



DOWNTOWN
REVITALIZATION &
ECONOMIC
ASSISTANCE FOR
MISSOURI

BUILDING AND
STREETScape
CONCEPTS

MAY 2014



ACKNOWLEDGMENTS



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1.0 INTRODUCTION

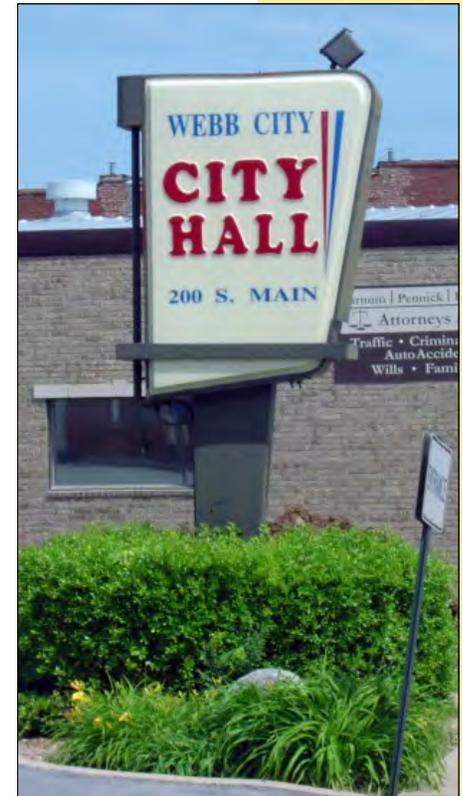
1.1 Historic Overview

Webb City is located in southwest Missouri in Jasper County. The City is about six miles to the northeast of Joplin, Missouri and five miles east of the Kansas State Line. Webb City is part of the Joplin, Missouri Metropolitan Statistical Area. The state highways of Business 71 and 171 intersect in town and Webb City is just northwest of State Highway 249. Main Street is also State Highway D and intersects in Downtown with Broadway or Highway HH. Interstate 44 is about seven miles to the south.

Webb City's was platted by John C. Webb in 1875 and incorporated in 1876 with a population of 700. Webb had discovered lead on his property in 1873 and by 1876 had leased the land to Center Creek Mining Company. By the 1890's, over 700 lead and zinc mines were located within Webb City and nearby Carterville. The St. Louis and San Francisco Railroad was built in 1879 and the Missouri Pacific line built in 1881. By 1900 the population had grown to 9,200 and remained a mining hub through World War I. As mining started to decline, Webb City turned to industrial and agricultural production in an attempt to diversify its economy. In the 1930's and during World War II, the area manufactured explosives. As of the 2010 Census, Webb City has grown to about 11,000 people living in about 4,200 households.

Downtown's building stock can be characterized as a core of multi-level commercial buildings with a unity of materials, scale, and style, around which modest homes had been constructed. Some Downtown commercial buildings possessed intricate design elements and the street had an appearance of rhythm and order. Similar patterns and elements were repeated on façades, providing each building with a visual connection to its neighbors.

In 1926, U.S. Route 66 was established from Chicago, Illinois to Santa Monica, California. This famous highway was located along what is now Interstate 44 and helped give rise to the nomadic automobile subculture. Communities near Route 66, such as Webb City, developed an early automobile-centric architecture and other visitor-oriented amenities that were loosely regulated and often oversized, flashy, or otherwise unique. The "Mother Road" as described by John Steinbeck, encapsulated a feeling of freedom to travel or wanderlust. Many also dubbed it "The Main Street of American" because it connected a great number of people throughout the country. The highways influence came to mean a uniqueness of commercial building architecture, but also in land-use and wayfinding. Architecture styles attributed to the era include Art Deco, Modern, Roadside, Novelty, and Goochie. Land uses were often combined and catered to automobile convenience or parking. Wayfinding was designed for maximum attention or repetition.



Route 66 era-style signage found in Downtown Webb City.

By the 1950's, American lifestyles had changed with the rise of highway construction and affordable automotive travel. Neighborhoods and commercial areas shifted away from the traditional downtown business district. While still the center of much community life, downtown's started to experience a loss of commercial activity. Consumers expected easy vehicular access and parking. New and modern design was preferred over traditional and old construction. As a result, by the mid 1970's many American downtowns suffered from a lack of investment and declining importance in the minds of residents.

Historically, commercial façades in Missouri were one, two, or three-stories of masonry with a high level of detail in the upper façade. Masonry details would have included corbelled recessed window panels, stone window sills, and arched window openings. Some of the buildings would have also featured painted pressed metal cornices with rich architectural details such as the building name and year of construction. Façade construction may have incorporated cast iron and pressed metal. Today, many façades in Downtown Webb City still demonstrate these historical features. However, some buildings in Downtown have lost their historic character as their detailed architecture was removed and replaced with inappropriate materials. In some cases, entire buildings have been removed and replaced with new buildings that fail to account for the existing historical context of Downtown. In some cases, Route 66-era, automobile-oriented buildings are located adjacent to historic structures. This situation can create conflicting building styles, but also adds to the unique qualities of Downtown Webb City.

The historical streetscape was generally not spared either; historic light poles and fixtures were removed and replaced with out-of-scale "cobra-head" fixtures and poles. Collectively, the changes to the built environment of downtowns were not positive and contributed to the loss of main street business activity. In Webb City, the Downtown streetscape has been reconstructed with elements of a historically complementary nature.

1.2 Intent of Concepts

This document represents conceptual planning recommendations for the City of Webb City to consider regarding the future policy and procedural decisions that affect Downtown buildings and public spaces. The intent of this document is to help preserve the architectural character and improve the visual appearance of Downtown Webb City. Some property owners may find inspiration from the illustrations. Included are recommendations to help recapture the characteristics of existing buildings and guide new private and public development; including in-fill construction.



Examples of architectural details found in Downtown Webb City.

1.3 Existing Context

Downtown Webb City, Missouri has a typical grid street layout with a commercial core that stretches along Main Street/Highway D, the primary north/south corridor, from Highway 71 north. Broadway Street/Highway HH provides the primary east/west traffic artery and is the north/south dividing street. With the commercial area being primarily linear along Main Street, there is no main orientation or meeting point. However, there are several parking lots and distinctive businesses. The DREAM Study Area includes the 144-acre King Jack Park located south of Highway 71 at South Main Street.

There is currently no designated Downtown Webb City Historic District and only one property, located at the Main and Broadway streets, is listed on the National Register of Historic Places. There are likely many more properties in Downtown that would qualify for listing on the National Register. A listing on the National Register does not include any state or federal level preservation limitations. However, the City could bolster its commitment to historic preservation by forming a Historic Preservation Commission (HPC), designating Downtown as a local historic district, and developing design guidelines for all new construction (including rehabilitation of existing buildings).

The massing of buildings in Downtown Webb City is significant and fairly dense, particularly along Main Street and the first block off of Main Street on the side streets. Many Downtown Webb City buildings retain their architecturally significant elements. However, some buildings have been inappropriately altered and now present diminished historic integrity. Some Downtown buildings have been compromised by the application of inappropriate materials, upper-floor window coverings, signage, and awnings. Interspersed among the historic properties, are several buildings that had been developed to serve the motoring visitors along Route 66. These properties have a unique style that does not necessarily detract from the overall historic fabric of Downtown.

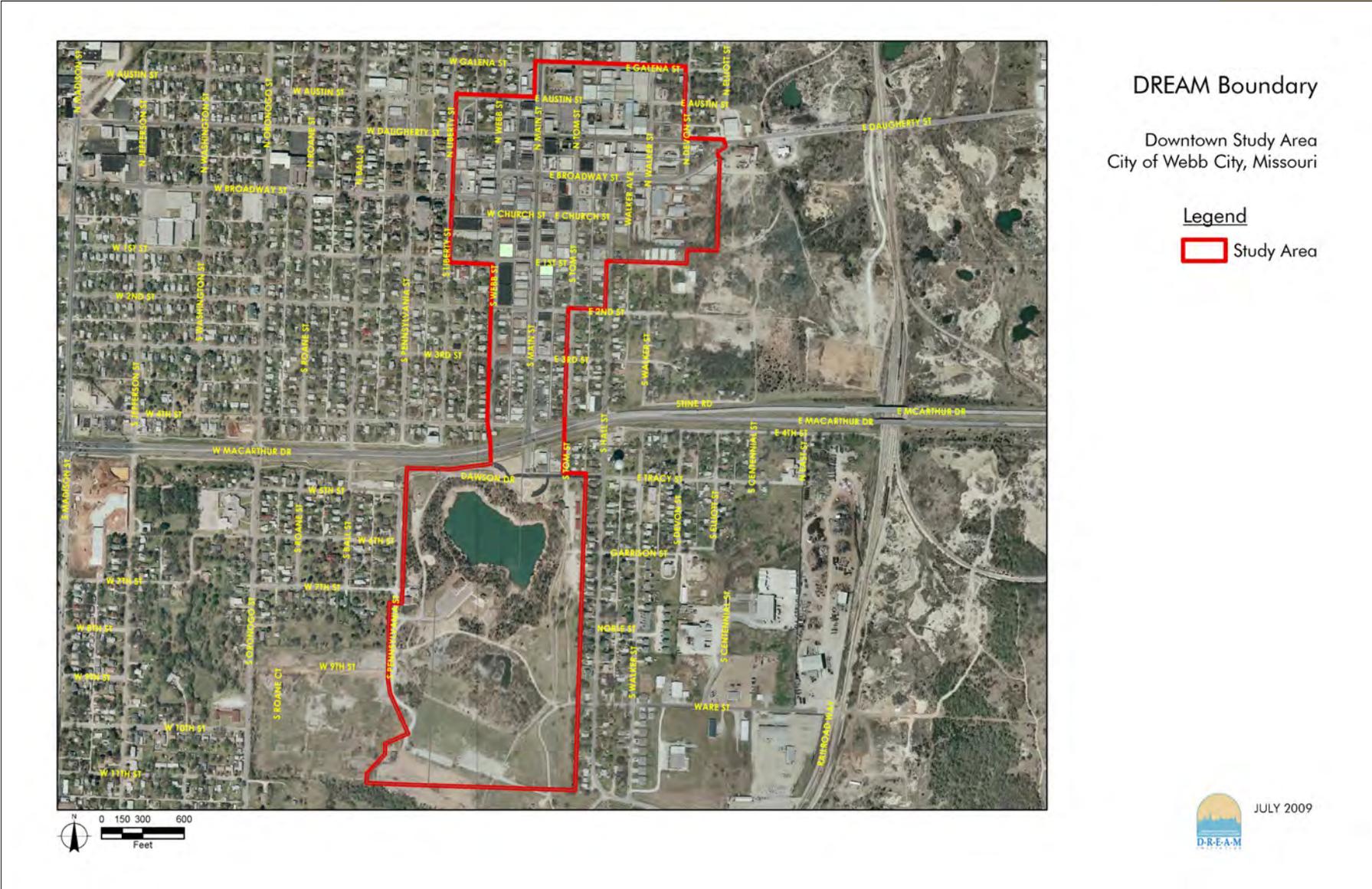
To complement the buildings, the City of Webb City has worked to construct a historically sensitive streetscape project along Main Street, primarily from 1st Street to Austin Street. This feature of Downtown is very positive and includes period-style lighting, pavers, decorative street signs, street trees, banners, and trash receptacles.

The following page provides a map of the Downtown Webb City DREAM Boundary.



Downtown Webb City streetscape elements.

1.4 DREAM Boundary Map



2.0 SUSTAINABLE DESIGN

2.1 Introduction

The construction of sites and buildings have a significant impact on the natural environment. The operations of a site and/or a building, can also affect the air, land and soil of the downtown. Sustainable Design measures seek to lessen the impact on the natural and built environment. Such efforts also aim to increase the efficiency at which buildings operate, in regard to energy use and operating costs. The design process is comprehensive and includes site selection, building orientation, and specification of sustainable materials and energy efficient operating systems. These design properties should be considered with private buildings, as well as with the public streetscape.

Downtown Webb City is a built environment of many historic buildings, modern buildings, public streets, parking lots, a few vacant lots and other open space. Sustainable design measures can be applied to existing buildings, new buildings, and streetscape and site improvements. The U. S. Green Building Council (USGBC) has become the leading organization in developing standards for sustainable design for buildings. The USGBC's certification system is known as Leadership in Energy and Environmental Design (LEED). The majority of LEED certified buildings are new construction projects; however the USGBC has also developed standards for the upgrade of existing buildings. Sustainable design is a broad and encompassing initiative which strives to create a built environment which is good for both man and nature. The following recommendations only introduce the basic fundamentals of sustainable design regarding downtown buildings and environments. For additional information beyond these guidelines, resources include:

- USGBC: www.usgbc.org
- Whole Building Design Guide: www.wbdg.org
- American Society for Testing and Materials International (ASTM): ASTM E2432— Standard Guide for General Principles of Sustainability Relative to Buildings: www.astm.Standards.e2432.htm
- Sustainable Sites Initiative: www.sustainablesites.org
- Grow Native: www.grownative.org
- Smart Growth America: www.smartgrowthamerica.org



Permeable paving used for parking areas to allow storm water to percolate back into the soil.



Interior flooring fabricated from bamboo; a rapidly renewable resource.

2.2 Fundamentals

Sustainable design measures are constantly changing, however there are six fundamental principles which constitute sustainability:

- 1) **Optimal Site Potential:** Consider site selection, building orientation and existing natural features of a site, including topography, drainage, landscape and natural habitats. The rehabilitation and reuse of existing buildings should always be evaluated as an alternative to new construction.
- 2) **Efficient Use of Water:** The design and use of water systems in a building maximize efficiency and recycle water for on-site use when feasible. Site design should seek to reduce storm water run-off from the site. Use Best Management Practices to limit storm water run-off, clean storm water, and prevent suspended pollutants from reaching the sewer system.
- 3) **Environmental Materials and Resources:** Utilize building materials with a high percentage of recycled content or contain rapidly renewable materials such as cork flooring, bamboo cabinetry, wool carpeting, etc. Specify or use materials or items which are manufactured within proximity to the project site. Ideally, this proximity is no more than 500 miles.
- 4) **Optimal Energy Use:** The operation of a site and building identify methods for increased energy efficiency or use renewable resources such as solar or geothermal energy.
- 5) **Interior Environmental Quality:** Identify methods for creating a healthy environment, and increasing the comfort of building users. Proper ventilation, use of natural light, and moisture control are a few methods to ensure a quality interior space.
- 6) **Optimal Operations and Maintenance Methods:** Utilize building systems, furnishings and finishes which will have minimal operations and maintenance needs. Such systems will require less energy, less water, and can be maintained with natural cleaners which are non-toxic to the environment or occupants.



"Green Roofs" reduce storm water run-off, reduce heat gain, and provide aesthetic areas for building users.



Solar panels can be used to provide an energy source for lighting, building use, signals, and even automobiles.

2.3 Elements

Sustainable design elements vary in intensity and cost to implement. The following list seeks to introduce only a few recommendations which are applicable to Downtown Webb City:

- **Parking and Service Areas:** Minimize storm water run-off by using pervious pavement materials such as pervious paver systems or pervious concrete. Such systems will allow storm water to percolate into the soil and not into the public storm water sewer system.
- **Building Materials:** Utilize materials which are composed of recycled materials or manufactured from rapidly renewable materials, which are made from plants that are typically harvested within a 10 year cycle. Examples include: bamboo flooring, linoleum flooring (made of wheat flour and linseed oil), cotton batt insulation, and wheat board cabinetry. Recycled bricks from demolished buildings should also be used for new building construction or restoration projects.
- **Alternative Transportation:** Promote by providing secure bicycle storage and changing/shower facilities for employees.
- **Solar Energy Alternatives:** Install solar panels to supplement the power system for commercial and residential buildings. Utilize prefabricated solar water heaters to provide the majority of the hot water needs for buildings.
- **Stewardship:** New wood products, including construction lumber, should be certified by the Forest Stewardship Council, which promotes responsible forest management.
- **Lighting:** Develop a lighting plan for public spaces which minimizes excessive lighting, which affects night sky viewing and the migratory patterns of birds. Flags which require lighting should be lit from the top shining down on the flags instead of being lit from the ground, projecting light into the sky.
- **Operations:** Use timers on public fountains and lights in non-essential areas to shut off lights after 1:00 a.m., in order to reduce energy consumption.
- **Landscaping:** Plant native landscape materials which can survive on natural rainfall once established.
- **Street Furnishings:** Specify site furnishings such as benches, waste receptacles, bollards, and planters which are made from recycled plastic materials.
- **Water Conservation:** Capture rain water runoff from roofs in rain barrels for irrigation use or direct to rain gardens on site. Inside buildings, consider waterless urinals or low flow water closets to limit potable water use.



Permeable pavement system installation.



Rain garden with native landscape plants.



Solar water heater.

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3.0 BUILDING DESIGN CONCEPTS

3.1 Rehabilitation and Maintenance of Existing Buildings

Any original element or material that still exists on historic or Downtown buildings should be retained. These original components provide historic value that cannot be replaced and are particularly important for storefronts. Prism glass in transom windows or a decorative wooden door with beveled glass are examples of original materials to retain.

Replacement of missing architectural elements should be based on accurate duplications of original features. When an entire detail must be reconstructed, the new material should match the original in design, color, texture, and other visual qualities. Where reconstruction of an element is impossible because of a lack of historical evidence, then a new simple design complementary of the building in size, scale, and material may be considered. Use elements that complement the surrounding buildings. Often a simplified interpretation of similar features found on nearby buildings is attractive and acceptable.



Historic Building in Downtown Webb City.

3.1.1 Rehabilitation, Restoration, and Renovation

The Secretary of the Interior's Standards for Rehabilitation (a summary of which is found in the Appendix as Exhibit A on Page 59) define rehabilitation as "The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values."

Rehabilitation is distinguished from *restoration*, which is defined as "The act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work or by the replacement of missing earlier work." There are many lightweight and durable materials available to replicate historic building features such as trim, cornices, medallions, columns, lintels, and other details. However, if research does not yield evidence of a particular design, the replacement element should be simple and complementary to surrounding buildings.

In contrast to rehabilitation and restoration, renovation seeks to modernize a building. Little attention is paid to retaining historically significant architectural features. Renovation, by its very nature, destroys the historic integrity of a building. Once a building is renovated, it may no longer be eligible for State or Federal Rehabilitation Tax Credits or listing on the National Register of Historic Places.

As property owners prepare to invest in their buildings, they should seek professional guidance for rehabilitation or construction projects. The Secretary of The Interior's Standards for Rehabilitation and various Preservation Briefs are included on the National Park Service website at www.nps.gov. Property owners interested in applying for Historic Tax Credits should contact the Missouri State Historic Preservation Office at www.dnr.mo.gov/shpo/.

3.1.2 The Benefits of Rehabilitating Buildings

The rehabilitation of buildings will provide several long-term benefits for the property owner and Downtown Webb City. Appropriate rehabilitation of a building adds value. Improvements to the façade and updates to mechanical, electrical, and plumbing systems are investments that help limit maintenance costs. Repairs also address codes and safety regulations and make the building more marketable. A well-maintained building displays a positive image of the occupant, owner, and Downtown.

Buildings in Downtown Webb City demonstrate a wide range of design character. There are some that show significant architectural character, and some that are very simple and plain. Collectively these buildings contribute to the atmosphere of Downtown. Buildings which have lost their character due to alterations or neglect detract from the Downtown experience. Restoration to the original design is not always necessary, unless the building is on the National Register of Historic Places. The objective is to maximize elements that enhance the Downtown Webb City experience and minimize detracting elements.

The façade of a building is the first image presented to every potential customer of the business which is located in the building. This image needs to be positive so the visitor will want to enter the building. The rehabilitation of the façade is vital for the survival of the business. An appropriately rehabilitated façade is inviting and sets a high-quality standard for other buildings. Façades combine with public elements to create the outdoor living room of Downtown Webb City. This space is at the core of the community and should be alive with activities and events. As such, proper maintenance of all elements is essential. Downtown revitalization is an ongoing process that takes time and will evolve, pick-up speed, slow down, be applauded, and be criticized. The one constant should be the desire to adjust Downtown Webb City to an atmosphere that is attractive to residents, businesses, and visitors. This effort begins with preserving existing Downtown buildings.

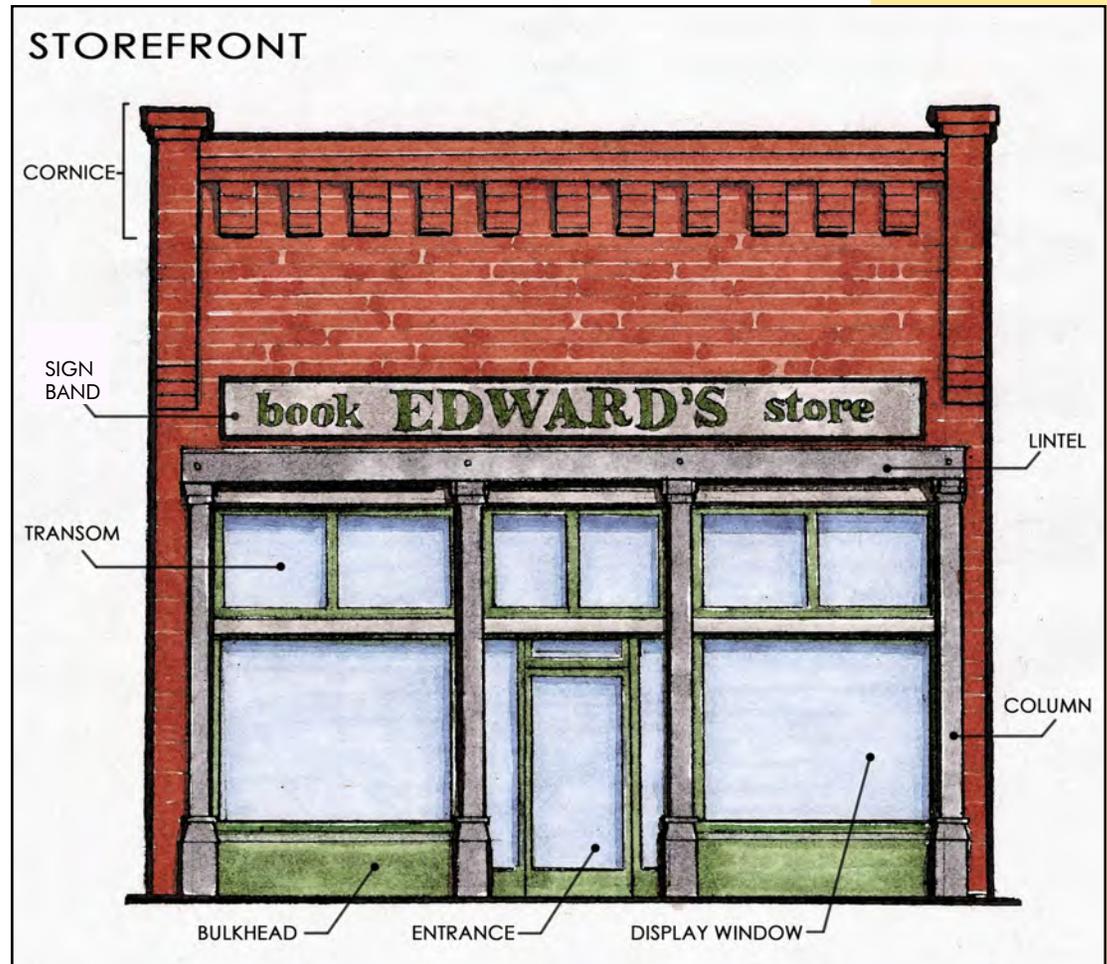


The Downtown Webb City Post Office.

3.1.3 Building Zones

Improvements to buildings will be discussed in the context of three distinct zones; the **Storefront (Façade)**, the **Upper Façade**, and the **Rear (or Side) Elevation**. The storefront is the most critical element, as it provides the interface between the business and the street. Components of the storefront include some upper façade elements, but the primary focus is on the building façade at the pedestrian level. A storefront zone and some important upper façade elements are shown on the illustration at right and discussed in Section 3.1.15 on Page 23.

The upper façade is found above the pedestrian level on the front of the building. This is an important part of the building façade which should be well-maintained by the property owner. The Upper Façade Zone is discussed in Section 3.1.16 on Page 24. Rear or side elevations of a building may present opportunities to create a pleasing shopping atmosphere. Rear and side elevations are discussed in Section 3.1.17 on Page 25.



3.1.4 Façade Elements

The various elements of a façade must be balanced. Restoring appropriate massing, building and floor heights, proportions, roof lines, materials, and setbacks are critical considerations in rehabilitation construction. Other aspects such as architectural details, colors, and cornices are more important to the restoration of historic buildings, but can be used effectively in rehabilitation construction. Developing a balance of all façade elements can allow a building to be very individual in its character, but at the same time be a complementary thread woven into the overall fabric and feel of Downtown.

3.1.5 Rhythm

The defined rhythm of Downtown Webb City should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing. Proper repetition of architectural details and orientation to the street, along with vertical elements such as entrances, lighting, and street furnishings can help develop the rhythm of a specific block.

3.1.6 Alterations

Encourage removal of inappropriate alterations or additions that disrupt the fabric of the storefront. It is possible that non-historic and new construction can complement the building fabric that has developed, therefore some alterations may not need to be removed. Decks, structures providing access for people with disabilities, and other “detachable” alterations can be used, but should be as unobtrusive as possible and located on the rear or sides of the building.

As a rule, any and all alterations or additions to the Upper Façade Zone should be removed. Alterations in this zone can significantly change the overall appearance of the building. Signage and building lighting should be carefully considered, as these elements are generally restricted to the storefront. Avoid removing or altering any historic material or significant architectural features. Care should be taken during the removal process to avoid damage to original elements hidden behind the alterations. When disassembly of a historic element is necessary, use methods that minimize damage to the original materials.

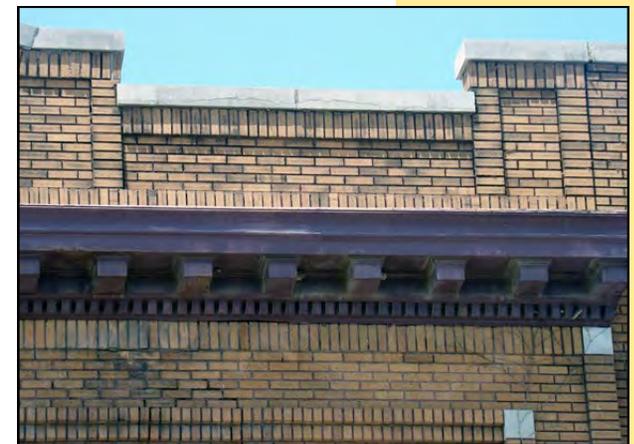


This well-maintained façade and building is located on Webb Street in Downtown Webb City.

3.1.7 Masonry

Masonry is typically the preferred façade material for Downtown. Most existing construction will utilize some masonry. In most instances, metal and wood siding is not a suitable choice for Downtown building façades. These types of siding provide harsh lines, stark contrast, and no relief or warmth to the buildings. If wood was the historic material, it may be restored. Effective recommendations related to the treatment of masonry façades include:

- Maintain the original color and texture of masonry walls. Stucco or paint should not be removed if this was the historic covering and only applied if it was the historic covering. If painted or stained masonry is going to be returned to its original state, a minimally intrusive removal process should be used.
- Clean masonry and mortar only to limit deterioration or to remove heavy soiling. Avoid techniques such as sandblasting, caustic solutions, and high-pressure water blasting, as they may erode the surface and accelerate deterioration.
- Masonry restoration, particularly on historic structures, should always be done by experienced professionals.
- Damaged bricks and stone should be repaired or replaced with similar color, texture, and style of masonry products. Re-point masonry walls when there is evidence of disintegrating mortar, cracks in mortar joints, loose brick, or moisture retention in the walls. New mortar should duplicate the old mortar in composition, bonding strength, profile, color, and texture. Do not use cement mortar in brick construction as it is too hard and will result in cracking of the softer brick material.
- Portland cement as a patch for masonry is unacceptable.
- If a historic façade has been covered with metal or wood siding, it should be removed. Exposing the underlying brick masonry will help re-establish the character of the building and contribute to the visual continuity of the block. Metal cladding or siding can hide interesting details and may be easy to remove. If, after removing the covering, portions of the façade must be replaced, use a material similar to the original façade in color and texture.
- Regular maintenance of foundations is required to prevent structural and water damage. Any water-proofing methods for foundations should be applied beneath the finished grade or inside the structure.

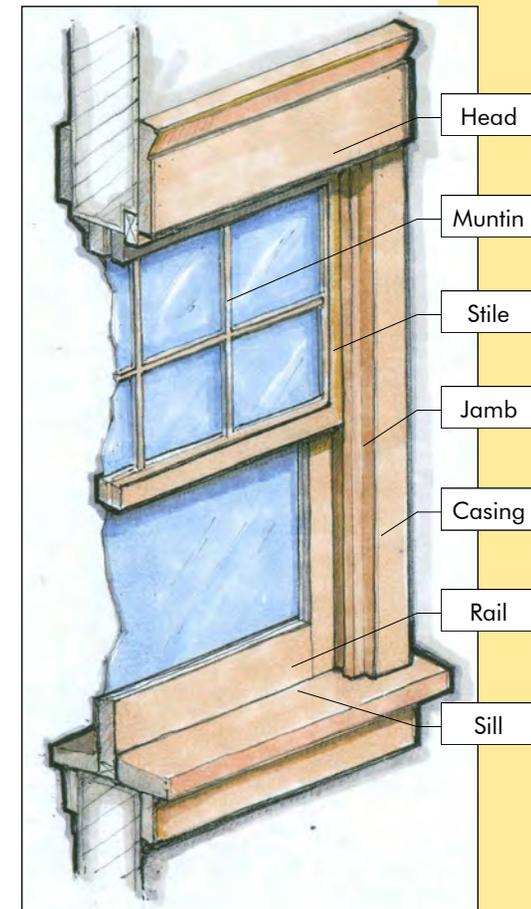


Examples of poor masonry conditions on buildings in Downtown Webb City.

3.1.8 Windows

Windows are a major feature of the building exterior and vary with each building style. Windows have a proportional relationship to the structure as a whole, and they also have a decorative function. The shape and glazing pattern of windows on a building may be one of the principle characteristics in identifying its historic period and style. Thus, if original windows are removed and replaced with incompatible modern windows, the basic character of the building will be altered substantially. Recommendations for the treatment of windows in Downtown buildings include:

- The number, size, and locations of existing window openings should be retained. Do not “block-in” windows to reduce the size of the window opening or to fit stock window sizes. New window openings should not be added on elevations that are subject to view from a public street. If the original window still exists, it should be restored.
- Retain and repair window frames, sash, decorative glass, panes, sills, heads, hoodmolds, moldings, and exterior shutters and blinds. Replacement window parts should duplicate the material and design of the original part. Reuse parts in their original configuration if disassembly is required.
- If duplication of the original window or window part is not technically or economically feasible, a simplified version of the original may be acceptable as long as it has the same size and proportion. Modern window types that are inappropriate in commercial buildings include large picture windows, casements, and bow windows, unless they are original to the structure.
- Do not install shutters on windows that did not originally have shutters. The shutter should measure the full height of the window and half its width, so as to cover the entire window when closed. Fasten shutter to the window frame and not the siding.
- Inappropriate modern window features such as plastic and metal awnings or fake, non-operable, synthetic shutters and blinds, distract from the historic appearance of a building and should be avoided.
- Storm windows should have wooden frames, or if metal, should be anodized or painted to blend with the trim and be installed on the interior, rather than exterior.



- Upper floor windows are typically vertically-oriented and uniformly spaced across the building front. These upper façade windows help establish rhythm and are an important unifying feature of Downtown.
- Masonry infill, wood panels, or mismatched windows should be replaced with appropriate materials.
- If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the interior window.
- Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be considered.
- Encourage window shades or curtains in colors that coordinate with accent trim.
- Property owners should be encouraged to keep interior coverings, shades, or blinds closed on upper floor windows. Vacant, ground-floor spaces should also have displays or other materials to reduce the visual impact that a vacant building can have on Downtown.



The upper floors, along with the storefronts, of these buildings should be restored to maintain block rhythm in Downtown Webb City.

3.1.9 Architectural Details

There are some intricate architectural details found on Downtown Webb City buildings. The existing details are the signatures of the builders and designers and represent a connection to Webb City's past. In typical modern construction, such details are omitted. Restoration of features such as cornices, medallions, ornamental glass, brackets, and brick patterns are often forgone in renovation work for covering that hides any hint of the heritage of the building. Exposing and restoring these elements develops a unique atmosphere that only Downtown can offer. Suggestions for treatment of these details include:

- Replacement of missing architectural elements should be based on accurate duplications of original features. In some cases, an entire element must be reconstructed. In the event that complete replacement is

necessary, the new material should match the original in design, color, texture, and other visual qualities. Photographic evidence is a good source for research.

- If the cornice is missing, a similar cornice of like size and scale should be installed. If no evidence exists as to form and detail, the reconstructed cornice should be as simple and non-intrusive as possible. A cornice is an important building element that leaves a very obvious absence if it is missing. If the cornice is intact it should be repaired and maintained.
- Where architectural details have been removed, refer to historic photos for details to use as patterns for new designs. Where exact reconstruction of details is not feasible, consider a simplified interpretation of the original, in which its major forms and lines are retained.

3.1.10 Awnings and Canopies

Awnings used in the Storefront Zone provide shade for merchandise, shelter for pedestrians, and bring a colorful accent to the building façade that can be changed frequently and without great expense. Canopies are more permanent structures built onto the front of the building and may include lighting for the sidewalk in front of the store. Upper window awnings provide shade and help establish rhythm along the street. The following suggestions enhance appropriate use of awnings and canopies:

- Mount the top edge to align with the top of the transom or the framing between the transom and the main display window. This will help strengthen the visual continuity of storefronts.
- Awnings should be installed over the original storefront opening and not extend beyond. Awnings over the building entrance will help customers navigate.
- Use multiple awnings on upper-floor windows, not one large awning. Do not use canopies on upper-floor windows. The use of a canopy or one large awning imbalances the façade and looks too imposing from the street view.



An inappropriate metal covering hides the handsome architectural details of the façade of this Downtown Webb City building. Unfortunately, the metal covering was repaired and remains in place as of the date of this report.

- Aluminum, steel, and wood shingle canopies are typically not original building material elements and tend to detract from the overall appeal of building façades. These canopies should be removed to expose and repair transom windows, the sign band, and any architectural details.
- Roll-up awnings were common on historic storefronts and can be restored with modern fabric. If a roll-up awning is not operable, the awning should follow the shape of an operable awning.
- Awning colors should coordinate with the palette for the entire building and complement any overall scheme established for Downtown buildings. Awnings on upper windows should match the storefront awning in color and material.
- “Quarter round” or “Barrel” awnings were not used on typical historic downtown buildings and should be avoided.
- Awning signage or lettering should not be allowed where another flush-mounted sign exists and may be limited to the vertical front flap of the storefront awning only; not the upper-floor awnings. Signboards under the awning to assist pedestrians, should be of a limited, uniform size and complement the awning and building.
- Awnings will wear and replacement should be acknowledged as an operating cost of doing business. Awnings can be changed every few years for a fresh look.

3.1.11 Entrances

An entrance is an important feature that affects all building tenants and beckons the visitor on the street. The primary building entrance should be obvious, but side and rear entrances should also be well-defined and attractive. Easily identified entrances assist in wayfinding for pedestrians and motorists. Suggestions for enhancing entrances include:

- Recessed entries allow customers to exit the main pedestrian flow on the sidewalk as they are being invited into a store. Maintain recessed entrances in good condition where they exist. These areas also provide protection from the weather, and the repeated rhythm of shade along the street helps to identify the



An example of a rounded awning and covered transoms on an otherwise attractive Downtown Webb City building.

entrance. If the original recessed entry has been removed, consider reestablishing it in the same location.

- Side and rear entries should be visible from nearby parking lots and should provide a clear, well-lighted pathway for pedestrian access.
- At least one public entrance to the building is required to be compliant with the Americans with Disabilities Act Accessibility Guidelines (ADAAG).
- Avoid doors with raised thresholds and consider using an accent color on the door.
- If there is only one business in the building, center the business signage over the main entrance.

3.1.12 Building Lighting

Buildings should be interesting to view at night, as well as by day. A well-lit façade or rear elevation creates a positive impression about Downtown. This lighting also can improve Downtown safety and security. Suggestions to enhance the building lighting in Downtown Webb City include:

- Use lighting as a subtle and complementary design element to draw attention to the building. Lighting that emphasizes architecture in the upper façade and rear elevation can provide attractive highlights.
- Warm-colored lighting of the storefront should accent the entrance, flush-mounted signage, and any architectural details.
- The light fixture design should be simple, non-intrusive, and in a style that matches the period of the building. Neon and cool fluorescent lights should be avoided and well as other lighting that is too intense and overpowering of the building.
- Building lighting on rear elevations should be similar to the lighting on the façade. This lighting should also provide illumination at the entry door and along the pedestrian path from the parking area.
- Building lighting should also incorporate sustainable design concepts as noted in Section 2.0 on Page 5.



Use lighting to highlight building, signage & entrances (St. Charles, Missouri at top, Washington, Missouri at bottom).

3.1.13 Business Signage

For a successful business environment, each Downtown merchant must have its own identity, but at the same time reinforce and complement the overall Downtown design. Effective signage identifies the business without detracting from the architecture of the building and distracting the pedestrian. Sign types and their locations should be kept simple and consistent for ease of public awareness. Signage should be restricted to the Storefront Zone or Rear Elevation and generally not allowed on the Upper Façade.

Business signage in Downtown Webb City consists of a variety of signs. There are some positive examples of attractive flush-mounted signs within the sign band and some poor installations where the signage is haphazard, excessive, or in an inappropriate location on the building. While such signage may not be in violation of the City Sign Code, the City may wish to review its sign regulations to determine if changes are desired. General recommendations to enhance the business signage in Downtown Webb City include:

—General Business Signage Design Issues

- A business sign should be a part of the building design and not hide architectural features or details. Flush-mounted signs that fit within the outer edges of the façade and in the sign band help reinforce the horizontal lines along the street.
- The size of the sign should be of an appropriate scale for the building and street. Large signs are not needed as downtown signage is oriented to the pedestrian, not the motorist.
- In general, for all signs, the material and color should complement other building and façade materials. Clashing colors and inconsistent material types can cause visual distress in the viewer or customer.
- Signs should be well-designed and professional with a simple message. The name and type of business should be sufficient. A logo or symbol of the type of business could also be included.
- Signs should be constructed of high-quality materials to withstand extreme weather conditions. Good craftsmanship results in longer service time and conveys a stronger image. A deteriorating sign presents a very poor image.
- Encourage innovative sign design to reinforce uniqueness. Mass-produced signs, such as rectangular plastic “box”-type signs with internal lighting, fail to make a lasting impression in the mind of the visitor.



Examples of existing business signage found in Downtown Webb City.

- Illuminate signs in such a way to enhance the overall composition of the façade. External lighting cast from period-style, non-intrusive fixtures is preferable to internal lighting.
- Rooftop, blade, pole, neon, electronic message boards, flashing or otherwise moving or animated signs, signs playing music or sounds, and billboard signage should be severely restricted in a downtown environment, if even allowed. Abandoned signs should be removed.
- Place signs near the business entrance to guide a customer's eyes to the door.
- Where several businesses share a building, coordinate the signs by grouping them onto a directory panel. Use similar forms or backgrounds that tie together visually and make each business sign easier to read.

—**Style and Location of Signs**

Projecting Signs: Encourage projecting wall signs that give the name, logo, or image of the product being sold. Projecting signs should have the following characteristics:

- **Material:** Unframed painted wood or metal panels hung from painted wall brackets. Wood signs with carved or sandblasted designs that are painted are also appropriate.
- **Lighting:** Non-illuminated or externally lit with appropriate fixtures is preferred.
- **Location:** The bottom of projecting signs should be no lower than eight feet above the sidewalk and the top of the sign should be below the building parapet or upper-floor windows.
- **Message:** The use of symbols instead of text on projecting signs can help a customer more easily identify and remember the store.



This Downtown Webb City building demonstrates well-restored elements including transoms, storefronts, upper floors, and appropriate signage in the sign band.

Wall Signs: These signs are painted on the brick wall or a panel above the storefront windows or on the side of the building. The old faded signs painted directly on the brick are commonly called “ghost signs” and should be preserved as historical building elements. Downtown Webb City was home to many wall signs, of which some evidence remains today. A restoration effort to bring back these signs could add to the historic atmosphere of Downtown. Generally, wall signs should have the following characteristics:

- **Material:** Painted directly on the building brick or on wood or metal panels. Signs painted on brick typically had white lettering on a black background. Signs that advertised a product, such as Coca-Cola, were multi-colored.
- **Lighting:** Non-illuminated or externally lit with appropriate lighting fixtures.
- **Location:** Many of these signs were located in recessed brick panels above the storefront. Research historic photos for locations of original signs that may still exist beneath old paint or panels. Wall signs should not be located above the building parapet.

Window Signs: Painted or foiled lettering on the display window glass often advertised a doctor, dentist, or attorney. Signs of his type should have the following characteristics:

- **Material:** Painted, or gold or silver foil, lettering in colors that complement the building’s paint scheme.
- **Lighting:** Natural lighting or the inside lights of the building.
- **Location:** The glass of the entry door or on the display window at eye level. These signs are fairly simple and should not dominate the window to allow an unobstructed view of store merchandise. These signs can also be used in upper-floor windows to identify upper-floor businesses.

Awning and Canopy Signs: Lettering on awnings or canopies should only be used where there is no flush-mounted sign. However, a simple logo or symbol can help identify the business. These signs should have the following characteristics:



An example of the Route 66 era architecture in Downtown Webb City. This building now serves as the Chamber of Commerce. Research has shown, and some evidence remains, of several wall/ghost signs on the wall of the building behind the Chamber.

- Material: Lettering on awning fabric or painted on wood or metal panels.
- Location: Six to eight inch high lettering on the front valence of a fabric awning or a hung sign panel. These panels should not exceed 12 inches in height.

Sidewalk Signs: Symbols, such as barber poles, were often set on the sidewalk to attract customers. If sidewalk signs are permitted, they can effectively advertise Downtown businesses. These signs should have the following characteristics:

- Material: Painted wood or metal. Merchants should avoid stock displays that also advertise the names of products such as beverages. A well-maintained, high-quality sign of this type is important.
- Lighting: Natural illumination. Do not internally illuminate.
- Location: At the edge of the sidewalk or building face. Maintenance of the sidewalk thoroughfare is important, and these signs should not obstruct pedestrians. If the right-of-way is not large enough, sidewalk signs should be avoided or used only on the building's parking lot.

—Number and Area of Signs

Principal Business Signs: Signs to identify the name and nature of the business should be the primary type of sign and limited to two per building storefront. This signage could be any of the sign types discussed previously.

Auxiliary Signs: A business should have a sign stating hours of business and an "open" sign. These should be limited to two square feet. Other indoor directional signage and brand information should not be placed as another outdoor sign.

Side Street Directories: The side walls of corner buildings can be used for wayfinding directions to parking and other attractions. These signs should be of uniform size and design. A suggested sign panel size would be 12 inches high by 48 inches long with 6 inch high lettering.

Sign Area: The aggregate area of all signs for a specific Downtown business should not exceed 100 square feet, except buildings with a front wall area of 1,000 square feet or more, where the aggregate sign area should not exceed approximately 10% of the front wall area.



Another Route 66 era building in Downtown Webb City. A business in a corner property such as this could make good use of Sidewalk signage.

Lettering Size: The height of lettering for any sign type should not exceed 12 inches, except for the capital letters (the first letter of each word), which should not exceed 18 inches.

Lettering Style: Because the historic signs spanned a long time period, a variety of lettering styles existed together. Lettering style for new signs could be either simple block letters or more elaborate lettering styles. Each business should express their individuality in their sign design.

3.1.14 Building Color

Color can enhance the details and patterns of a building's façade. The most effective and economical paint schemes often start with the natural colors of the building materials themselves, such as the red of many brick buildings. Avoid painting brick or masonry. Color techniques for buildings include:

- Use one base color for the majority of the background wall surface, but use a different color for accents. Avoid painting a building entirely one color.
- Base colors should be muted earth tones or pastels that will hold their color well. Owners should consider color stability when choosing paints.
- Window frames, sills, moldings, details, and cornices are potential architectural elements to accent with a different color. Signs, awnings, and entrance doors are also components that could be accented. Use bright colors only in small amounts and on the first-floor to direct the customer's eyes to the business.

3.1.15 Storefront Zone (see Page 11 for components)

By applying the design concepts found in this report to the Storefront Zone, the overall image of Downtown Webb City buildings will be improved. Generally, all architectural details, spandrel panels, and intricate brickwork should be restored and maintained, and in some cases, accented. Suggestions for the treatment of primary storefront elements, include:

- The main entrance should be recessed to emphasize the entryway. A recessed entry, when combined with display windows, creates a unique display area and should be creatively lit during evening hours.
- The main entrance door should provide a view into the building and a sense of openness. Solid doors and clutter such as flyers and posters on the entry door should be avoided.



An example of a restored Downtown Webb City façade with an attractive color scheme and mural along the side elevation.

- Lintels and columns help frame the building entrance and should be restored and maintained. Often these features are hidden behind an inappropriate covering.
- Large display windows should be preserved if present and reestablished if they have been removed. These windows are important to the overall balance and scale of the façade and provide important interior lighting. Display windows connect products with customers.
- The bulkhead serves as the base for the display window and should be in good condition to protect the window. Additionally, storage space can be provided, under the display area and behind the bulkhead.
- Framing of the windows should be of a material similar to the original framing, or of a material complementary to the historic profile. Clear, insulated glass with a 'Low-E' coating is an appropriate window. Tinted or reflective glazing, or interior films should be avoided.
- Transoms are typically a band of glass panels located above the entrance and display windows. This feature is usually at the same level on an entire block of buildings. Maintaining transoms will reinforce the horizontal lines on a street frontage.
- Transoms should be uncovered and restored. If the ceiling on the interior of the building has been lowered, pull the ceiling back away from the window to maintain the original ceiling height. Transom glass may be different from the display window, but it should complement all façade elements.
- Some transoms originally had hinged windows to provide building circulation. The building owner may wish to consider restoration of this feature. However, a hinged transom's ability to assist with air conditioning and/or heating expenses is limited by modern, efficient building systems.
- Window air conditioning units should never be installed in the front of a building, particularly not over a main entrance. These units should be restricted to the rear elevation.



An example of an intricate building entrance in Downtown Webb City.

3.1.16 Upper Façade

The Upper Façade of a building is often neglected, but the elements in this zone should adhere to the same concepts as previously suggested. The impression of a building and business is formed by the overall image of the property. Customers notice the condition of the upper façade, even though they may never set foot in the upper floors of the building. The Upper Façade is a building element that presents a significant amount of architectural style and detail to the street. Windows, brackets, cornices, and medallions can help showcase the building's character and are often found in this zone.

3.1.17 Rear (or Side) Elevation

The rear elevation typically faces an alley and provides access for deliveries and maintenance. In some cases, customer parking and a rear entrance to the building is provided. Attention to the appearance of the rear elevation can be extremely important to the quality of the customers' experience as well as the maintenance of the building. The building and business image can likely be improved here, while accommodating service functions. The view of rear elevations from parking areas is critical and should welcome the visitor using the lot. In addition to previously mentioned recommendations for upper-floor building elements and entrances, suggestions for other rear or side elevation components include:

- A customer entry through a side or rear door creates an access no longer just for service and should be just as inviting as the primary entrance. Customers might also develop a sense of loyalty or "special access" by using this building entry.
- An awning should be considered for a rear entrance. Also, building elements such as gutters, downspouts, service boxes, conduit, fencing, and screens should be in good repair.
- Rear fixtures such as signage, fencing, lighting, and awnings should use materials and colors that coordinate with the building front so customers will learn to associate any rear or side entrances with a particular business. Chain-link fencing and barbed-wire should be strictly avoided in Downtown.
- Rear exit stairs, elevators, and parking lots can enhance the marketability of upper-floor space. Trash containers should be placed in an enclosure or behind a screen that harmonizes with the surrounding buildings in scale and color. Wood, masonry, lattice, or hedges can all be effective screening methods. Landscaping can also screen ground-level utility units. A color scheme that complements the building should be used on all screening.
- Ancillary structures should match the surrounding buildings in style, scale, and color scheme. Ancillary structures should not be used as residential units.
- A restaurant can take advantage of a rear parking area for outdoor seating. While this type of seating is typically located in the front or side of a building, the rear elevation could also accommodate this vibrant element. Outdoor seating is also discussed as a streetscape element in Section 4.6 on Page 40.



This Downtown Webb City building provides an important façade, but also has a side elevation that should be improved.

3.2 New Construction Concepts

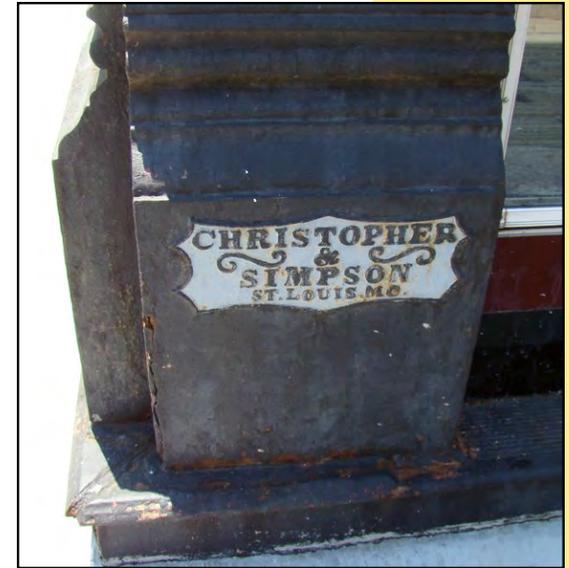
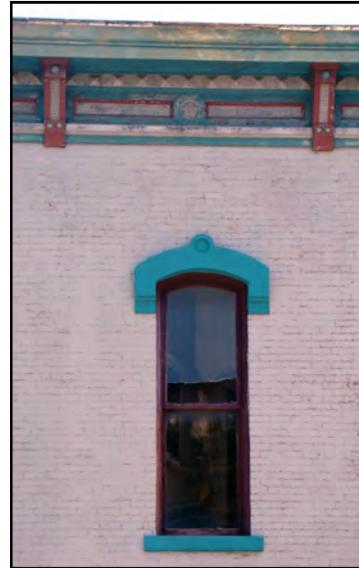
The construction of any new structure within Downtown is important because the new structure needs to be compatible with existing buildings. New construction should seek to harmonize with the visual characteristics of the neighborhood and address elements of scale, design quality, and massing; as well as the relationship of the new construction to existing nearby buildings. Avoid recreating historical styles or themes with new construction. This practice can create a “theme park” type of atmosphere. While new buildings can be inspired by past design, the new building should feel genuine and not a creation of a false past.

The following concepts consider additions to existing buildings as well as entirely new infill construction. The concepts are intended to identify a range of design options that will complement existing buildings, not to dictate specific styles or features. The design of new construction should generally focus on massing, rhythm and directional emphasis, materials, and building elements.

Massing and rhythm are defined by the relationship of a building to the open space along the street frontage, the relationship of solids to voids on building façades, and the relationship of the entrances and porch projections to the street. The directional emphasis (whether vertical or horizontal) of new construction should relate to that of the neighboring buildings. The defined rhythm of Downtown Webb City should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing. Materials and other building elements speak to the style of the building and should be complementary of surrounding structures.

An illustration depicting concepts for designing new Downtown commercial infill buildings is shown on Page 27. The general concepts provided in this illustration include:

- Designs should be considerate of the traditional building elements as described in Section 3.1 or on nearby existing buildings that contribute to the historic context of Downtown Webb City. Often a simple design is best, using three basic elements: a unified paint and color scheme, an awning, and non-intrusive signage.



Architectural details on existing buildings can provide clues for infill project developers interested in designing complementary buildings for Downtown Webb City.

- Properly orient the building to the street and carefully consider the relationship to nearby buildings.
- Emphasize horizontal features that can align with other buildings to reinforce the rhythm of the block. Vertical elements, entrances, lighting, and other street furnishings can also help develop the block rhythm.
- Include architectural details sparingly, with proper repetition.
- Restrict off-street parking areas to the side and rear of the building. Typically, a downtown building is built up to the sidewalk and it is important for an infill building to maintain this building line.
- Encourage multi-story construction to maintain the building roof line and to accommodate mixed-use development that reserves the ground floor for retail uses.



Infill development should support the historical architectural character of the surroundings.

Upper façade elements such as windows and sign panels should be continued.

Encourage traditional storefront awnings and upper and display window proportions. Use awnings instead of canopies.

Establish clear guidelines that prohibit materials such as metal and vinyl siding that are architecturally inappropriate. Discourage covering on transoms.

Recreate storefront elements and build to the sidewalk line to establish vibrant street rhythm.

An infill building example and illustration from Caruthersville, Missouri.



3.3 Franchise Architecture

To maintain the unique atmosphere in Downtown Webb City, branding buildings in the style of a company should not be allowed. Large franchises and national chains typically have a “downtown-style” in addition to their trademarked brand. These styles are more fitting to Downtown, as opposed to a highway corridor.

The company’s “downtown-style” is particularly important if a franchise store is to locate in an attached Downtown building of historical nature. This is an infrequent occurrence as Downtown buildings are typically not suitable or attractive to franchise stores. However, the City should be prepared if a franchise store prospect desires a Downtown location. These stores can still be complementary to the historical fabric of Downtown. The design concepts for new construction provided in Section 3.2 on Page 26 should apply, and other aspects such as parking requirements, pedestrian-oriented signage, building setbacks, and building lighting may require consideration. Store owners should be able to adapt their brand to create a complementary Downtown building.

3.4 Building Design Examples

The City and Downtown Webb City leaders selected specific buildings upon which to focus attention for illustrating building design concepts. The buildings chosen are along the west side of South Main Street; from 4 to 18 South Main Street and on the east side of North Main Street; from 201 to 207 North Main Street.

When the DREAM Land Use, Buildings, and Infrastructure survey was completed in 2009, most of these buildings were in poor or fair condition, with only the Bradbury Bishop Deli building having undergone recent rehabilitation. Recently, some of the subject buildings on South Main, nearby and including the Minerva Candy Company, have also been rehabilitated.

The following pages include photographs of the conditions of the subject buildings in 2009, illustrations of suggested improvements, and recent photographs showing improvements where buildings have been improved.

3.4.1 Buildings at 18—8 S. Main Street

Historical research of these buildings provide clues to original façade details. The Minerva Candy Company building is quite distinctive. The entire block has been rehabilitated. However, the Minerva building and the small building on the right are vacant as of the date of this report.



Conditions.—2009

Remove shingle canopy.



Proposed Improvements.

Restore upper-floor windows to maintain rhythm along the block.

Restrict signage to the sign band.

Install trapezoidal canvas awnings if needed.

Awnings can also include business signage.

Recent improvements made to buildings at 18—8 S. Main Street



Restored transom.

Restored storefronts.

Attractive, not flashy, color scheme.

3.4.2 Buildings at 6—4 S. Main Street

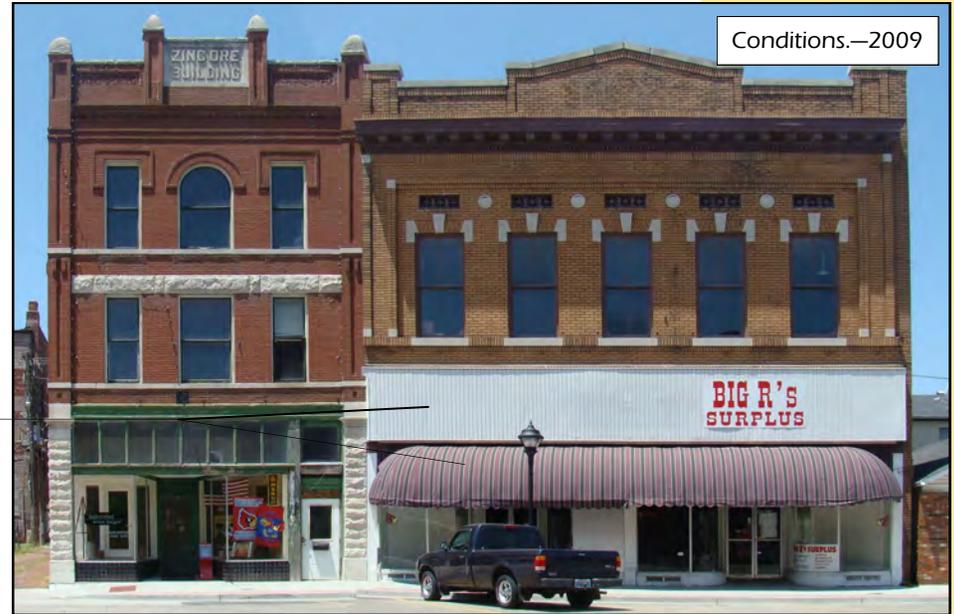
These buildings present a large amount of architectural detail for Main Street. There have been no recent improvements. The most significant impact could be made by the removal of the inappropriate awning and covering over the transom windows on the building on the right.

Remove inappropriately-shaped awning and expose the building architecture in the sign band.

Professionally clean and repair masonry.

Consider building lighting to add interest.

Maintain upper-floor access and storefront elements.

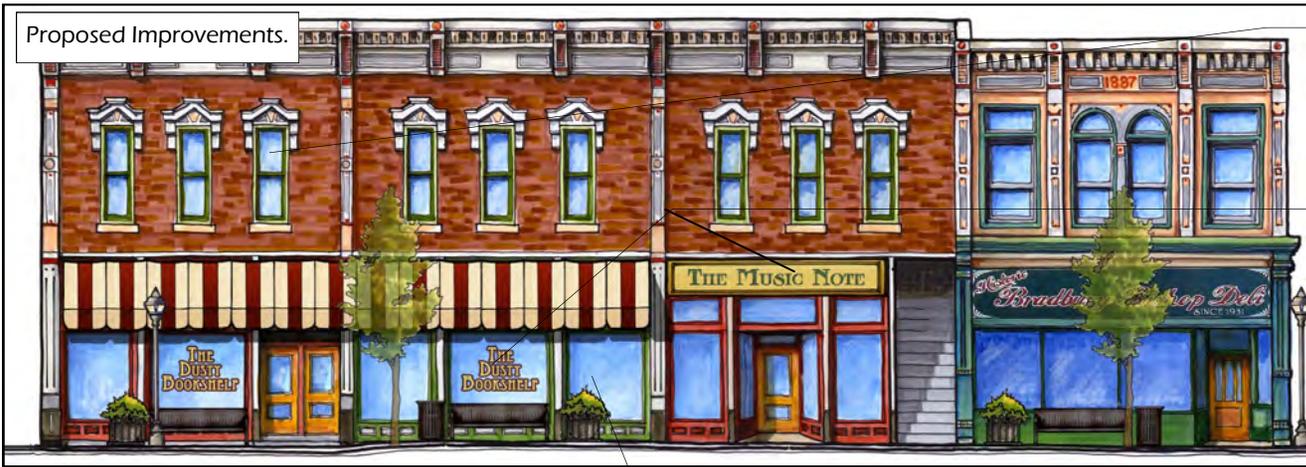


3.4.3 Buildings at 207—201 N. Main Street

These buildings present significant façade details and provide important building massing the length of the entire block on North Main Street. Unfortunately, the building on the left is in poor condition and requires extensive work to improve. As of the date of this report, the Bradbury Bishop Deli on the right is vacant.



Restore upper-floor windows to maintain rhythm along the block.



Install trapezoidal canvas awnings if needed.
Awnings can also include business signage.

Restrict signage to the sign band on display windows.

Restore display windows and other storefront elements..

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4.0 STREETScape DESIGN CONCEPTS

The term “Streetscape” typically refers to exterior public spaces located between the building façades on each side of the street. DREAM proposes an organized streetscape with coordinated lighting, site furnishings, landscaping, and wayfinding. In some cases, streetscape recommendations can include the development of parks, plazas, or other pedestrian gathering areas.

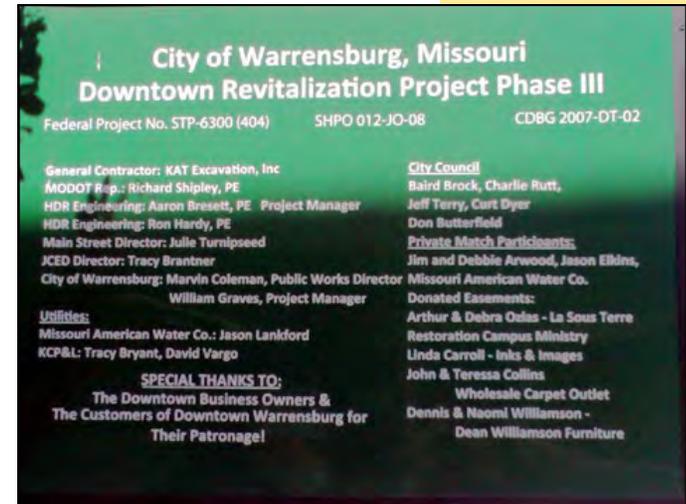
As noted on Page 3, Webb City has installed recent streetscape improvements along sections of Main Street. The Streetscape design is quite handsome and includes lighting, decorative street signs, street trees, and decorative pavement. The City should work to add benches, landscaping, and pedestrian plazas to this design and seek to expand these design elements south on Main to Highway 71. The intersection of Main and Highway 71 is an important intersection that serves as the entrance to Downtown on the north and the 144-acre King Jack Park on the south.

Although Downtown Webb City has an existing streetscape, the concepts in this section can be used to reinforce sound design ideas and provide guidance as existing elements are replaced. For example, the City is pursuing improvements to the King Jack Park entryway and the design concepts noted in this report should be considered to help tie the park and Main Street together.

4.1 Design Coordination

An overall design approach will complement the existing streetscape and will help pull together the buildings, streets, parking areas, public spaces, and pedestrian walkways into a pleasing experience that encourages the visitor to explore. A downtown should display a sense of order and rhythm through the repetition of building design features and street furnishings. In addition to the recommendations found in this report, there are other streetscape design issues that the City should address generally, including:

- Relocation of overhead power lines to underground conduit or alleyways.
- Sidewalk replacement and updating to current Americans with Disabilities Act Accessibility Guidelines (ADAAG) or the Proposed Right-Of-Way Accessibility Guidelines (PROWAG). Ideally, overhead power lines can be buried as sidewalks are reconstructed.



An example of signage to promote a streetscape project and funding sources from Warrensburg, Missouri.



The Downtown entryway sign at the intersection of South Main Street and Highway 71.

Without exception, all of the physical, public-owned elements of Downtown Webb City must be maintained. Streetscape fixtures should be reviewed on a regular basis and repairs or replacements made as timely as resources will allow. Maintenance costs required by a physically improved Downtown are unavoidable, but Webb City cannot afford to broadcast a message of neglect and decline. The City must commit to enhancing Downtown and strive to keep it that way. Public streetscape enhancements demonstrate that the City is an investment partner in the ongoing improvement of Downtown.

4.2 Infrastructure

Downtown cannot function without intact infrastructure, but this does not mean infrastructure should just be functional. The City should view infrastructure as a design element that can be enhanced aesthetically for the benefit of residents, visitors, merchants, and property owners. Recommendations for effective and attractive infrastructure include:

- Curbs should be in good repair and constructed of a consistent material. There should be no gaps or areas of uneven elevation along the curb line. At street intersections there should be ADAAG or PROWAG compliant ramps as noted in Section 4.3 to follow. Generally, existing curbs in Downtown Webb City are in good condition. However, there are areas along side streets which are deteriorating.
- Poorly working storm drains can cause storm water run-off to pool at street intersections. This condition makes pedestrian access virtually impossible and must be corrected.
- Street improvements such as pavement, curbs, or sidewalks should coincide with other public works projects to minimize street closings and costs.
- New sidewalks should transition smoothly into the same grade as street surfaces. New or replacement curb and gutter should be vertical curb design.
- Relocating overhead utility lines, although costly, can improve Downtown aesthetics greatly. Above ground utility enclosures should not obstruct the pedestrian walkway.



An example of a new curb.

4.3 Accessibility

The U.S. Access Board is an independent Federal agency that has been established to monitor and issue updated accessibility guidelines for new or altered facilities covered by Americans with Disabilities Act (ADA) and the Architectural Barriers Act (ABA). These major civil rights laws prohibit discrimination on the basis of disability and

establish design criteria for the construction or alteration of both public and private sector facilities for public and commercial use. These guidelines address new construction and alterations and are referred to as the ADA Accessibility Guidelines (ADAAG). A recent addition to the Proposed Right-Of-Way Accessibility Guidelines (PROWAG), is meeting acceptance by various federal agencies and will soon expand upon the ADAAG for public improvements.

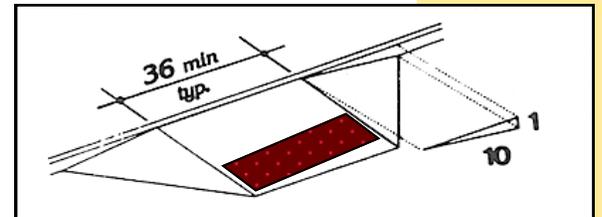
Without the required curb ramps, sidewalk travel is dangerous, difficult, and in some cases, impossible for people who use wheelchairs, scooters, and other mobility aids. Ramps allow people with mobility impairments to gain access to sidewalks and pass through center islands in streets. Additionally, vision impaired visitors to urban downtowns require detectable warning strips along ramps leading to streets. When streets and roads are newly built or altered, they must have ramps wherever there are curbs or other barriers to entry from a pedestrian walkway. When new sidewalks or walkways are built or altered, they must contain curb ramps or sloped areas wherever they intersect with streets or roads. Resurfacing a street or sidewalk is considered an alteration for these purposes. However, regular maintenance such as filling-in potholes will not trigger the requirements.

ADAAG and PROWAG provide for flexibility in many cases, such as Program Access; where an acceptable alternative route to a building may make use of existing ramps, provided people with disabilities must travel only a marginally longer route. Other accessibility considerations that will benefit Downtown pedestrians include:

- A clean, clear, and well-lit pathway should be provided from public parking areas to major Downtown activity centers.
- The City should occasionally review lot and stall conditions and conduct necessary repairs. The City should also consider enforcing private parking lot surface standards.
- To help reduce traffic and assist visitors with wayfinding, the City should encourage parking lot border treatments to help delineate private parking lots.
- Sidewalks should run continuously through an entire block to create a clearly defined pedestrian pathway and minimize conflicts between people and vehicles. All roadway crosswalks should be clearly marked with signage and striping.



An example of an ADAAG compliant ramp in Sedalia, Missouri.



An illustration of ADAAG ramp construction dimensions and location of warning strip.

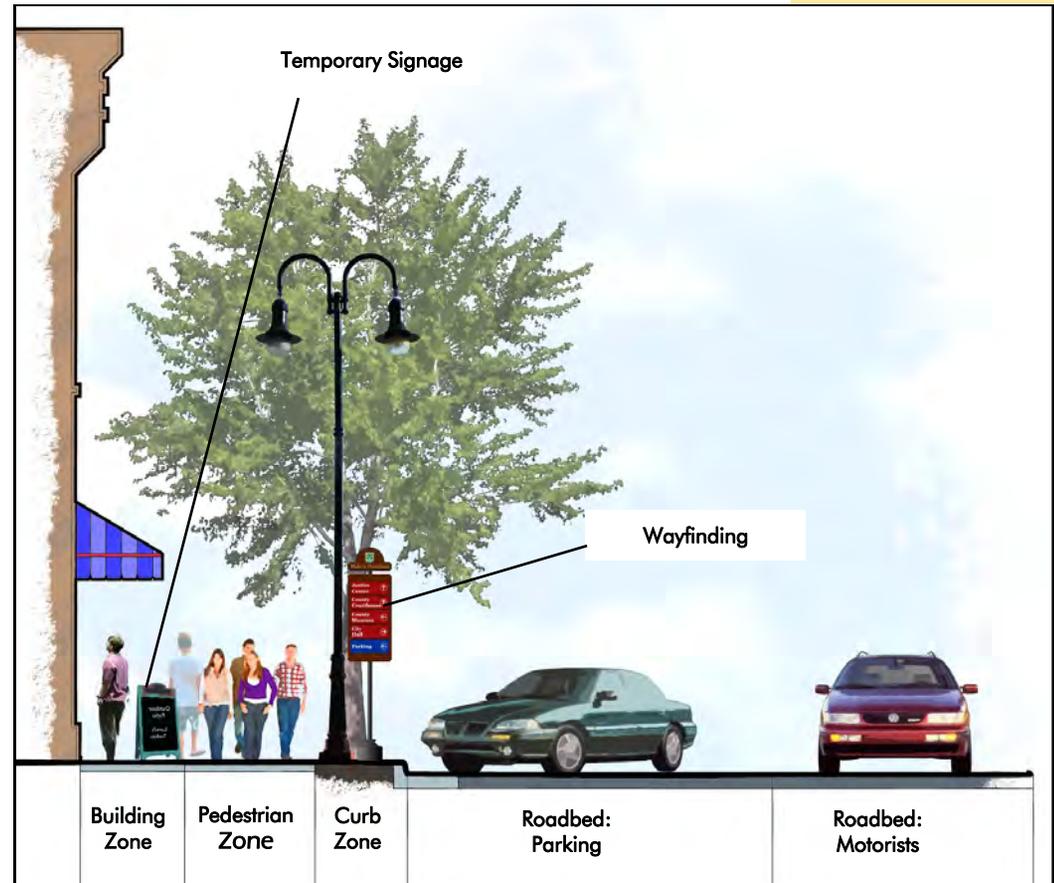
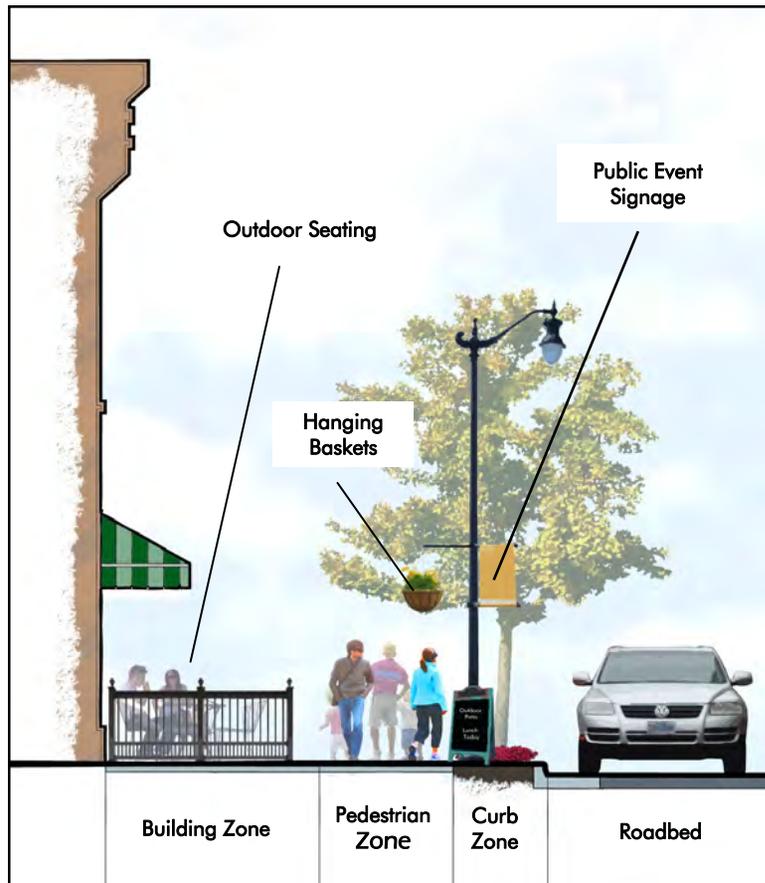
4.4 Sidewalk Zones

As a streetscape project is contemplated, it is important that adequate zones in front of a building are maintained. The Building Zone, Pedestrian Zone, and Curb Zone all have unique characteristics that should be regulated to ensure that private elements do not adversely impact public improvements.



Aside from ADAAG or PROWAG pavement improvements, streetscape amenities should remain clear of the Pedestrian Zone and allow for free movement of pedestrians. Amenities should enhance the pedestrian experience, not be obstructive. Businesses and property owners should be educated on the importance of maintaining Sidewalk Zones. Each business should care for the zones within their building's street frontage.

Street furnishings such as bicycle racks, bollards, and benches can add value and functionality to a streetscape, but must be installed in useful locations. Care must also be taken that furnishings are not excessive and do not block on-street parking lanes.



4.5 Parking and Service Areas

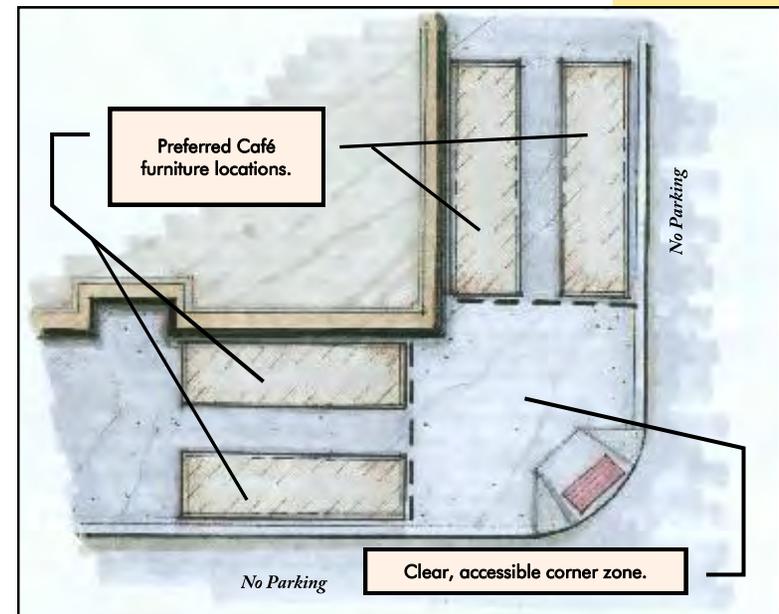
Parking lots and service areas are required to support Downtown Webb City businesses. Street parking will accommodate some, but not all, of this required parking. Publicly owned parking lots can be improved with streetscape elements to provide pedestrian gathering places. Recommendations for Downtown parking and service areas include:

- Parking and service areas should be well-lighted and landscaped. Vacant lots may provide a temporary solution for additional parking, but a lot should be paved if it will be used as parking for more than six months. Planting buffers or decorative paving should be installed at the edges of lots to define the site border. Landscape islands should be included throughout the lot to improve aesthetics as well minimize storm water run-off.
- Parking lots and service areas should be located to the side or rear of the main business area. Parking should be clearly marked as public, or private, parking. In the case of Webb City, some of the existing parking lots require maintenance and improvements. Additionally, there are several vacant lots that could be converted to parking as needed.
- Parking should provide a clear and well-lighted pathway for pedestrians to reach activity centers. Lots should also be maintained in good condition, along with all parts of the street, alley, and sidewalk. Parking should be connected to clearly-marked crosswalks. Visitors should feel safe in the parking lot and not have to navigate tripping hazards to reach Downtown destinations.

4.6 Outdoor Café Seating

Outdoor cafés or sidewalk seating is a common element of a vibrant downtown. Such seating areas can be accommodated in Downtown Webb City, but require special attention. A proper arrangement will have the following characteristics:

- Be located in the sidewalk area fronting the restaurant. If located on the side or rear of the building, the seating should be far enough from the parking or street to avoid an unappetizing experience with car exhaust, inattentive motorists, and road grit.



- Allow a clear and unencumbered path along the sidewalk for pedestrian traffic. The sidewalk must maintain accessibility compliance and the restaurant owner should be held responsible for the pathway.
- Not obstruct entrances to the building and provide a clearly defined area connected with the restaurant. Areas adjacent to the building should be ideal.
- Use umbrellas or other patron shelter in a color and style that complements the building.
- Consist of furnishings that are durable, weatherproof, windproof, sturdy, and properly maintained. Furnishings should be stored off-site during the winter months.
- Should include sturdy trash receptacles. The restaurant owner should be held responsible for maintaining the cleanliness of the outdoor seating area.

4.7 Street Lighting

Street lighting should enhance the pedestrian experience and nighttime image of Downtown, while also providing an attractive installation during the day. Generally, street lighting should:

- Provide pools of light on the sidewalks at a higher level of illumination than the roadway. Storefront lighting can add to this illumination.
- Be on 12'-14' high poles and project light down onto the sidewalk, not into second floor windows.
- Be uniform in style, type, height, color, type of illumination (LED, compact-fluorescent, high-pressure sodium, etc.) and brightness throughout Downtown.
- Be equipped with brackets for banners and electrical outlets that can display banners and decorations.
- Be part of an overall lighting design strategy to ensure desired lighting levels. Street lighting should also illuminate parking areas, rear entrances, and alleys, as well as streets.

4.8 Signs and Banners

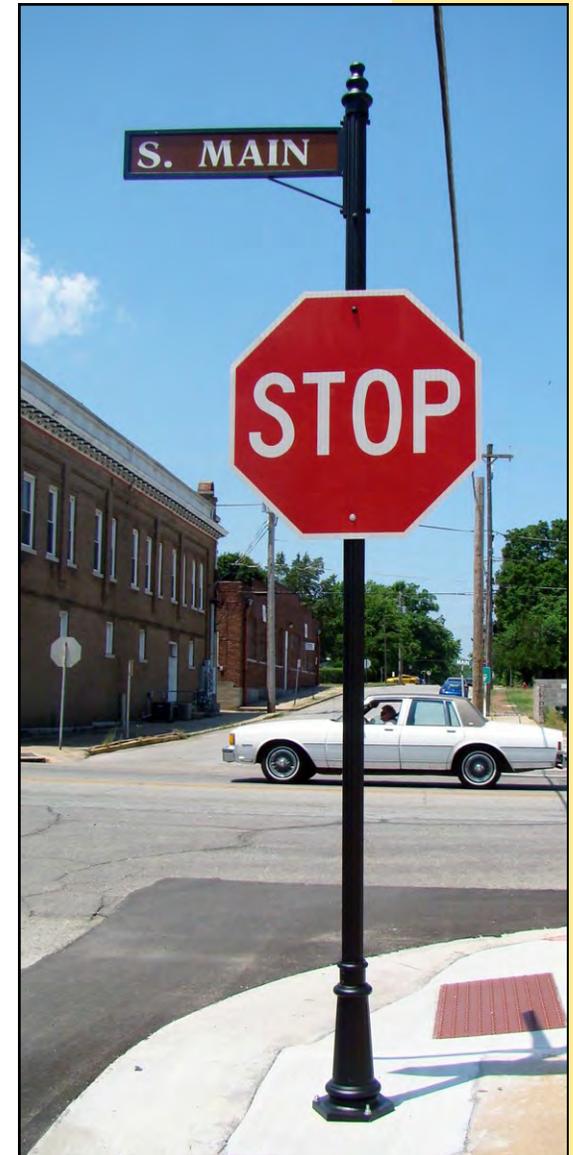
Public signage should be used in the streetscape design to identify, define, and promote Downtown Webb City. This type of signage is often only thought of as a functional streetscape element, with no concern for aesthetics. With every Downtown business having at least one sign, public signs can quickly get lost. These signs are critical for wayfinding, traffic flow, and ensuring the safety of Downtown pedestrians. Wayfinding techniques and components to assist in navigation through the area will be addressed in detail in Section 4.14 on Page 47.



The existing lighting style in Downtown Webb City.

Coordinated signage for Downtown can also help define the boundaries of Downtown. Concepts to improve the public signage in Downtown Webb City, include:

- The existing design of the street name signs in Downtown Webb City should be continued to help reinforce a feeling of place for Downtown. This is an important decorative feature the City should maintain as the streetscape is expanded off of Main Street. To reduce visual clutter, regulation and directional signage should be combined where possible.
- Encourage Downtown property owners to install street address signs on their buildings that complement or match the style of the street name signs. A historic plaque-type can provide a very elegant touch. The style, font, and colors of these signs should be easy-to-read for local emergency personnel.
- Temporary banners and other signs for public events and attractions can be allowed, but should be restricted as to size, number in one location, and length of display.
- Seasonal banners or decorations that are approved by the City can be installed to create a festive and vibrant atmosphere. Banners can also add a sense of civic identity, but must be well-designed and are most effective with a simple, repetitive, design. Lettering should be kept to a minimum and sponsor panels should only be allowed within a uniform design panel, if at all. The City should provide all maintenance of public signage, banners, outlets, and brackets. The City should also change the banners on a regular schedule, replacing hardware or faded banners as needed.
- Balloons, pennants, and other distracting sign novelties should be strictly regulated in a downtown environment. These elements can be used on public signage, but this should be uncommon.
- Murals should always have an artistic component and should only be allowed by City approval. A mural is a professionally painted piece of art and should not be used as a business sign. Any mural not approved should be considered in violation of the sign code.
- As noted, Downtown Webb City has an opportunity to restore a number of wall signs painted on historic buildings. These signs should also be professionally painted and allowed by City approval. They are essentially treated as murals.



Example of existing decorative traffic signs found in Downtown Webb City.

4.9 Public Furnishings

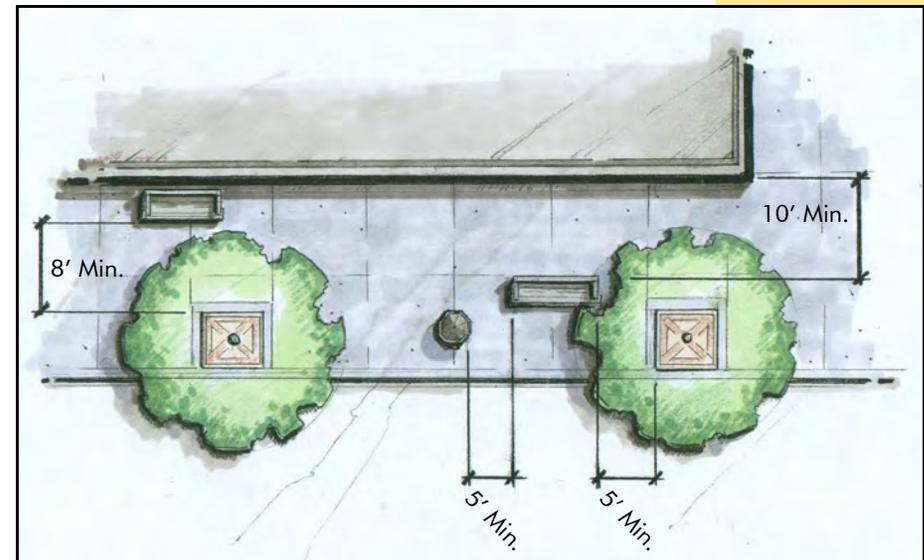
The furnishings included along a street or in a pedestrian plaza, parking lot, or park should be considered as part of the overall streetscape in terms of design. Elements should complement and introduce users to the theme of Downtown Webb City. Clusters of furnishings will provide gathering places for pedestrians and will encourage visitors to park their vehicles and explore.

The City of Webb City has some installed furnishings in Downtown. General guidelines for the future installation of public furnishings, include:

- Benches within the streetscape encourage social interaction which contributes to a successful Downtown. However, some benches should be oriented so that a pedestrian can sit facing into the storefront. Suggested minimum distances for bench placement are shown below right. Actual distances may vary due to site conditions.
- Planters and window boxes provide color and an opportunity to include volunteers from local garden clubs.
- Trash receptacles, bollards, tree grates, and boundary fencing should all be an attractive Downtown accent, not just utilitarian components.
- Sturdy materials that can be painted are preferred for public furnishings. Wood and soft materials can be vandalized and should be avoided. Concrete is a sturdy material, but is not easily moved, repaired, or replaced. Painted metal is a better choice.
- When grouped together, streetscape furnishings help to enhance Downtown's appearance and create a pedestrian-welcoming environment with functional gathering spaces.
- Furnishings should complement light and sign posts to present a unified streetscape design. The styles of furnishings should be simple and not too intricate or flashy. Furnishings should be economical to maintain and of a style that can be easily repaired or replaced.



An example of a streetscape bench that is sturdy and easily maintained.



An example of Downtown bench placement.

- The City may wish to restrict private property owners from installing their own furnishings on the right-of-way in front of their building; instead, providing a catalog of approved choices that are complementary to the rest of the streetscape design components.
- Many site furnishing manufacturers offer customization options for their products, usually via plaques or laser-cut designs (logos, icons, text, etc.) This provides a great way to personalize streetscape elements and creates opportunities for community/organizational involvement, dedications, memorials and sponsorship fundraising.

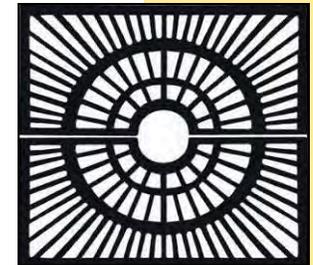
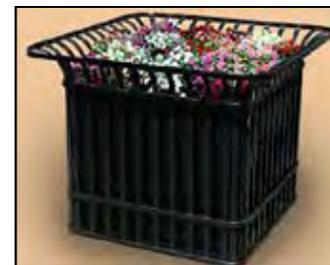


Downtown Webb City may wish to consider a customizable trash receptacles.

4.10 Bicycles

Downtown Webb City should be friendly toward bicyclists as well as pedestrians. Issues for a cyclist include routes of travel, clearance, access to water, type of traffic signals, traffic lanes, signage, drainage grates and curbing obstacles, and parking. Downtown plans should implement bicycle facilities which can be used by local citizens and visitors. The City should identify opportunities for future bicycle facilities in Downtown and throughout the community. Considerations to improve the bicycle friendliness of Downtown Webb City, include:

- Bicycle racks which should be of uniform design of materials, color, and style as other streetscape elements and located at useful activity nodes throughout Downtown.
- Directional and regulatory signage which identifies bike routes and share-the-road routes. Dedicated bicycle lanes on streets, where feasible.
- Wayfinding signage to various destinations within Downtown.
- Public restrooms and drinking fountains.



Examples of sturdy furnishings suitable for installation in Downtown Webb City.

4.11 Fountains and Public Art

Fountains and public art can enhance Downtown and the pedestrian experience greatly. These features will be most effective as simple interactive elements which Downtown visitors can enjoy. Fountains could be simple bubblers that provide a refreshing respite in the summer months, and are lower maintenance than fountains in a pool. General guidelines for future Downtown Webb City streetscape projects, include:

- Fountain water should be left in its natural state without coloring.
- Commissioned professional public art and sculpture provides an inspirational atmosphere in which people enjoy lingering. Professional temporary or seasonal exhibits could also be installed.
- Fountains and art can also serve as memorials, commemorating City founders or other notable citizens or stories of historical events.
- As with other streetscape elements, fountains and art should not encroach upon the pedestrian walkway.



An example of art district signage installed on a light pole.

4.12 Landscaping

Existing landscaping in Downtown Webb City is limited to street trees that the City has planted along Main Street. These trees appear to be Red Maples or Japanese Cherry trees that will provide sufficient shade and aesthetics. However, the City may find these trees require more maintenance and are not as hardy as other species. Additionally, planting beds or rain gardens could be established for added color. There are parking areas that could be defined with landscaping and there are some opportunities where excess pavement could be removed for landscaping beds. Planting palettes for street trees, shrubs, and rain gardens are included in the Appendix as Exhibit B on Page 61. Other general landscaping concepts for Downtown Webb City, include:

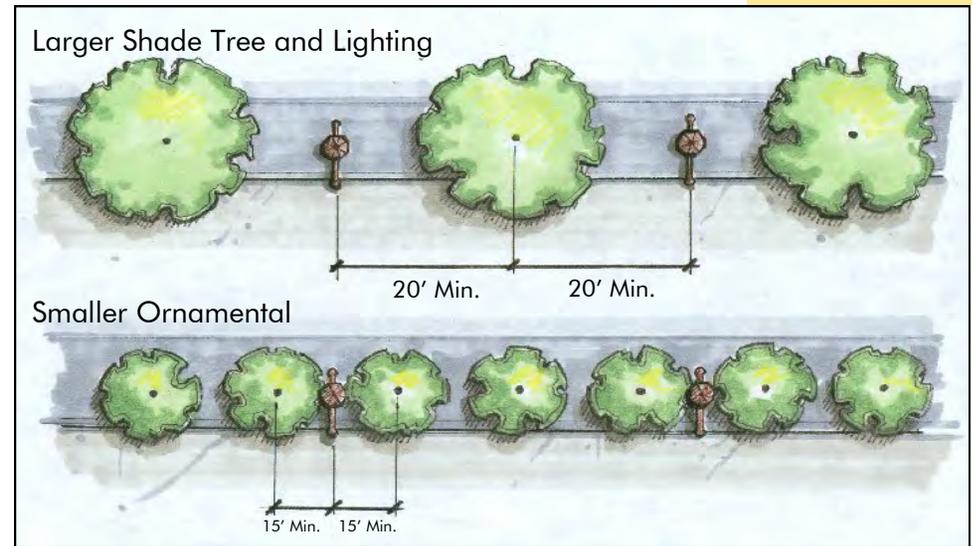
- If landscaping in front of a business is desired, plants in movable containers should be used where no landscape strip is present. Containers should be placed immediately adjacent to buildings or curbs.
- Street trees work best when planted in groups or islands where they can thrive on larger volumes of soil.



An existing street tree installation in Downtown Webb City.

The trees should be of a hardy variety, common to the region, and at a size which will allow a minimum of seven feet of clearance before any lateral branching begins.

- The canopy of the tree should be considered to avoid excessive roosting of birds.
- Trees that produce fruiting berries should be avoided to reduce sidewalk maintenance.
- Trees should also be chosen with downward growing roots, not lateral roots that will damage surrounding pavement.
- Shrubs should be massed in groupings of five to seven plants with no more than two different species within a planting bed.
- Suggested minimum placement distances are shown at right. Actual distances may vary due to site conditions.



4.13 Rain Gardens

Rain Gardens are low-lying landscape beds designed to collect rainwater from adjacent impervious areas. A properly designed and installed Rain Garden will ease the load of the existing storm sewer system and reduce erosion and pollution. Rain Gardens also help to facilitate filtration and absorption of rainwater back into the ground. Rain Gardens require proper design and plant selection for maximum efficiency and reduced maintenance costs. Careful consideration to these factors will reward Downtown Webb City with significant environmental impacts for a low cost. Even small Rain Gardens can have a large impact on storm water run-off. General recommendations for Rain Gardens include:

- Low maintenance native plants are recommended due to their greater tolerance for climatic and soil conditions, as well as extreme moisture.



An example of a Downtown Rain Garden from Springfield, Missouri.

- Rain Gardens should provide aesthetic benefits to the streetscape.
- Rain Gardens should be located so that they do not create an obstacle for street cleaning and maintenance.
- As with any landscape bed, regular weeding and clearing of litter is required.
- Rain Gardens can replace existing landscape beds so as to not require a reduction of parking spaces.
- Species that have trouble thriving should be immediately removed and replaced.
- The City should consider the use of porous pavement in nearby parking areas to help reduce the amount of overall standing water impacting Downtown.
- Rain Gardens should be viewed as a treatment for storm water before it enters the water system. Access to conventional drainage structures should be maintained to prevent flooding during heavy storm periods.
- Curb inlets should be provided to allow water to enter and exit the Rain Garden if necessary. The curb inlets should be tapered to minimize damage from maintenance vehicles.

4.14 Wayfinding

Wayfinding is an indispensable tool for directing travelers to destinations and creating a positive first impression. The term wayfinding was originally coined by Kevin Lynch in his 1960 book *The Image of the City*. Lynch presented the concept that people use a cognitive map to move through their environment to their destination. Wayfinding is a system to assist travelers in interpreting their cognitive maps. The goal of a wayfinding system is to make the journey to a destination as transparent and seamless as possible. By taking a comprehensive approach in developing the wayfinding system, a community can reinforce its unique identity and sense of place. Any approach to wayfinding in Downtown Webb City should include design and building codes centered on four primary aspects:

Architecture:

- Visual clues of buildings and other features of a street aid people in knowing their location and the direction of their destination without the use of signage. Such clues also draw the pedestrian's eyes to where an entrance or display window is expected to be located.
- Buildings presenting strong architecture serve as landmarks and orientation points. These points are often destinations and starting points.



An example of wayfinding from Washington, Missouri.

Sight Lines:

- Visitors will feel most comfortable if they can maintain visual contact with their destination and will want to make few directional changes. Clear sight lines down streets at key intersections should be maintained. Avoid allowing buildings to encroach on, or block, these lines.
- Repetitive landscaping and furnishings can enhance the view down these streets, but care must be taken that streetscape components do not obstruct important navigational landmarks.

Lighting:

- Lighting can be used to encourage routes and pathways. Warmly-lit sidewalks and streets draw the customer onward, while similarly lit storefronts and entrances will be appealing to the customer.
- A repetitive line of lighting can be an effective navigation tool. Poor lighting causes missed information and leaves an unsafe impression in a visitors mind.

Signage:

- Uniform signage at important decision points is a critical element of Downtown wayfinding. Kiosks can direct visitors to various attractions, advertise events, and consolidate signage. Excessive signage will lessen the effectiveness of any individual sign. Fewer, easy to read signs placed at strategic locations are preferred.

Wayfinding systems create an arrival sequence to Downtown. The system consists of themed signs of various types that direct travelers to attractions. All too frequently existing wayfinding systems are inadequate. Typical problems include:

- Lack of accuracy, with arbitrary sign location.
- Visual clutter from too many signs.
- Lack of focus in directing traffic to Downtown.
- Diffuse allocation of signs, across many entrances.
- Lack of charm or just standard Department of Transportation signage.
- Routes actually direct travelers around Downtown.
- Signs are too small with inconsistent sizes, colors, and types or are too wordy and hard to read.

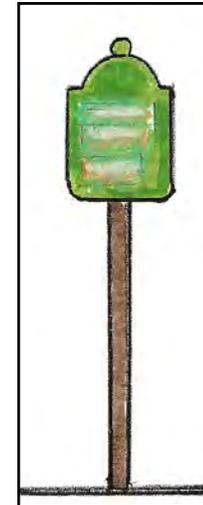


Illustration of a Proximity Sign.

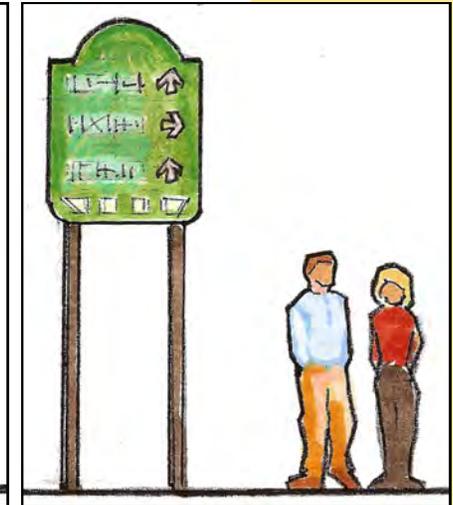


Illustration of a Trailblazer Sign.

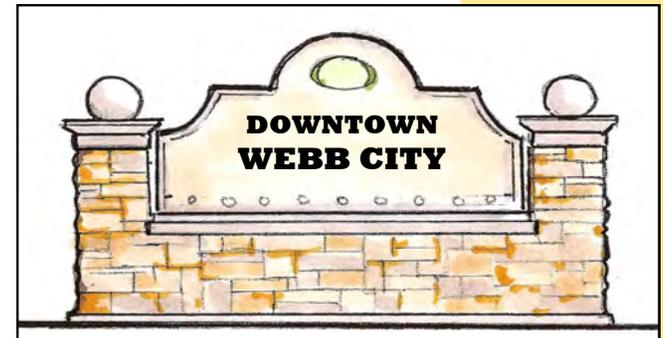
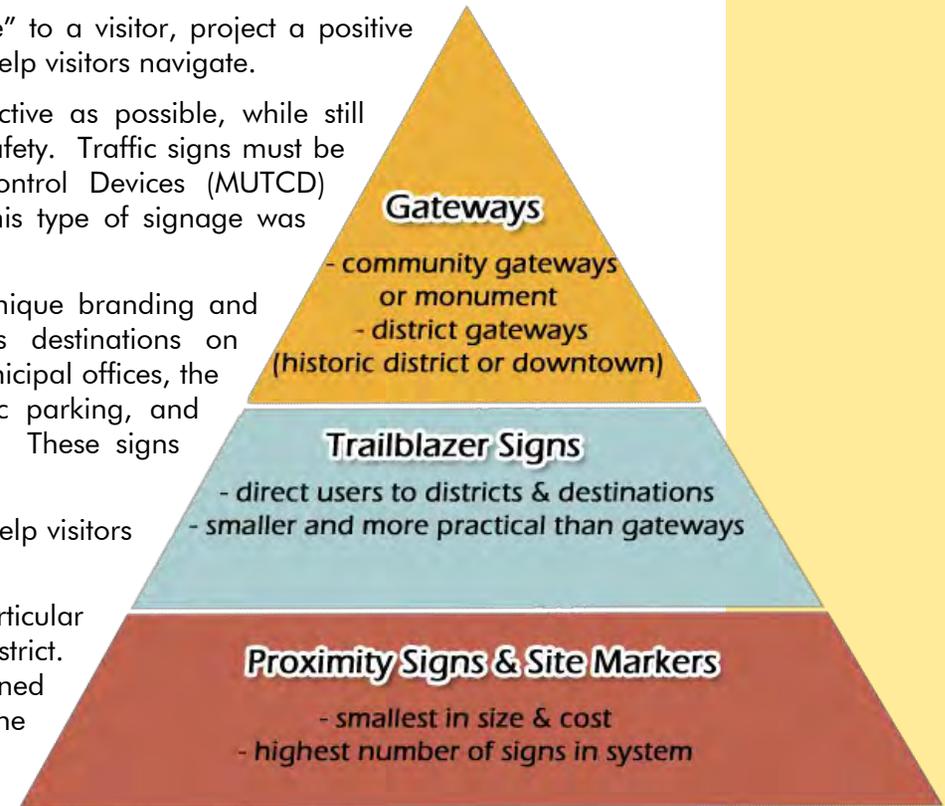


Illustration of a Primary Gateway Sign.

Components of successful wayfinding systems seamlessly integrate the visitors experience with the messages needed to navigate to, and around, Downtown. These components include:

- Primary Gateway Sign—Should serve as the “Welcome” to a visitor, project a positive image of Downtown, and be a significant landmark to help visitors navigate.
- Traffic & Directional Signs—As unobtrusive and attractive as possible, while still meeting Department of Transportation guidelines for safety. Traffic signs must be developed using the Manual of Uniform Traffic Control Devices (MUTCD) published by the Federal Highway Administration. This type of signage was discussed in Section 4.8 on Page 41.
- Trailblazer Sign—Utilitarian purpose combined with unique branding and design elements. Area attractions to consider as destinations on Trailblazer Signs should include City Hall and other municipal offices, the Chamber of Commerce, the Farmer’s Market, public parking, and nearby recreational facilities such as King Jack Park. These signs should be located near key transportation nodes.
- Proximity Signs—Installed near attractions, these signs help visitors with their final steps to the destination.
- District Gateway Sign—Creates a boundary for a particular district within the Downtown, such as a historic or arts district. These signs can be used within the district to be defined and should reflect the size, scale and character of the existing architecture of the district.



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5.0 NEXT STEPS

The City and Chamber should continue collaborating on Downtown projects. The City should consider launching an effort to develop building design standards for Downtown, including rehabilitation to existing buildings. These standards could be voluntary, but should help preserve Downtown buildings with significant architecture, while allowing innovative techniques to rehabilitate non-historic properties. The City should base these standards upon the Secretary of the Interiors Standards for Rehabilitation; a summary of which is found in the Appendix as Exhibit A on page 59.

The City has made numerous public investments in Downtown and should continue with the effort to implement future phases to expand the Main Street streetscape as funding becomes available. The Chamber of Commerce should also collaborate on Downtown projects and seek ways to increase its capacity to provide events, promotions, and marketing.

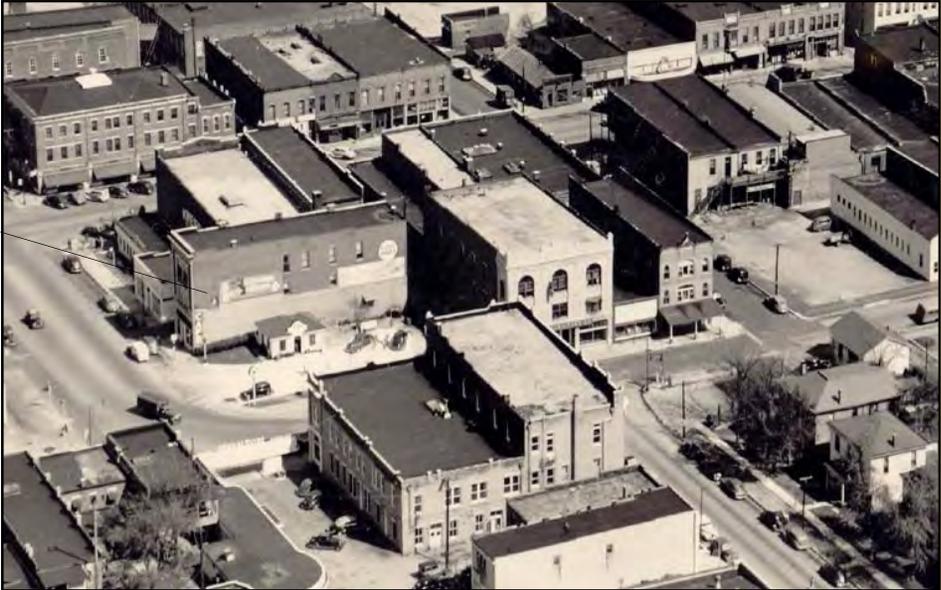
Future projects Downtown Webb City should consider, include:

- Encourage façade rehabilitations
 - ◇ Building standards
 - ◇ Incentive mechanisms
 - ◇ Technical help from historic-oriented contractors
- Signage Enhancements
 - ◇ Restore wall/ghost signs to promote Route 66 connection (Historic evidence provided by Webb City Area Genealogical Society Archives is shown on pages 52 to 54)
 - ◇ Directional signage
 - ◇ Gateway signs
- Improvements to green space and pedestrian gathering plazas
 - ◇ Connection with King Jack Park (preliminary entryway concepts are shown on page 55. However, the City chose not to include the design work for King Jack Park in the DREAM process)
- Revitalize and maintain existing streetscape elements
 - ◇ Tree, shrub, and rain garden landscaping palettes that provide hardy and appropriate plant species for use in Downtown Webb City are included in the Appendix as Exhibit B on Page 61.
 - ◇ Expansion plans for existing streetscape should include Daugherty, Broadway, and Church Streets

Webb City Wall/Ghost Sign Evidence

Source—Webb City Area Genealogical Society

Wall signs behind what is now the Chamber of Commerce building.



Along South Main Street, just north of Church Street, the furniture building has been lost. However, this photo demonstrates it was a popular location for wall signs.



Webb City Wall/Ghost Sign Evidence

Source—Webb City Area Genealogical Society

The side of the Dermott Building on S. Main just north of 1st Street was the location of a wall sign.



At N. Main and W. Daugherty Streets a wall sign was located on the side of what is now the Bruner Pharmacy building. Today this is the location of an attractive mural.



Webb City Wall/Ghost Sign Evidence

Source—Webb City Area Genealogical Society

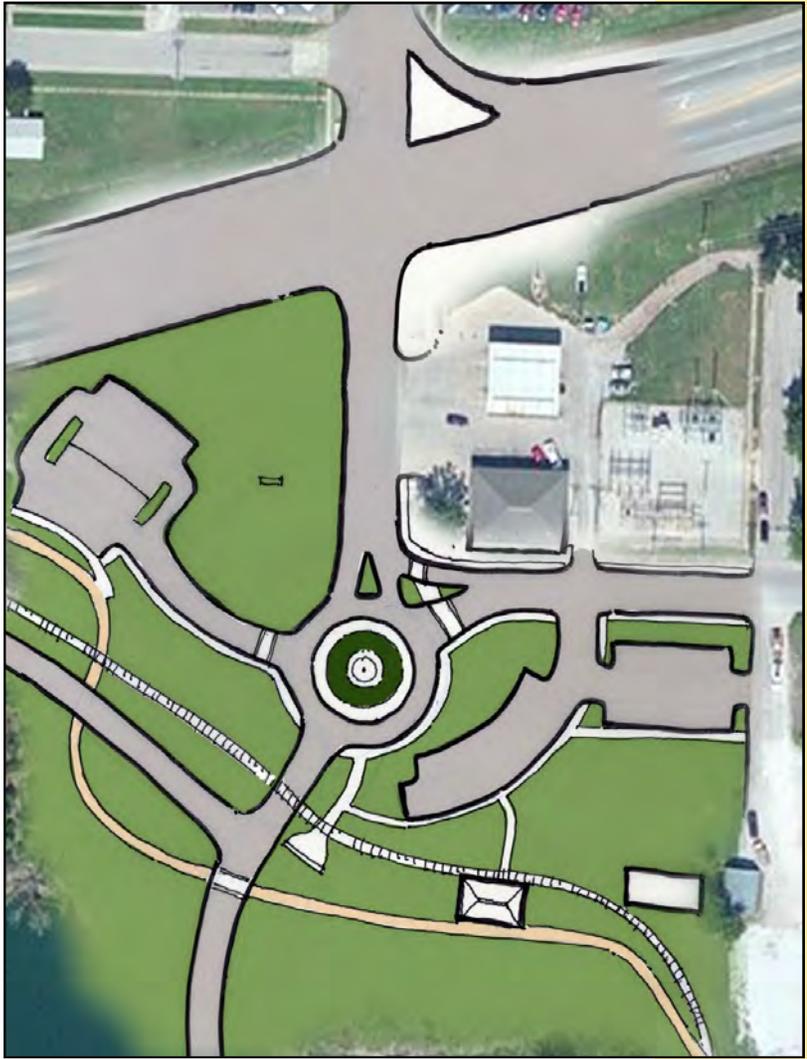


The two wall signs visible in this photo were painted on buildings on S. Main Street that no longer exist.

King Jack Park Entryway Preliminary Concepts



Diverted road preliminary concept for King Jack Park entryway.



Roundabout preliminary concept for King Jack Park entryway.

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APPENDIX

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EXHIBIT A:

SECRETARY OF THE INTERIOR STANDARDS FOR REHABILITATION (36 CFR Part 67)

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

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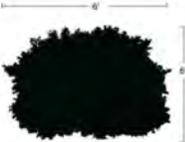
EXHIBIT B:

LANDSCAPING PALETTES FOR MIDWEST DOWNTOWN PLANT SPECIES

STREET TREE PLANTING PALETTE

Species Information	Form / Ave. Size	Selected Images		
<p><i>Nyssa sylvatica</i> Black Gum</p> <p>The Black Gum, a Missouri native, is a slow growing deciduous tree adaptive to many diverse conditions. The species tolerates poorly drained soils and some periods of drought. A long taproot is characteristic of the species minimizing damage to nearby pavement. The form of the Black Gum is characterized as a broad pyramidal to rounded crown. The plant holds dark green glabrous leaves, which transform into a stunning orange-scarlet color in the fall.</p>				
<p><i>Ginkgo biloba</i> 'Autumn Gold' Autumn Gold Maidenhair Tree</p> <p>The 'Autumn Gold' Ginkgo tree is a deciduous conifer that is recognized by its distinctive fan-shaped leaves. The 'Autumn Gold' is an all male cultivar that grows to approximately 50' tall and holds a broad pyramidal form. Male cultivars, such as the 'Autumn Gold', will not produce the messy and odorous fruit characteristic of the female Ginkgo. The Ginkgo is tolerant of disease, saline soil conditions, heat, compaction, and a wide range of soil conditions. This tolerance to adverse conditions makes it an excellent candidate for urban environments. The Ginkgo is especially attractive in the fall when the leaves turn a uniform golden-yellow.</p>				
<p><i>Ostrya virginiana</i> Eastern Hop Hornbeam</p> <p>The Hop Hornbeam is a very adaptable species that can tolerate a wide variety of soil and moisture conditions. The species produces a cluster of hop-like fruit changing from green to tan. The catkins of the male species are more prominent and will persist into winter. The size and durability of the species make it a very useful street tree candidate.</p>				
<p><i>Cladrastis kentukea</i> Yellowwood</p> <p>The Yellowwood, a Missouri native, typically grows into a rounded spreading crown. This species is tolerant of a wide range of soil conditions. This species has a very impressive flower set. The flowering begins in May and produce white panicle flowers that reach a length of 10-15". The flowers carry an intense fragrance. The species also carries a smooth-barked trunk and a yellow fall color.</p>				

SHRUB PLANTING PALETTE

Species Information	Form / Ave. Size	Selected Images	
<p><i>Ilex verticillata</i> Winterberry</p> <p>The Winterberry, a Missouri native, is a slow growing deciduous shrub adaptive to many diverse conditions. The species tolerates poorly drained and wet soils. This species is a deciduous holly with an upright-rounded form. Female cultivars will produce showy reddish-orange berries in fall after flowering and pollination from male plants. The Winterberry is best used in a shrub border, foundation planting, accent planting, or as a hedge.</p>			
<p><i>Callicarpa americana</i> Beautyberry</p> <p>The Beautyberry is a native deciduous shrub that prefers medium soil and sun to part shade. Once established this species requires little care. This species produces an insignificant bloom in late summer that develop into small clusters of fruit. The profuse attractive fruit, a small berry-like drupe, is violet to magenta in color and persist to winter. The loose form of the Beautyberry lends itself to shrub massing and informal borders.</p>			
<p><i>Itea virginica</i> Virginia Sweetspire</p> <p>The Sweetspire is a deciduous shrub, native to Missouri, that is tolerant of many soil and light conditions. The species prefers moist to wet soil conditions in full sun or part shade. The Sweetspire produces 2-5" long drooping flowers in late spring / early summer. The dark green leaves will turn to a red color in the fall. This plant is best used in moist areas as a shrub border or massing.</p>			
<p><i>Amelanchier x grandiflora 'Autumn Brilliance'</i> Serviceberry</p> <p>The Serviceberry, a Missouri native, is tolerant of a wide range of soil and cultural conditions. This hybrid variety is a large multi-stemmed shrub. The species produces showy white flowers in early spring which lead to purplish-black berries in June. The leaves turn to an orangish-red color in fall.</p>			

RAIN GARDEN PLANTING PALETTE

Native Flower and Forb Species

Black-Eyed Susan - *Rudbeckia fulgida*



Marsh Milkweed - *Asclepias incarnata*



Southern Blue Flag - *Iris virginica*



Prairie Blazing Star - *Liatris pycnostachya*



Butterfly Milkweed - *Asclepias tuberosa*



Wild Bergamot - *Monarda fistulosa*



Foxglove Beardtongue - *Penstemon digitalis*



Ironweed - *Vernonia arkansana*



Native Grass, Rush, and Sedge Species

Bottlebrush Sedge - *Carex lurida*



Brown Fox Sedge - *Carex vulpinoidea*



Virginia Wild Rye - *Elymus virginicus*



Winter Scouring Rush - *Equisetum hyemale*



Common Rush - *Juncus effusus*



Prairie Dropseed - *Sporobolus heterolepis*



Tussock Sedge - *Carex stricta*



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