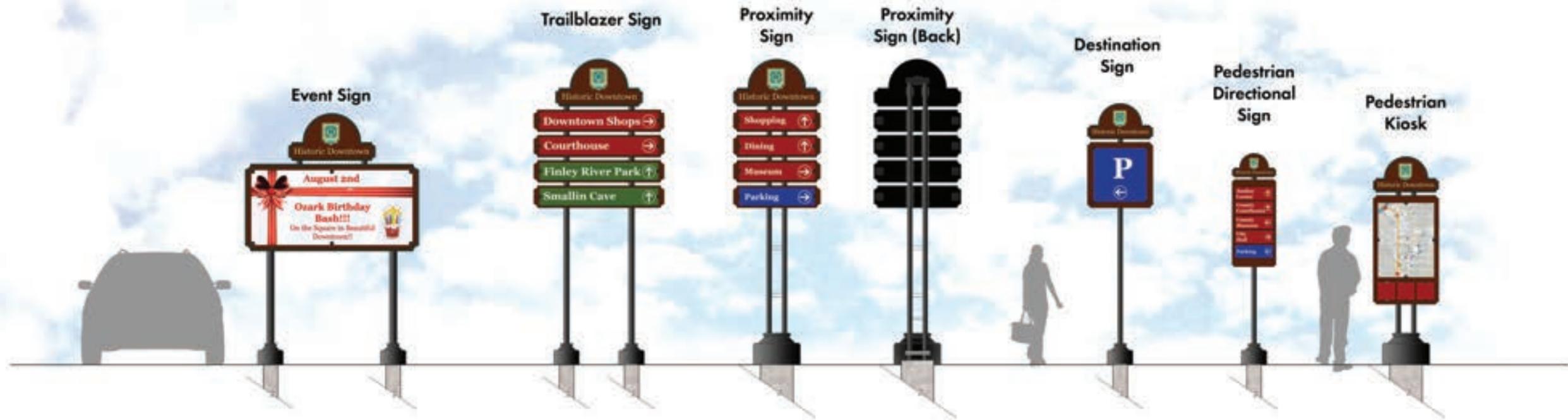
Proposed sign locations are shown on the wayfinding plan maps on pages 100 and 101.

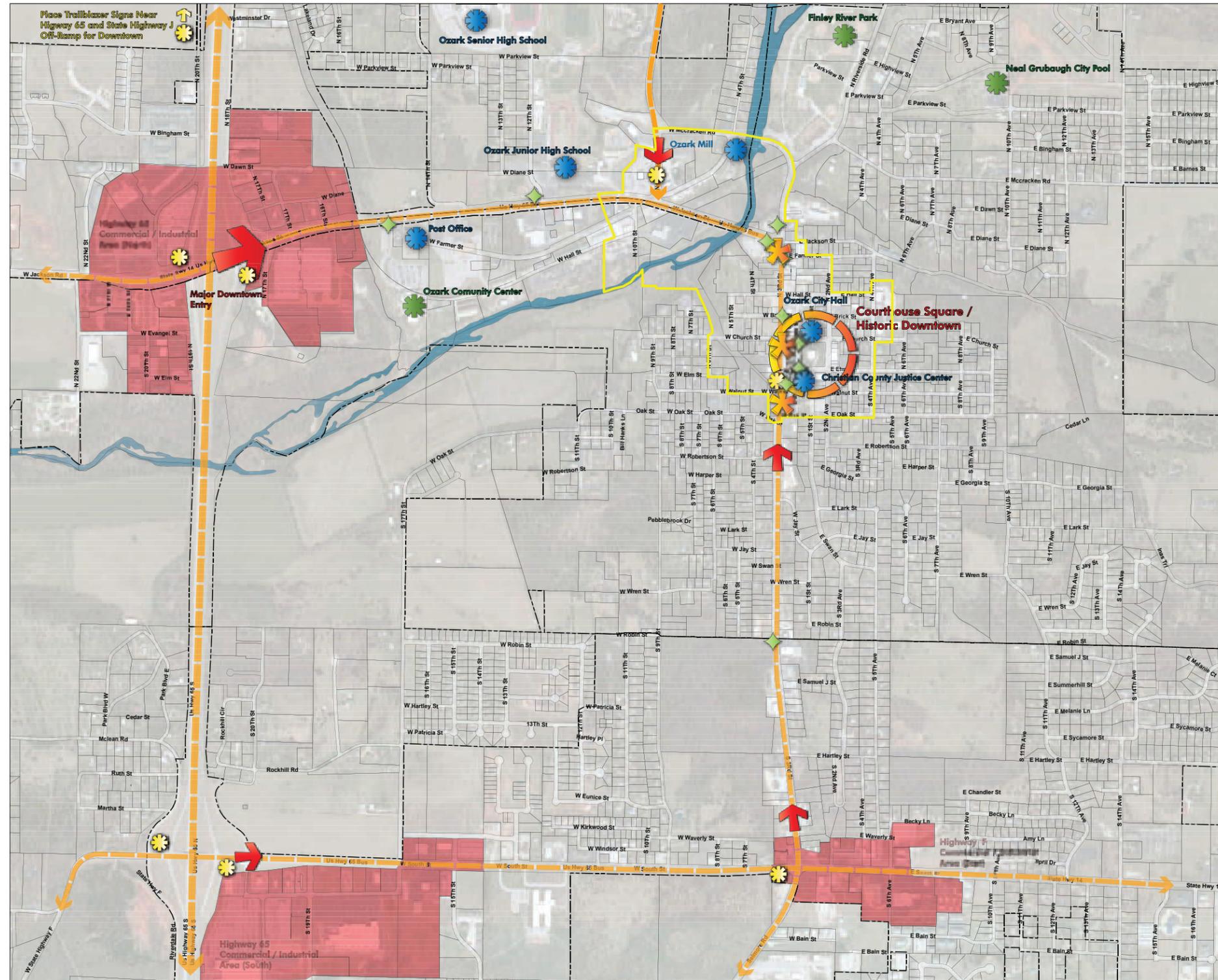


Destination and Pedestrian signage.



Street light block signage.



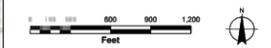


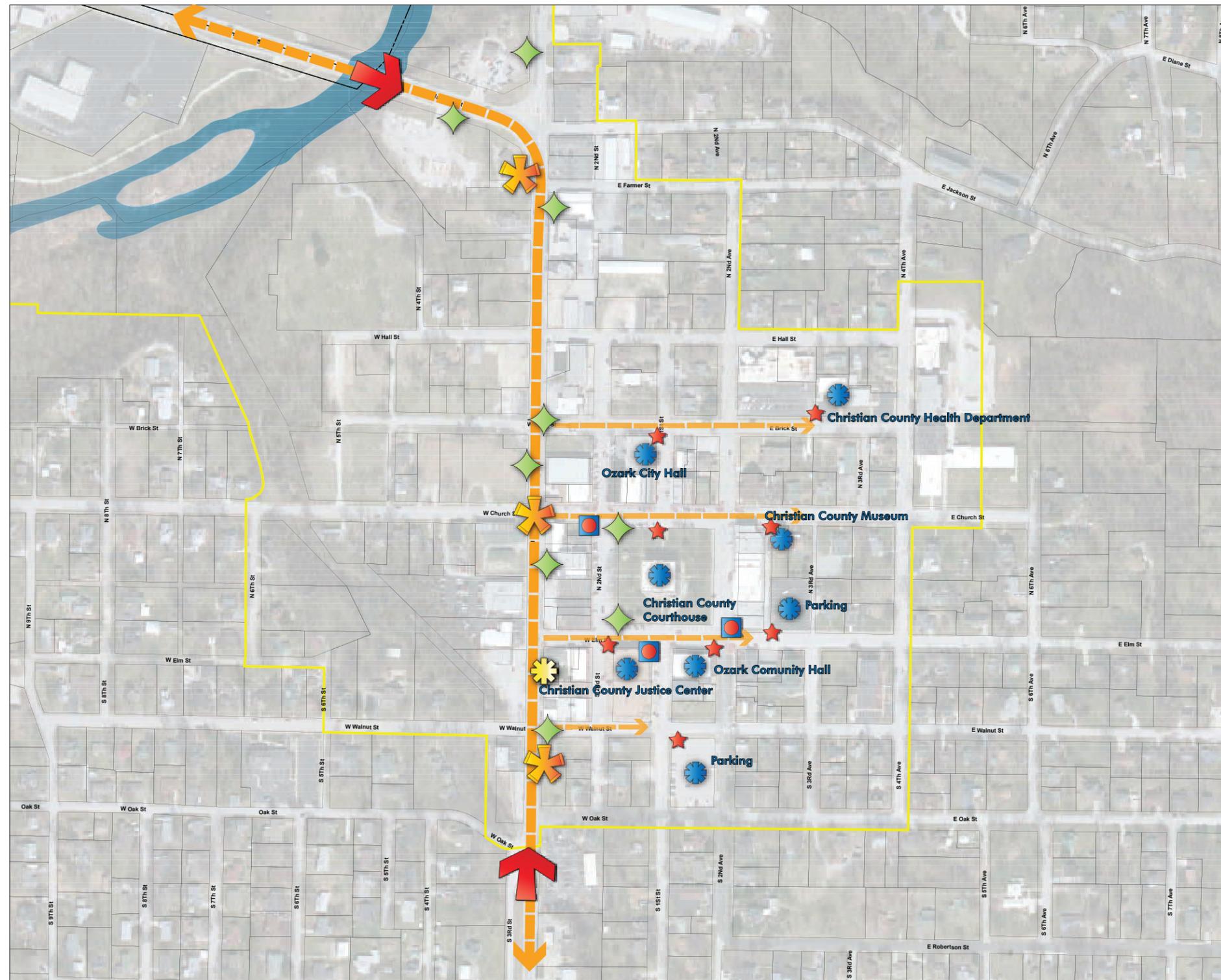
# Ozark Wayfinding Plan

## City-wide Plan

### Legend

- Major Destination
- Recreational Destination
- Gateway Signage
- Trailblazer Sign
- Proximity Sign
- Historic Downtown Area
- Highway Commercial Area
- Parcels
- City Limits
- Major Traffic Routes
- Major Traffic Entry Ways
- DREAM Boundary
- Finley River





## Ozark Wayfinding Plan

### Downtown Detail

#### Legend

-  Major Destination
-  Recreational Destination
-  Gateway Signage
-  Trailblazer Sign
-  Proximity Sign
-  Destination Sign
-  Pedestrian Kiosk
-  Parcels
-  City Limits
-  Major Traffic Routes
-  Major Traffic Entry Ways
-  DREAM Boundary
-  Finley River



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## 8.0 Implementation and Next Steps

Downtown Ozark is full of beautiful historic structures and has great natural beauty, being situated on a crest overlooking the Finley River. The primary goal of the building design guidelines is to promote the preservation and rehabilitation of historic buildings and the development of appropriate new construction in Downtown Ozark, which includes both the Courthouse Square and the LCRA property. The historic buildings, architectural character, and pedestrian-friendly nature helps to define Downtown and makes it an attractive place to live, work, and visit. Ozark continues to have a large amount of untapped potential as a destination. Protecting the character and scale of Downtown will allow Ozark to realize this potential.

The design guidelines and the concepts developed in this report were developed as a resource for both property owners and the City to utilize in the rehabilitation of existing buildings and the development of new buildings – including the LCRA property. The long-term goal of these guidelines is to encourage a high standard of quality rehabilitation and new construction Downtown. Implementation of the guidelines and development concepts described herein will sustain the character of Downtown Ozark. The following is a list of tasks for implementing the recommendations found in this report:

### ★ **Implement Wayfinding Plan**

Working with the Downtown group and the Chamber of Commerce, the City should begin to implement the wayfinding concepts found on page 101. Due to cost considerations, it is recommended that the City implement the wayfinding plan found on pages 102 and 103 incrementally, beginning with the most heavily traveled intersections and driver/pedestrian decision points. In addition, as new development is constructed, particularly in the LCRA property, wayfinding signage and gateway features should be incorporated into the development plan for the property(ies).

### ★ **Incrementally Improve 3rd Street Building Façades**

During the public interviews and meetings held for the building and streetscape design guidelines report, the most frequently identified concern was the poor initial impression that Downtown Ozark gives visitors, most of whom arrive from 3rd Street. While gateway features and wayfinding signs will assist in notifying visitors that they have arrived Downtown, the overall cluttered, industrial, and run-down look of 3rd Street must be improved to match the charming and attractive appearance of the Courthouse Square. The City should work with individual property owners to implement the improvement concepts found on pages 48 through 53. The City should consider an overlay district for the corridor and encourage improvements to properties as they come up for sale or change uses.

### ★ **Completely Revise the Zoning Standards for LCRA Property**

The City adopted the Ozark Revitalization Plan for the LCRA property after it was drafted in June 2006. The plan served as the basis for the PUD – Planned Unit Development zoning adopted for the LCRA redevelopment area (formerly called the Finley River Neighborhood District. As mentioned in this report, the real estate market collapsed soon after this PUD was enacted and the very ambitious plans for the LCRA property were never begun. Unfortunately for the future development potential of the area, the PUD still stands for the parcels, even though the project is not likely to ever be constructed. Based on the development plans and concepts on pages 77 through 85, it is recommended that the

City once again issue a request for proposals for the LCRA property to solicit developers interested in developing the area according to the guidelines outlined in this report. Once a development proposal is received and has been reviewed by city staff, the Preservation Commission, and the Board of Alderman, the PUD should be replaced with a new plan, based on the development plans, concepts, and design guidelines outlined herein. Special attention must be paid to ensure these plans complement the rest of Downtown, are appropriately scaled, have buildings pulled to the street, have significant density, are pedestrian-friendly, and represent a market-based approach.

★ **Strengthen the Existing Design Standards**

The City of Ozark updated its zoning code in 2009, specifically to revise its historic preservation and signage standards, among other sections. Most of Downtown (outside of the LCRA property) is covered under this revision, which includes Section 420 – Historic Preservation zoning designation of the City’s Zoning Code. The remainder is covered under C-4 Commercial Central Business District zoning regulations. Section 420 contains many protections and design requirements to ensure that historic properties are protected. These include the requirement that the Ozark Historical Preservation Commission review “new construction, alterations, removals, and demolitions within the boundaries of a landmark or district.”

The City should task the Preservation Commission with reviewing the existing guidelines for Section 420 as well as those for the C-4 zoning designation in light of the recommendations found in this report. Where appropriate updates need to be made, the Preservation Commission should recommend the Board of Alderman make changes to the zoning code in order to incorporate the design guidelines as described in the preceding sections of this report.

## 9.0 Glossary

**BASE:** The lowest part of a column, below the shaft; the supporting, or lowest, part of a building.

**BALUSTRADE:** A railing or low wall consisting of a handrail on balusters (vertical posts) and a base rail.

**CAP:** The top member of a column or pilaster.

**CLAPBOARDS:** Long, thin horizontal boards with a triangular cross section that are overlapped and applied as the exterior surfacing material on homes and buildings.

**CLERESTORY:** An upper portion of a wall which has windows for the purpose of admitting light into a large room.

**CONTEXT:** The surrounding environment (streets, buildings, landscape, etc.) in which a building or site exists.

**COPING:** A covering (or capping) course on the top of a wall or parapet.

**CORBEL:** An architectural member (of stone, wood or metal) which projects from the side of a wall to serve as a support for another element, such as: a cornice, the spring of an arch, a balustrade.

**CORNICE:** A projecting ornamental molding which caps the top of a building.

**DORMER:** A window set vertically in a small gable projecting from a sloping roof; the roofed projection in which this window is set.

**ELEVATION:** A scaled, non-perspective drawing of a building façade.

**FACADE:** An exterior face of a building, usually the front.

**FASCIA:** A horizontal band of vertical face trim.

**FREESTANDING SIGN:** A sign which is detached from the building, and is mounted to columns, posts, or any upright member that is supported from the ground or other object; or a detached sign which is erected on the ground.

**GABLE:** The triangular wall section, formed by ends of a sloping roof.

**HOOD MOLDING:** A projecting molding on the face of a wall, over an opening (doorway or window), to deflect the rain.

**INDIRECT LIGHTING:** Light from a concealed source, which reflects onto the sign face.

**INTERNAL ILLUMINATION:** The means of lighting from a concealed or contained source within the sign, which becomes visible through a translucent surface.

**KICK PLATE:** A solid panel beneath a storefront display window.

**LANDMARK:** A prominent building or feature officially designated as having special status and protection.

**LATTICE:** An openwork screen or grill made of interlocking or overlapping strips.

**LINTEL:** A horizontal structural member (such as a stone or beam) which spans an opening.

**LUMINAIRE:** A complete lighting unit or the housing for a light bulb or lamp.

**MOLDING:** A decorative, or shaped strip of wood, metal, brick, etc., usually mounted horizontally, and used to ornament or finish the surface of a structure.

**MOTIF:** A significant, repeated element of design in a composition.

**MONUMENT SIGN:** A free-standing sign, generally low to the ground with a continuous connection to the ground (as opposed to being supported on a pole).

**PARAPET:** The top section of a wall which projects above the roof line.

**PRESERVE:** To protect and keep in an unaltered condition. Preservation usually includes the overall form of the building, its structural system and finishes, decorative details, and even landscaping. Preservation may also include keeping alterations and additions that have become important.

**RECONSTRUCT:** To reproduce, in detail, a structure as it existed at some time in the past, either through the original construction methods, or other methods which produce the same visual result. Accurate reconstruction requires knowledge and evidence of the original design.

**REHABILITATION:** The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values.

**REMODEL:** To remake; to make over. In remodeling, the appearance is changed by removing original detail and altering spaces. New materials and forms are installed. Applying a modern front to an older building is an example of remodeling. Often, these changes are not reversible.

**RENOVATION:** The act or process of modernizing a building without making an effort to retain historically significant architectural features. Renovation permanently destroys the historic integrity of a building.

**RESTORATION:** The act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work and/or by the replacement of missing earlier work.

**SHAFT:** The main portion of a column, between the base and capital.

**SILL:** The bottom horizontal member of a window or door frame.

**SPANDREL PANEL:** A sculpted panel or other decorative element put in the space between the top of a window in one story and the sill of a window in the story above.

**STABILIZE:** To make resistant to change in condition. A building is usually stabilized to retard deterioration until it can be repaired. A weather-resistant closure and a safe structural system are minimum stabilization efforts.

**STRING COURSE:** A thin projecting horizontal strip of masonry on the façade of a building.

**TERRA COTTA:** A decoratively molded ceramic material, often glazed, used for architectural motifs or ornamentation on a building.

**TRANSOM:** A horizontal cross bar in a window, over a door or between a door and the window above it. This also refers to the window (often hinged) above a door.

**VOUSOIR:** One of the wedge like stones of which an arch is composed.

**WEATHERBOARDS:** Long, thin horizontal boards with a square cross section that are overlapped and applied as the exterior surfacing material on homes and buildings.