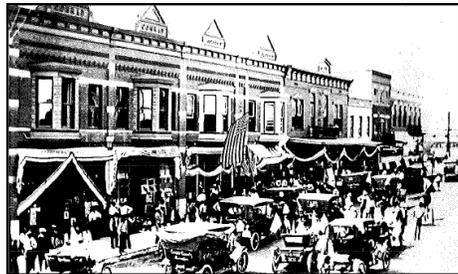


# California, Missouri

DOWNTOWN  
REVITALIZATION &  
ECONOMIC  
ASSISTANCE FOR  
MISSOURI



BUILDING AND  
STREETScape  
CONCEPTS

JUNE 2014



## ACKNOWLEDGMENTS



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## 1.0 INTRODUCTION

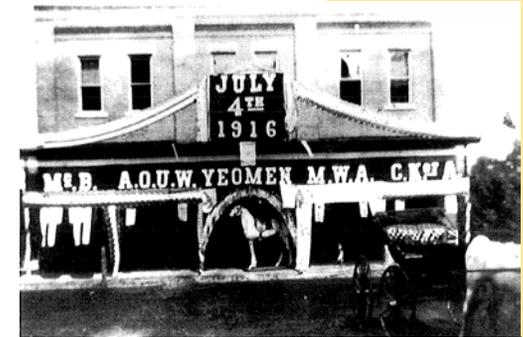
### 1.1 Historic Overview

The City of California was originally established in 1834 as the town of Boonsborough. It became the county seat when Moniteau County was organized in 1845. In 1846, Boonsborough changed its name to California because another Boonsborough already existed in Missouri. In 1847, the county courthouse was erected on the square. California was incorporated in 1848 and elected its first officials in 1858. The construction of the current courthouse was completed in 1868 and is still in use.

Completion of the railroad was instrumental in the commercial growth of California. Downtown (throughout this report, 'Downtown' refers to the DREAM Study Area. Please refer to page 4 for a boundary map; which includes portions of areas locally referred to as 'Uptown' and 'Downtown' California.) was integral to the town's growth, serving as the commercial and civic hub for California. Similar to many towns, Downtown served the local community as the place to go for trade, religious services, education, and socializing. Prominent buildings showcased the prosperity and quality of life in California. Historic buildings/districts such as the Moniteau County Courthouse Square, Finke Opera House, Gray-Wood Buildings, and Old Barnhill Building are examples of California's architectural assets.

Downtown's building stock can be characterized as a core of multi-level commercial buildings with a unity of materials, scale, and style, around which modest homes had been constructed. Some Downtown commercial buildings possessed intricate design elements and the street had an appearance of rhythm and order. Similar patterns and elements were repeated on façades, providing each building with a visual connection to its neighbors. Historically, commercial façades in Missouri were one, two, or three-stories of masonry with a high level of detail in the upper façade. Masonry details would have included corbelled recessed window panels, stone window sills, and arched window openings. Some of the buildings would have also featured painted pressed metal cornices with rich architectural details such as the building name and year of construction. Façade construction may have incorporated cast iron and pressed metal.

By the 1950's, American lifestyles had changed with the rise of highway construction and affordable automotive travel. Neighborhoods and commercial areas shifted away from the traditional downtown business district. While still the center of much community life, downtowns



Historical image of the building that houses the present-day Fraternal Order of the Eagles.



View of California, Moniteau County ca. 1869. (Library of Congress; Map Collections)



Historical image of west side of Courthouse Square.

started to experience a loss of commercial activity. Consumers expected easy vehicular access and parking. New and modern design was preferred over traditional and old construction. As a result, by the mid 1970's many American downtowns suffered from a lack of investment and declining importance in the minds of residents.

The historical streetscape was generally not spared either; historic light poles and fixtures were removed and replaced with out-of-scale "cobra-head" fixtures and poles. Collectively, the changes to the built environment of downtowns were not positive and contributed to the loss of main street business activity.

## 1.2 Intent of Concepts

This document represents conceptual planning recommendations for the City of California to consider regarding the future policy and procedural decisions that affect Downtown buildings and public spaces. Some property owners may find inspiration from the illustrations. Included are recommendations to help recapture the characteristics of existing buildings and guide new private and public development; including in-fill construction.

Downtown California has many buildings which have design merit and character. The recommendations within these design concepts will help preserve existing buildings and spaces which still have historic integrity. This report also suggests methods of improvements for buildings which have lost their character due to inappropriate alterations or neglect. Restoration of buildings to the original design is not the goal of this report, unless the building is on the National Register of Historic Places. The focus for this report is on improving the public façade and appearance of structures, streets and public spaces to help create a unique identity for Downtown. The objective is to maximize contributing elements and minimize detracting elements.

While these guidelines are written for Downtown California, the design concepts are sound advice that might be applicable elsewhere in the community. These concepts are written primarily for commercial areas, however they also briefly address issues regarding residential buildings and sites. The main intent of the guidelines is to help preserve the architectural character and improve the visual appearance of Downtown California.

## 1.3 Existing Context

Downtown California has a typical grid street layout. High Street runs north-south with the highest concentration of older buildings in varying condition. The buildings and surrounding streets comprise a dense, well-defined central business district. Although there are a number of cross streets and access points, the primary access route into Downtown California is directly along Oak Street connected by interchange to Highway 50 south of Downtown. The railroad runs east-west at the southern edge of the DREAM Study Area.

The massing of buildings in Downtown California is most concentrated along High Street resulting in nearly complete building block formats. The buildings found on the Moniteau County Courthouse Square and along both High and Oak streets provide a mix of commercial, institutional, and residential uses, along with several parcels that are vacant or used as surface parking lots. Some of the buildings retain their architecturally significant elements, however many structures suffer from deferred maintenance or were subjected to unfortunate alterations. Many buildings were altered negatively by the addition of elements which are inappropriate in scale, material, and finish. Also, some façades were compromised by the boarding of upper-floor windows, excessive signage, and out of scale awnings.

While many buildings were altered negatively, Downtown California also has examples of good maintenance and rehabilitation efforts. Many of these structures represent a good cross-section of style and age within monumental buildings in Downtown. There are also a few buildings with good façade design elements such as fabric awnings, appropriately-scaled signage and the correct replacement windows. Other rehabilitation efforts are also ongoing on several buildings in and around Downtown.

The existing streetscape in Downtown California has some positive attributes, however there is great opportunity to upgrade and coordinate all the elements, including installing additional period style lights, planters, waste receptacles, and street trees. All of these elements, combined with a largely intact building stock, provide a tremendous opportunity to enhance the architectural character and civic identity of California.

The following page provides a map of the Downtown California DREAM Boundary.



View of East North Street on Courthouse Square.



Buildings that contribute to the architectural character of Downtown California.



## 2.0 SUSTAINABLE DESIGN

### 2.1 Introduction

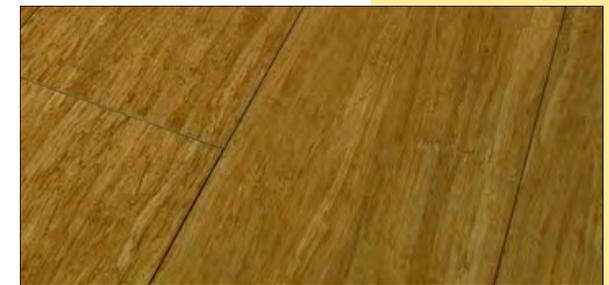
The construction of sites and buildings has a significant impact on the natural environment. The operations of a site and/or a building, can also affect the air, land and soil of the downtown. Sustainable Design measures seek to lessen the impact on the natural and built environment. Such efforts also aim to increase the efficiency at which buildings operate, in regard to energy use and operating costs. The design process is comprehensive and includes site selection, building orientation, and specification of sustainable materials and energy efficient operating systems. These design properties should be considered with private buildings, as well as with the public streetscape.

Downtown California is a built environment of many historic buildings, modern buildings, public streets, parking lots, a few vacant lots and other open space. Sustainable design measures can be applied to existing buildings, new buildings, and streetscape and site improvements. The U. S. Green Building Council (USGBC) has become the leading organization in developing standards for sustainable design for buildings. The USGBC's certification system is known as Leadership in Energy and Environmental Design (LEED). The majority of LEED certified buildings are new construction projects; however the USGBC has also developed standards for the upgrade of existing buildings. Sustainable design is a broad and encompassing initiative which strives to create a built environment which is good for both man and nature. The following recommendations only introduce the basic fundamentals of sustainable design regarding downtown buildings and environments. For additional information beyond these guidelines, resources include:

- USGBC: [www.usgbc.org](http://www.usgbc.org)
- Whole Building Design Guide: [www.wbdg.org](http://www.wbdg.org)
- American Society for Testing and Materials International (ASTM): ASTM E2432— Standard Guide for General Principles of Sustainability Relative to Buildings: [www.astm.Standards.e2432.htm](http://www.astm.Standards.e2432.htm)
- Sustainable Sites Initiative: [www.sustainablesites.org](http://www.sustainablesites.org)
- Grow Native: [www.grownative.org](http://www.grownative.org)
- Smart Growth America: [www.smartgrowthamerica.org](http://www.smartgrowthamerica.org)



Permeable paving used for parking areas to allow storm water to percolate back into the soil.



Interior flooring fabricated from bamboo; a rapidly renewable resource.

## 2.2 Fundamentals

Sustainable design measures are constantly changing, however there are six fundamental principles which constitute sustainability:

- 1) **Optimal Site Potential:** Consider site selection, building orientation and existing natural features of a site, including topography, drainage, landscape and natural habitats. The rehabilitation and reuse of existing buildings should always be evaluated as an alternative to new construction.
- 2) **Efficient Use of Water:** The design and use of water systems in a building maximize efficiency and recycle water for on-site use when feasible. Site design should seek to reduce storm water run-off from the site. Use Best Management Practices to limit storm water run-off, clean storm water, and prevent suspended pollutants from reaching the sewer system.
- 3) **Environmental Materials and Resources:** Utilize building materials with a high percentage of recycled content or contain rapidly renewable materials such as cork flooring, bamboo cabinetry, wool carpeting, etc. Specify or use materials or items which are manufactured within proximity to the project site. Ideally, this proximity is no more than 500 miles.
- 4) **Optimal Energy Use:** The operation of a site and building identify methods for increased energy efficiency or use renewable resources such as solar or geothermal energy.
- 5) **Interior Environmental Quality:** Identify methods for creating a healthy environment, and increasing the comfort of building users. Proper ventilation, use of natural light, and moisture control are a few methods to ensure a quality interior space.
- 6) **Optimal Operations and Maintenance Methods:** Utilize building systems, furnishings and finishes which will have minimal operations and maintenance needs. Such systems will require less energy, less water, and can be maintained with natural cleaners which are non-toxic to the environment or occupants.



“Green Roofs” reduce storm water run-off, reduce heat gain, and provide aesthetic areas for building users.



Solar panels can be used to provide an energy source for lighting, building use, signals, and even automobiles.

## 2.3 Elements

Sustainable design elements vary in intensity and cost to implement. The following list seeks to introduce only a few recommendations which are applicable to Downtown California:

- **Parking and Service Areas:** Minimize storm water run-off by using pervious pavement materials such as pervious paver systems or pervious concrete. Such systems will allow storm water to percolate into the soil and not into the public storm water sewer system.
- **Building Materials:** Utilize materials which are composed of recycled materials or manufactured from rapidly renewable materials, which are made from plants that are typically harvested within a 10 year cycle. Examples include: bamboo flooring, linoleum flooring (made of wheat flour and linseed oil), cotton batt insulation, and wheat board cabinetry. Recycled bricks from demolished buildings should also be used for new building construction or restoration projects.
- **Alternative Transportation:** Promote by providing secure bicycle storage and changing/shower facilities for employees.
- **Solar Energy Alternatives:** Install solar panels to supplement the power system for commercial and residential buildings. Utilize prefabricated solar water heaters to provide the majority of the hot water needs for buildings.
- **Stewardship:** New wood products, including construction lumber, should be certified by the Forest Stewardship Council, which promotes responsible forest management.
- **Lighting:** Develop a lighting plan for public spaces which minimizes excessive lighting, which affects night sky viewing and the migratory patterns of birds. Flags which require lighting should be lit from the top shining down on the flags instead of being lit from the ground, projecting light into the sky.
- **Operations:** Use timers on public fountains and lights in non-essential areas to shut off lights after 1:00 a.m., in order to reduce energy consumption.
- **Landscaping:** Plant native landscape materials which can survive on natural rainfall once established.
- **Street Furnishings:** Specify site furnishings such as benches, waste receptacles, bollards, and planters which are made from recycled plastic materials.
- **Water Conservation:** Capture rain water runoff from roofs in rain barrels for irrigation use or direct to rain gardens on site. Inside buildings, consider waterless urinals or low flow water closets to limit potable water use.



Permeable pavement system installation.



Rain garden with native landscape plants.



Solar water heater.

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## 3.0 BUILDING DESIGN CONCEPTS

### 3.1 Rehabilitation and Maintenance of Existing Buildings

Any original element or material that still exists on historic or Downtown buildings should be retained. These original components provide historic value that cannot be replaced and are particularly important for storefronts. Prism glass in transom windows or a decorative wooden door with beveled glass are examples of original materials to retain.

Replacement of missing architectural elements should be based on accurate duplications of original features. When an entire detail must be reconstructed, the new material should match the original in design, color, texture, and other visual qualities. Where reconstruction of an element is impossible because of a lack of historical evidence, then a new simple design complementary of the building in size, scale, and material may be considered. Use elements that complement the surrounding buildings. Often a simplified interpretation of similar features found on nearby buildings is attractive and acceptable.

#### 3.1.1 Rehabilitation, Restoration, and Renovation

The Secretary of the Interior's Standards for Rehabilitation (a summary of which is found in the Appendix as Exhibit A on Page 69) define rehabilitation as "The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical, architectural and cultural values."

Rehabilitation is distinguished from *restoration*, which is defined as "The act or process of accurately recovering the forms and details of a property and its setting as it appeared at a particular period of time by means of removal of later work or by the replacement of missing earlier work." There are many lightweight and durable materials available to replicate historic building features such as trim, cornices, medallions, columns, lintels, and other details. However, if research does not yield evidence of a particular design, the replacement element should be simple and complementary to surrounding buildings.



Example of a restored building in Downtown California.

In contrast to rehabilitation and restoration, renovation seeks to modernize a building. Little attention is paid to retaining historically significant architectural features. Renovation, by its very nature, destroys the historic integrity of a building. Once a building is renovated, it may no longer be eligible for State or Federal Rehabilitation Tax Credits or listing on the National Register of Historic Places.

As property owners prepare to invest in their buildings, they should seek professional guidance for rehabilitation or construction projects. The Secretary of The Interior's Standards for Rehabilitation and various Preservation Briefs are included on the National Park Service website at [www.nps.gov](http://www.nps.gov). Property owners interested in applying for Historic Tax Credits should contact the Missouri State Historic Preservation Office at [www.dnr.mo.gov/shpo/](http://www.dnr.mo.gov/shpo/).

### 3.1.2 The Benefits of Rehabilitating Buildings

Proper rehabilitation of buildings will provide several long-term benefits for Downtown California property owners and tenants. Building rehabilitation may include façade improvements, updates to mechanical, electrical, and plumbing systems, and new interior finishes. Repairs also address code and safety regulations and make the building more marketable. A building's façade presents the first impression of the building, owner, and business to the public; which is often the most lasting impression and one that impacts the entire Downtown. An attractive façade is inviting and sets a high-quality standard for other buildings. Well-maintained buildings will add to the overall image of Downtown.

Buildings in Downtown California demonstrate a wide range of design character. There are some that show significant architectural character, and some that are simple and plain. Collectively these buildings contribute to the atmosphere of Downtown. Buildings which have lost their character due to alterations or neglect detract from the Downtown experience. Restoration to the original design is not always necessary; unless the building is on the National Register of Historic Places. The objective is to maximize elements that enhance the Downtown California experience and minimize detracting elements.

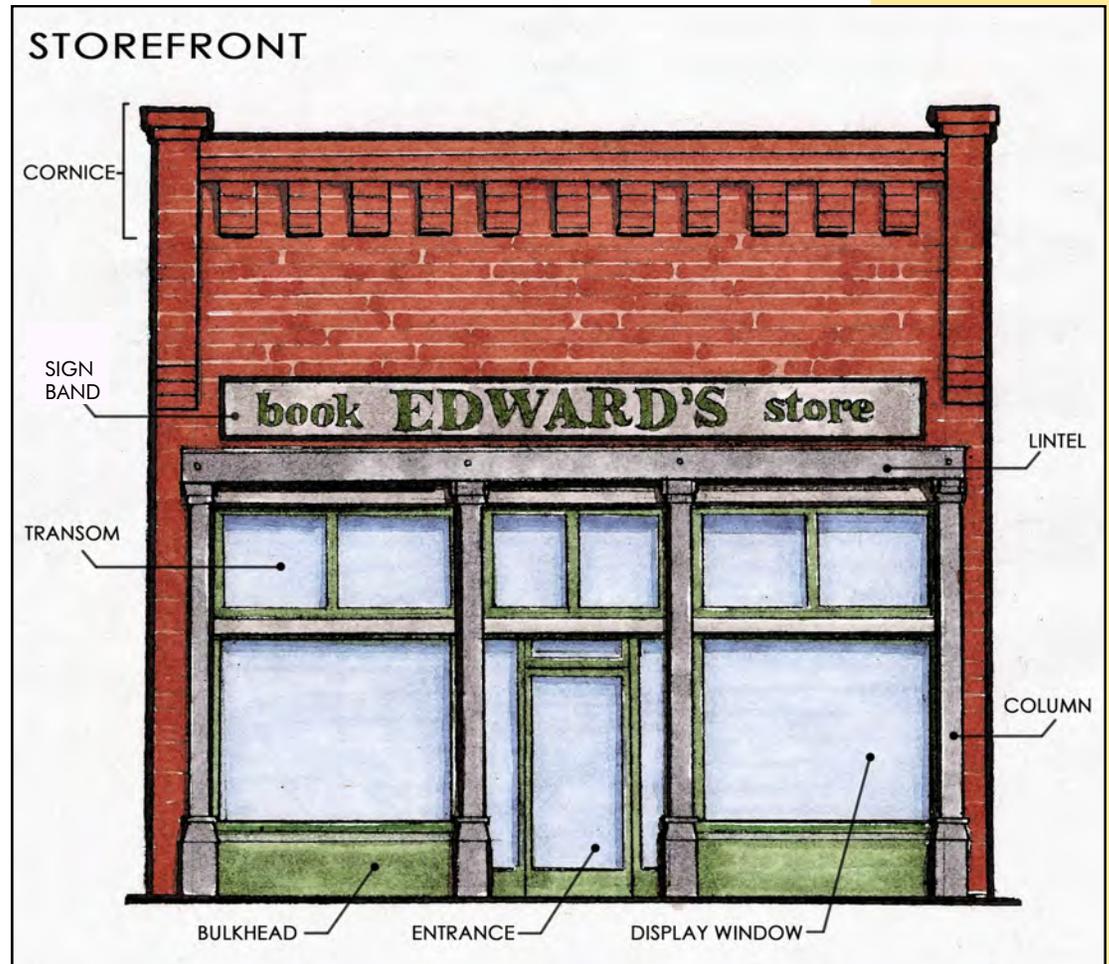


Example of rehabilitation in Downtown California.

### 3.1.3 Building Zones

Improvements to buildings will be discussed in the context of three distinct zones; the **Storefront (Façade)**, the **Upper Façade**, and the **Rear (or Side) Elevation**. The storefront is the most critical element, as it provides the interface between the business and the street. Components of the storefront include some upper façade elements, but the primary focus is on the building façade at the pedestrian level. A storefront zone and some important upper façade elements are shown on the illustration at right and discussed in Section 3.1.15 on Page 23.

The upper façade is found above the pedestrian level on the front of the building. This is an important part of the building façade which should be well-maintained by the property owner. The Upper Façade Zone is discussed in Section 3.1.16 on Page 24. Rear or side elevations of a building may present opportunities to create a pleasing shopping atmosphere. Rear and side elevations are discussed in Section 3.1.17 on Page 24.



### 3.1.4 Façade Elements

The various elements of a façade must be balanced. Restoring appropriate massing, building and floor heights, proportions, roof lines, materials, and setbacks are critical considerations in rehabilitation construction. Other aspects such as architectural details, colors, and cornices are more important to the restoration of historic buildings, but can be used effectively in rehabilitation construction. Developing a balance of all façade elements can allow a building to be very individual in its character, but at the same time be a complementary thread woven into the overall fabric and feel of Downtown.

### 3.1.5 Rhythm

The defined rhythm of Downtown California should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing. Proper repetition of architectural details and orientation to the street, along with vertical elements such as entrances, lighting, and street furnishings can help develop the rhythm of a specific block.

### 3.1.6 Alterations

Encourage removal of inappropriate alterations or additions that disrupt the fabric of the storefront. It is possible that non-historic and new construction can complement the building fabric that has developed, therefore some alterations may remain. Decks, structures providing access for people with disabilities, and other “detachable” alterations can be used, but should be unobtrusive and located on the rear or sides of the building.

As a rule, any and all alterations or additions to the Upper Façade Zone should be removed. Alterations in this zone can significantly change the overall appearance of the building. Signage and building lighting should be carefully considered, as these elements are generally restricted to the storefront. Avoid removing or altering any historic material or significant architectural features. Care should be taken during the removal process to avoid damage to original elements hidden behind the alterations. When disassembly of a historic element is necessary, use methods that minimize damage to the original materials.



An example of how rehabilitated façades and storefronts establish rhythm along this block of South Oak Street in Downtown California.

### 3.1.7 Masonry

Masonry is typically the preferred façade material for Downtown. Most existing construction will utilize some masonry. In most instances, metal and wood siding is not a suitable choice for Downtown building façades. These types of siding provide harsh lines, stark contrast, and no relief or warmth to the buildings. If wood was the historic material, it may be restored. Effective recommendations related to the treatment of masonry façades include:

- Maintain the original color and texture of masonry walls. Stucco or paint should not be removed if this was the historic covering and only applied if it was the historic covering. If painted or stained masonry is going to be returned to its original state, a minimally intrusive removal process should be used.
- Clean masonry and mortar only to limit deterioration or to remove heavy soiling. Avoid techniques such as sandblasting, caustic solutions, and high-pressure water blasting, as they may erode the surface and accelerate deterioration.
- Masonry restoration, particularly on historic structures, should always be done by experienced professionals.
- Damaged bricks and stone should be repaired or replaced with similar color, texture, and style of masonry products. Re-point masonry walls when there is evidence of disintegrating mortar, cracks in mortar joints, loose brick, or moisture retention in the walls. New mortar should duplicate the old mortar in composition, bonding strength, profile, color, and texture. Do not use cement mortar in brick construction as it is too hard and will result in cracking of the softer brick material.
- Portland cement as a patch for masonry is unacceptable.
- If a historic façade has been covered with metal or wood siding, it should be removed. Exposing the underlying brick masonry will help re-establish the character of the building and contribute to the visual continuity of the block. Metal cladding or siding can hide interesting details and may be easy to remove. If, after removing the covering, portions of the façade must be replaced, use a material similar to the original façade in color and texture.
- Regular maintenance of foundations is required to prevent structural and water damage. Any water-proofing methods for foundations should be applied beneath the finished grade or inside the structure.



An example of a poorly maintained masonry wall in Downtown California.

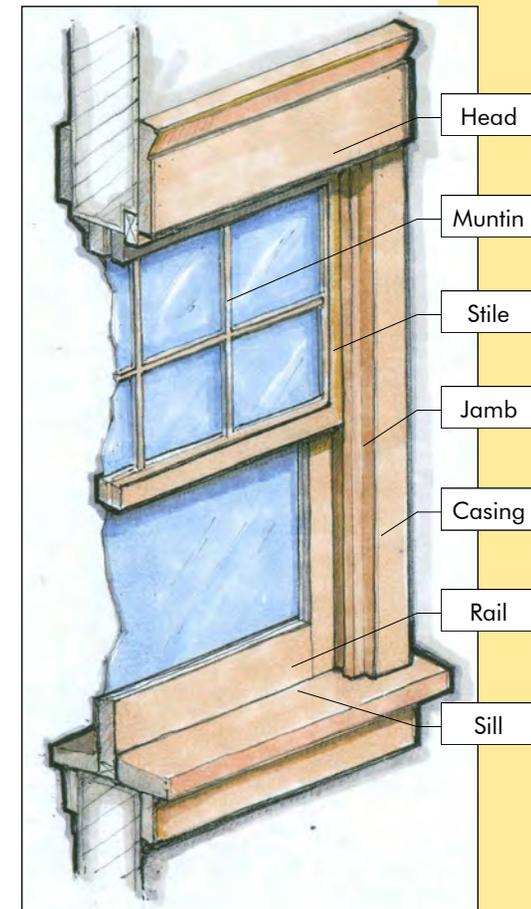


An example of siding in Downtown California that may be hiding attractive masonry details and/or windows.

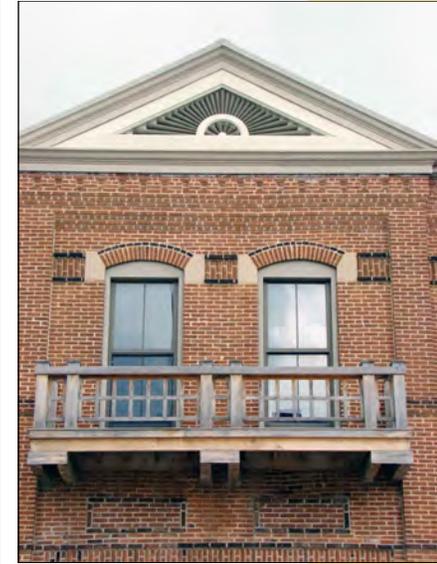
### 3.1.8 Windows

Windows are a major feature of the building exterior and vary with each building style. Windows have a proportional relationship to the structure as a whole, and they also have a decorative function. The shape and glazing pattern of windows on a building may be one of the principle characteristics in identifying its historic period and style. Thus, if original windows are removed and replaced with incompatible modern windows, the basic character of the building will be altered substantially. Recommendations for the treatment of windows in Downtown buildings include:

- The number, size, and locations of existing window openings should be retained. Do not “block-in” windows to reduce the size of the window opening or to fit stock window sizes. New window openings should not be added on elevations that are subject to view from a public street. If the original window still exists, it should be restored.
- Retain and repair window frames, sash, decorative glass, panes, sills, heads, hoodmolds, moldings, and exterior shutters and blinds. Replacement window parts should duplicate the material and design of the original part. Reuse parts in their original configuration if disassembly is required.
- If duplication of the original window or window part is not technically or economically feasible, a simplified version of the original may be acceptable as long as it has the same size and proportion. Modern window types that are inappropriate in commercial buildings include large picture windows, casements, and bow windows, unless they are original to the structure.
- Do not install shutters on windows that did not originally have shutters. The shutter should measure the full height of the window and half its width, so as to cover the entire window when closed. Fasten shutter to the window frame and not the siding.
- Inappropriate modern window features such as plastic and metal awnings or fake, non-operable, synthetic shutters and blinds, distract from the historic appearance of a building and should be avoided.
- Storm windows should have wooden frames, or if metal, should be anodized or painted to blend with the trim and be installed on the interior, rather than exterior.



- Upper floor windows are typically vertically-oriented and uniformly spaced across the building front. These upper façade windows help establish rhythm and are an important unifying feature of Downtown.
- Masonry infill, wood panels, or mismatched windows should be replaced with appropriate materials.
- If the ceiling is lower than the window head, pull the ceiling back from the window to keep the original height at the interior window.
- Use design elements that reflect the building's style. A simplified interpretation of similar features on comparable buildings may be considered.
- Encourage window shades or curtains in colors that coordinate with accent trim.
- Owners of vacant property should be encouraged to keep interior coverings, shades, or blinds closed on upper floor windows and allow displays or other materials in ground-floor spaces to reduce the visual impact that a vacant building can have on Downtown.



Examples of unique window features and architectural details found in Downtown California.

### 3.1.9 Architectural Details

There are some intricate architectural details found on Downtown California buildings. The existing details are the signatures of the builders and designers and represent a connection to California's past. In typical modern construction, such details are omitted. Restoration of features such as cornices, medallions, ornamental glass, brackets, and brick patterns are often forgone in renovation work for covering that hides any hint of the heritage of the building. Exposing and restoring these elements develops a unique atmosphere that only Downtown can offer. Suggestions for treatment of these details include:

- Replacement of missing architectural elements should be based on accurate duplications of original features. In some cases, an entire element must be reconstructed. In the event that complete replacement is necessary, the new material should match the original in design, color, texture, and other visual qualities. Photographic evidence is a good source for research.

- If the cornice is missing, a similar cornice of like size and scale should be installed. If no evidence exists as to form and detail, the reconstructed cornice should be as simple and non-intrusive as possible. A cornice is an important building element that leaves a very obvious absence if it is missing. If the cornice is intact it should be repaired and maintained.
- Where architectural details have been removed, refer to historic photos for details to use as patterns for new designs. Where exact reconstruction of details is not feasible, consider a simplified interpretation of the original, in which its major forms and lines are retained.

### 3.1.10 Awnings and Canopies

Awnings used in the Storefront Zone provide shade for merchandise, shelter for pedestrians, and bring a colorful accent to the building façade that can be changed frequently and without great expense. Canopies are more permanent structures built onto the front of the building and may include lighting for the sidewalk in front of the store. Upper window awnings provide shade and help establish rhythm along the street. The following suggestions enhance appropriate use of awnings and canopies:

- Mount the top edge to align with the top of the transom or the framing between the transom and the main display window. This will help strengthen the visual continuity of storefronts.
- Awnings should be installed over the original storefront opening and not extend beyond. Awnings over the building entrance will help customers navigate.
- Use multiple awnings on upper-floor windows, not one large awning. Do not use canopies on upper-floor windows. The use of a canopy or one large awning imbalances the façade and looks too imposing from the street view.
- Aluminum, steel, and wood shingle canopies are typically not original building material elements and tend to detract from the overall appeal of building façades. These canopies should be removed to expose and repair transom windows, the sign band, and any architectural details.
- Roll-up awnings were common on historic storefronts and can be restored with modern fabric. If a roll-up awning is not operable, the awning should follow the shape of an operable awning.



An example of inappropriately painted masonry and windows in Downtown California.



An inappropriate wood shingle awning, covered windows, and mismatching brickwork give this Downtown California building a disorderly appearance.

- Awning colors should coordinate with the palette for the entire building and complement any overall scheme established for Downtown buildings. Awnings on upper windows should match the storefront awning in color and material.
- “Quarter round” or “Barrel” awnings were not used on typical historic downtown buildings and should be avoided.
- Awning signage or lettering should not be allowed where another flush-mounted sign exists and may be limited to the vertical front flap of the storefront awning only; not the upper-floor awnings. Signboards under the awning to assist pedestrians, should be of a limited, uniform size and complement the awning and building.
- Awnings will wear and replacement should be acknowledged as an operating cost of doing business. Awnings can be changed every few years for a fresh look.

### 3.1.11 Entrances

An entrance is an important feature that affects all building tenants and beckons the visitor on the street. The primary building entrance should be obvious, but side and rear entrances should also be well-defined and attractive. Easily identified entrances assist in wayfinding for pedestrians and motorists. Suggestions for enhancing entrances include:

- Recessed entries allow customers to exit the main pedestrian flow on the sidewalk as they are being invited into a store. Maintain recessed entrances in good condition where they exist. These areas also provide protection from the weather, and the repeated rhythm of shade along the street helps to identify the entrance. If the original recessed entry has been removed, consider reestablishing it in the same location.
- Side and rear entries should be visible from nearby parking lots and should provide a clear, well-lighted pathway for pedestrian access.
- At least one public entrance to the building is required to be compliant with the Americans with Disabilities Act Accessibility Guidelines (ADAAG).
- Avoid doors with raised thresholds and consider using an accent color on the door.
- If there is only one business in the building, center the business signage over the main entrance.



Examples of awnings and canopies found in Downtown California.

### 3.1.12 Building Lighting

Buildings should be interesting to view at night, as well as by day. A well-lit façade or rear elevation creates a positive impression about Downtown. This lighting also can improve Downtown safety and security. Suggestions to enhance the building lighting in Downtown California include:

- Use lighting as a subtle and complementary design element to draw attention to the building. Lighting that emphasizes architecture in the upper façade and rear elevation can provide attractive highlights.
- Warm-colored lighting of the storefront should accent the entrance, flush-mounted signage, and any architectural details.
- The light fixture design should be simple, non-intrusive, and in a style that matches the period of the building. Neon and cool fluorescent lights should be avoided and well as other lighting that is too intense and overpowering of the building.
- Building lighting on rear elevations should be similar to the lighting on the façade. This lighting should also provide illumination at the entry door and along the pedestrian path from the parking area.
- Building lighting should also incorporate sustainable design concepts as noted in Section 2.0 on Page 5.

### 3.1.13 Business Signage

Each Downtown business must have its own identity, but at the same time reinforce and complement the overall Downtown design them. Effective signage identifies the business without detracting from the architecture of the building and distracting the pedestrian. Sign types and their locations should be kept simple and consistent for ease of public awareness. Signage should be restricted to the Storefront Zone or Rear Elevation and generally not allowed on the Upper Façade.

Business signage in Downtown California consists of a variety of signs, with some positive examples of attractive flush-mounted signs within the sign band and some poor installations where the signage is haphazard,



Use lighting to highlight building, signage & entrances (St. Charles, Missouri at top, Washington, Missouri at bottom).

excessive, or in an inappropriate location on the building. While such signage may not be in violation of the City Sign Code, the City may wish to review its sign regulations to determine if changes are desired. General recommendations to enhance the business signage in Downtown California include:

—**General Business Signage Design Issues**

- A business sign should be a part of the building design and not hide architectural features or details. Flush-mounted signs that fit within the outer edges of the façade and in the sign band help reinforce the horizontal lines along the street.
- The size of the sign should be of an appropriate scale for the building and street. Large signs are not needed as downtown signage is oriented to the pedestrian, not the motorist.
- In general, for all signs, the material and color should complement other building and façade materials. Clashing colors and inconsistent material types can cause visual distress in the viewer or customer.
- Signs should be well-designed and professional with a simple message. The name and type of business should be sufficient. A logo or symbol of the type of business could also be included.
- Signs should be constructed of high-quality materials to withstand extreme weather conditions. Good craftsmanship results in longer service time and conveys a stronger image. A deteriorating sign presents a very poor image.
- Encourage innovative sign design to reinforce uniqueness. Mass-produced signs, such as rectangular plastic “box”-type signs with internal lighting, fail to make a lasting impression in the mind of the visitor.
- Illuminate signs in such a way to enhance the overall composition of the façade. External lighting cast from period-style, non-intrusive fixtures is preferable to internal lighting.
- Rooftop, blade, pole, neon, electronic message boards, flashing or otherwise moving or animated signs, signs playing music or sounds, and billboard signage should be severely restricted in a downtown environment, if even allowed. Abandoned signs should be removed.



Examples of existing business signage found in Downtown California.

- Place exterior signs near the business entrance to guide a customer's eyes to the storefront door.
- Where several businesses share a building, coordinate the signs by grouping them onto a directory panel. Use similar forms or backgrounds that tie together visually and make each business sign easier to read.

#### —Style and Location of Signs

**Projecting Signs:** Projecting wall signs should provide the business name, logo, or product image and should generally have the following characteristics:

- **Material:** Typically unframed painted wood or metal panels hung from painted wall brackets. Wood signs with carved or sandblasted designs that are painted are also appropriate.
- **Lighting:** Non-illuminated or externally lit with appropriate fixtures.
- **Location:** The bottom of projecting signs should be no lower than eight feet above the sidewalk and the top of the sign should be below the building parapet or upper-floor windows.
- **Message:** The use of symbols instead of text on projecting signs can help a customer more easily identify and remember the store.

**Wall Signs:** These signs are painted on the brick wall or a panel above the storefront windows or on the side of the building. The old faded signs painted directly on the brick are commonly called "ghost signs" and should be preserved as historical building elements. Downtown California was home to some wall signs, of which some evidence remains today. Generally, wall signs should have the following characteristics:

- **Material:** Painted directly on the building brick or on wood or metal panels. Signs painted on brick typically had white lettering on a black background. Signs that advertised a product, such as Coca-Cola, were multi-colored.
- **Lighting:** Non-illuminated or externally lit with appropriate lighting fixtures.
- **Location:** Many of these signs were located in recessed brick panels above the storefront. Research historic photos for locations of original signs that may still exist beneath old paint or panels. Wall signs should not be located above the building parapet.



Examples of projecting signs found in Downtown California.

**Window Signs:** Painted or foiled lettering on the display window glass often advertised a doctor, dentist, or attorney. Signs of his type should have the following characteristics:

- Material: Painted, or gold or silver foil, lettering in colors that complement the building's paint scheme.
- Lighting: Natural lighting or the inside lights of the building.
- Location: On the glass of the entry door or the display window at eye level. These signs should be fairly simple and not dominate the window to allow for an unobstructed view of merchandise. Window signs can also be used in upper-floor windows to identify upper-floor businesses.

**Awning and Canopy Signs:** Lettering on awnings or canopies should only be used where there is no flush-mounted sign. However, a simple logo or symbol can help identify the business. These signs should have the following characteristics:

- Material: Lettering on awning fabric or painted on wood or metal panels.
- Location: Six to eight inch high lettering on the front valence of a fabric awning or a hung sign panel. These panels should not exceed 12 inches in height.

**Sidewalk Signs:** Symbols, such as barber poles, were often set on the sidewalk to attract customers. If sidewalk signs are permitted, they can effectively advertise Downtown businesses. These signs should have the following characteristics:

- Material: Painted wood or metal. Merchants should avoid stock displays that also advertise the names of products such as beverages. A well-maintained, high-quality sign of this type is important.
- Lighting: Natural illumination. Do not internally illuminate.
- Location: At the edge of the sidewalk or building face. Maintenance of the sidewalk thoroughfare is important, and these signs should not obstruct pedestrians. If the right-of-way is not large enough, sidewalk signs should be avoided or used only on the building's parking lot.



Examples of existing wall, window, and canopy signage in Downtown California.

### —Number and Area of Signs

**Principal Business Signs:** Signs to identify the name and nature of the business should be the primary type of sign and limited to two per building storefront. This signage could be any of the sign types discussed previously.

**Auxiliary Signs:** A business should have a sign stating hours of business and an “open” sign. These should be limited to two square feet. Other indoor directional signage and brand information should not be placed as another outdoor sign.

**Side Street Directories:** The side walls of corner buildings can be used for wayfinding directions to parking and other attractions. These signs should be of uniform size and design. A suggested sign panel size would be 12 inches high by 48 inches long with 6 inch high lettering.

**Sign Area:** The aggregate area of all signs for a specific Downtown business should not exceed 100 square feet, except buildings with a front wall area of 1,000 square feet or more, where the aggregate sign area should not exceed approximately 10% of the front wall area.

**Lettering Size:** The height of lettering for any sign type should not exceed 12 inches, except for the capital letters (the first letter of each word), which should not exceed 18 inches.

**Lettering Style:** Because the historic signs spanned a long time period, a variety of lettering styles existed together. Lettering style for new signs could be either simple block letters or more elaborate lettering styles. Each business should express their individuality in their sign design.

### 3.1.14 Building Color

Color can enhance the details and patterns of a building’s façade. The most effective and economical paint schemes often start with the natural colors of the building materials themselves, such as the red of many brick buildings. Avoid painting brick or masonry. Color techniques for buildings include:

- Use one base color for the majority of the background wall surface, but use a different color for accents. Avoid painting a building entirely one color.
- Base colors should be muted earth tones or pastels that will hold their color well. Owners should consider color stability when choosing paints.



Example of a good use of color which accents the building entrance. (Washington, Missouri)



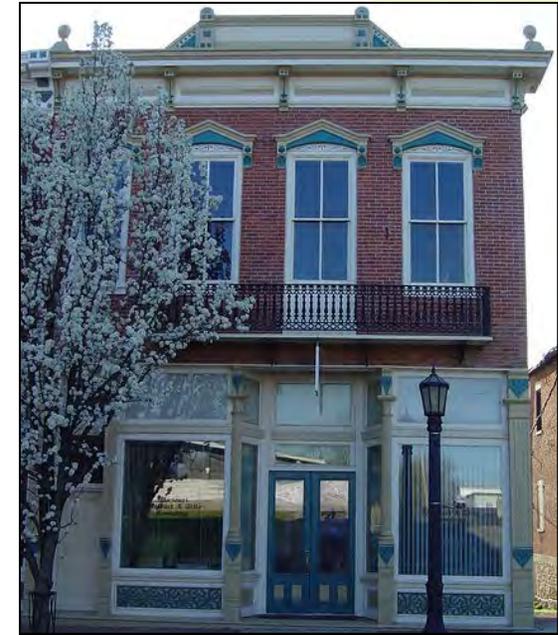
Block that follows same basic design principles. (St. Charles, Missouri)

- Window frames, sills, moldings, details, and cornices are potential architectural elements to accent with a different color. Signs, awnings, and entrance doors are also components that could be accented. Use bright colors only in small amounts and on the first-floor to direct the customer's eyes to the business.

### 3.1.15 Storefront Zone (see Page 11 for components)

By applying the design concepts found in this report to the Storefront Zone, the overall image of Downtown California buildings will be improved. Generally, all architectural details, spandrel panels, and intricate brickwork should be restored and maintained, and in some cases, accented. Suggestions for the treatment of primary storefront elements, include:

- The main entrance should be recessed to emphasize the entryway. A recessed entry, when combined with display windows, creates a unique display area and should be creatively lit during evening hours.
- The main entrance door should provide a view into the building and a sense of openness. Solid doors and clutter such as flyers and posters on the entry door should be avoided.
- Lintels and columns help frame the building entrance and should be restored and maintained. Often these features are hidden behind an inappropriate covering.
- Large display windows should be preserved if present and reestablished if they have been removed. These windows are important to the overall balance and scale of the façade and provide important interior lighting. Display windows connect products with customers.
- The bulkhead serves as the base for the display window and should be in good condition to protect the window. Additionally, storage space can be provided, under the display area and behind the bulkhead.
- Framing of the windows should be of a material similar to the original framing, or of a material complementary to the historic profile. Clear, insulated glass with a 'Low-E' coating is an appropriate window. Tinted or reflective glazing, or interior films should be avoided.
- Transoms are typically a band of glass panels located above the entrance and display windows. This feature is usually at the same level on an entire block of buildings. Maintaining transoms will reinforce the horizontal lines on a street frontage.



An example of a well designed & coordinated storefront found in Downtown California.

- Transoms should be uncovered and restored. If the ceiling on the interior of the building has been lowered, pull the ceiling back away from the window to maintain the original ceiling height. Transom glass may be different from the display window, but it should complement all façade elements.
- Some transoms originally had hinged windows to provide building circulation. The building owner may wish to consider restoration of this feature. However, a hinged transom's ability to assist with air conditioning and/or heating expenses is limited by modern, efficient building systems.
- Window air conditioning units should never be installed in the front of a building, particularly not over a main entrance. These units should be restricted to the rear elevation.

### 3.1.16 Upper Façade

The Upper Façade of a building is often neglected, but the elements in this zone should adhere to the same concepts as previously suggested. The impression of a building and business is formed by the overall image of the property. Customers notice the condition of the upper façade, even though they may never set foot in the upper floors of the building. The Upper Façade is a building element that presents significant architectural style and detail to the street. Windows, brackets, cornices, and medallions can help showcase the building's character and are often found in this zone.

### 3.1.17 Rear (or Side) Elevation

The rear elevation typically faces an alley and provides access for deliveries and maintenance. In some cases, customer parking and a rear entrance to the building is provided. Attention to the appearance of the rear elevation can be extremely important to the quality of the customers' experience as well as the maintenance of the building. The building and business image can likely be improved here, while accommodating service functions. The view of rear elevations from parking areas is critical and should welcome the visitor using the lot. In addition to previously



An example of a well-maintained building entrance with a restored transom found in Downtown California.

mentioned recommendations for upper-floor building elements and entrances, suggestions for other rear or side elevation components include:

- A customer entry through a side or rear door creates an access no longer just for service and should be just as inviting as the primary entrance. Customers might also develop a sense of loyalty or “special access” by using this building entry.
- An awning should be considered for a rear entrance. Also, building elements such as gutters, downspouts, service boxes, conduit, fencing, and screens should be in good repair.
- Rear fixtures such as signage, fencing, lighting, and awnings should use materials and colors that coordinate with the building front so customers will learn to associate any rear or side entrances with a particular business. Chain-link fencing and barbed-wire should be strictly avoided in Downtown.
- Rear exit stairs, elevators, and parking lots can enhance the marketability of upper-floor space. Trash containers should be placed in an enclosure or behind a screen that harmonizes with the surrounding buildings in scale and color. Wood, masonry, lattice, or hedges can all be effective screening methods. Landscaping can also screen ground-level utility units. A color scheme that complements the building should be used on all screening.
- Ancillary structures should match the surrounding buildings in style, scale, and color scheme. Ancillary structures should not be used as residential units.
- A restaurant can take advantage of a rear parking area for outdoor seating. While this type of seating is typically located in the front or side of a building, the rear elevation could also accommodate this vibrant element. Outdoor seating is also discussed as a streetscape element in Section 4.6 on Page 38.



This Downtown California building provides an important façade, but also a professionally painted mural on its side elevation that enhances a nearby parking lot.

### 3.2 New Construction Concepts

The construction of any new structure within Downtown is important because the new structure needs to be compatible with existing buildings. New construction should seek to harmonize with the visual characteristics of the neighborhood and address elements of scale, design quality, and massing; as well as the relationship of the new construction to existing nearby buildings. Avoid recreating historical styles or themes with new construction. This practice can create a “theme park” type of atmosphere. While new buildings can be inspired by past design, the new building should feel genuine and not a creation of a false past.

The following concepts consider additions to existing buildings as well as entirely new infill construction. The concepts are intended to identify a range of design options that will complement existing buildings, not to dictate specific styles or features. The design of new construction should generally focus on massing, rhythm and directional emphasis, materials, and building elements.



Architectural details on existing buildings can provide clues for infill project developers interested in designing complementary buildings for Downtown California.

Massing and rhythm are defined by the relationship of a building to the open space along the street frontage, the relationship of solids to voids on building façades, and the relationship of the entrances and porch projections to the street. The directional emphasis (whether vertical or horizontal) of new construction should relate to that of the neighboring buildings. The defined rhythm of Downtown California should be maintained along a street frontage by adhering to uniform lot widths, building widths, and window spacing. Materials and other building elements speak to the style of the building and should be complementary of surrounding structures.

An illustration depicting concepts for designing new Downtown commercial infill buildings is shown on Page 27. The general concepts provided in this illustration include:

- Designs should be considerate of the traditional building elements as described in Section 3.1 or on nearby existing buildings that contribute to the historic context of Downtown California. Often a simple design is best, using three basic elements: a unified paint and color scheme, an awning, and non-intrusive signage.

- Properly orient the building to the street and carefully consider the relationship to nearby buildings.
- Emphasize horizontal features that can align with other buildings to reinforce the rhythm of the block. Vertical elements, entrances, lighting, and other street furnishings can also help develop the block rhythm.
- Include architectural details sparingly, with proper repetition.
- Restrict off-street parking areas to the side and rear of the building. Typically, a downtown building is built up to the sidewalk and it is important for an infill building to maintain this building line.
- Encourage multi-story construction to maintain the building roof line and to accommodate mixed-use development that reserves the ground floor for retail uses.



Infill development should support the historical architectural character of the surroundings.

Upper façade elements such as windows and sign panels should be continued.

Encourage traditional storefront awnings and upper and display window proportions. Use awnings instead of canopies.

Establish clear guidelines that prohibit materials such as metal and vinyl siding that are architecturally inappropriate. Discourage covering on transoms.

Recreate storefront elements and build to the sidewalk line to establish vibrant street rhythm.

An infill building example and illustration from Caruthersville, Missouri.



### 3.3 Franchise Architecture

To maintain the unique atmosphere in Downtown California, branding buildings in the style of a company should not be allowed. Large franchises and national chains typically have a “downtown-style” in addition to their trademarked brand. These styles are more fitting to Downtown, as opposed to a highway corridor.

The company’s “downtown-style” is particularly important if a franchise store is to locate in an attached Downtown building of historical nature. This is an infrequent occurrence as Downtown buildings are typically not suitable or attractive to franchise stores. However, the City should be prepared if a franchise store prospect desires a Downtown location. These stores can still be complementary to the historical fabric of Downtown. The design concepts for new construction provided in Section 3.2 on Page 26 should apply, and other aspects such as parking requirements, pedestrian-oriented signage, building setbacks, and building lighting may require consideration. Store owners should be able to adapt their brand to create a complementary Downtown building.



An example of how a franchise façade could enhance the surrounding street fabric.

### 3.4 Building Design Examples

The purpose of this section is to inspire and provide guidance for building owners and businesses when they voluntarily improve their building's exterior. These suggested improvements and the elevation are designed solely for the subject buildings. However, the overall general concepts may apply to other properties in Downtown California.

Recovering and maintaining the historic character of buildings in Downtown California is a priority as indicated by the information collected from City residents during the DREAM Telephone Survey and Focus Groups. The leadership of Downtown California desired to illustrate building façades located from 501—517 North High Street and the Fraternal Order of the Eagles lodge at 314 N. High Street. The block of properties on High Street is illustrated below to show how implementing potential recommendations can change the image of the entire block or Downtown. Greater detail and recommendations for each building is provided on the following pages.

Existing Conditions—Entire Block.



Potential Improvements—Entire Block.



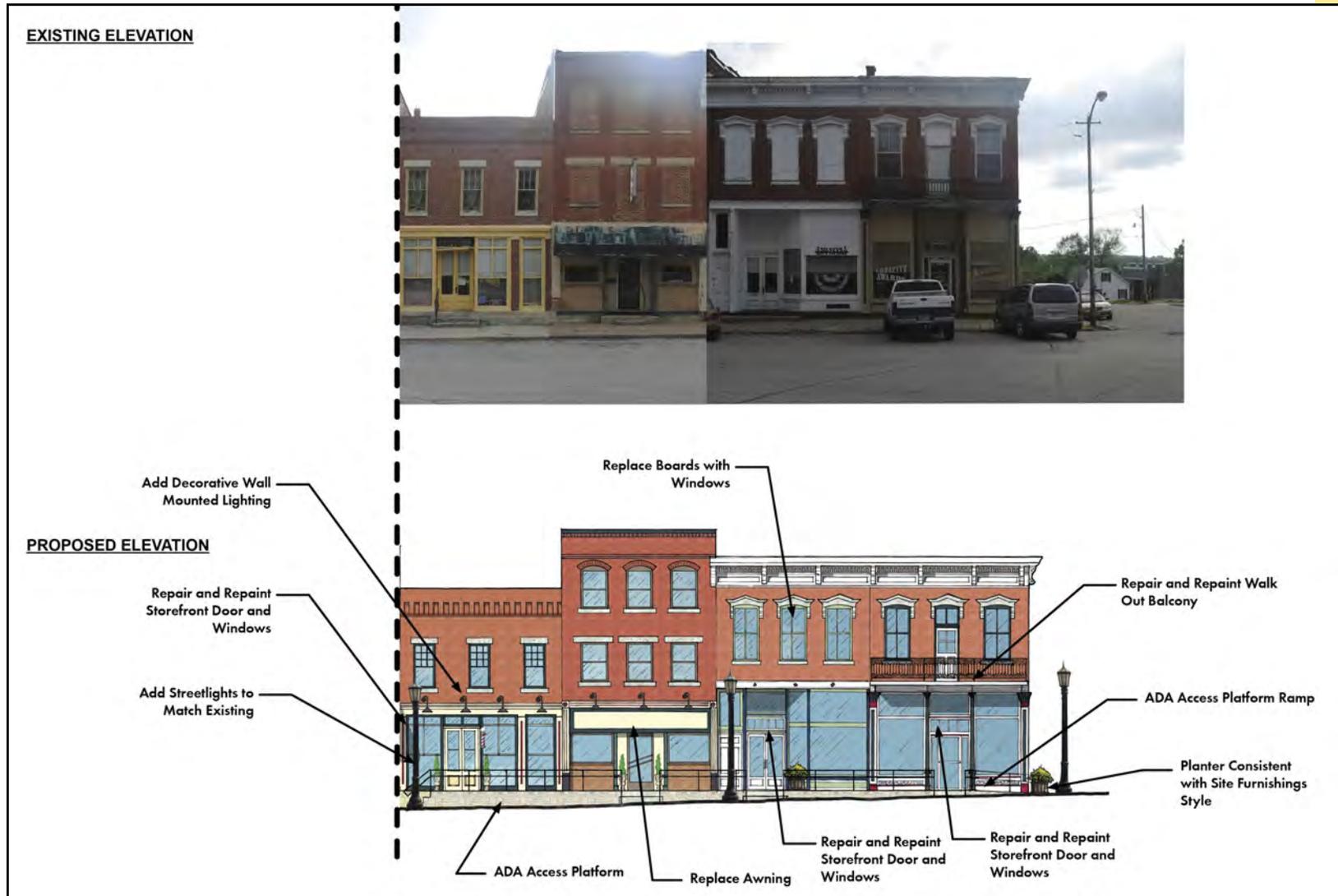
### 3.4.1 Buildings from 501—509 North High Street

This stretch of buildings is anchored by Derringer's Bar & Grill and provides significant upper-floor architecture, presence, and potential for housing units. These buildings have also undergone significant alterations that detract from their original appearance and image.



### 3.4.2 Buildings from 511—517 North High Street

This stretch of individual buildings again provides unique upper-floor architecture. Some have undergone significant alterations, but maintenance appears to be the primary concern.



### 3.4.3 F.O.E. at 314 North High Street

The façade of the Fraternal Order of The Eagles building has undergone significant alterations, but still demonstrates great potential.



## 4.0 STREETScape DESIGN CONCEPTS

The term “Streetscape” typically refers to exterior public spaces located between the building façades on each side of the street. DREAM proposes an organized streetscape with coordinated lighting, site furnishings, landscaping, and wayfinding. In some cases, streetscape recommendations can include the development of parks, plazas, or other pedestrian gathering areas.

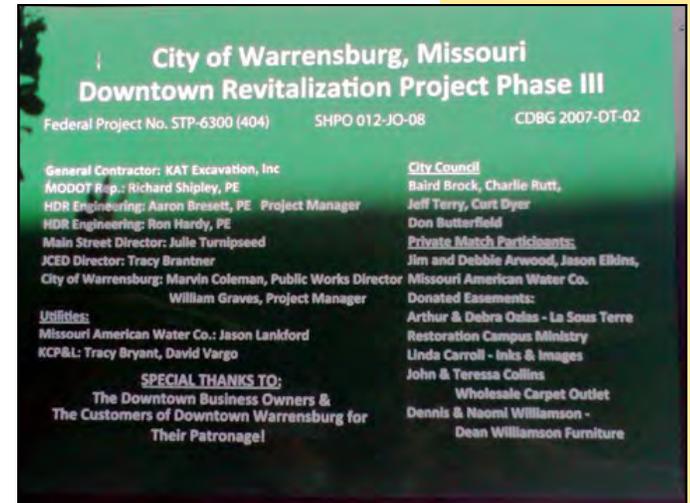
As noted on Page 3, California has installed streetscape elements, primarily decorative lighting located along High Street and around the courthouse. Existing streetscape features could be upgraded and expanded in both types of elements and geographic scope. The concepts in this section are not intended to replace the existing streetscape design, but to complement and enhance the elements that the City has already chosen. These concepts can be used to reinforce sound design ideas and provide guidance for the establishment of future public improvements in Downtown California.

### 4.1 Design Coordination

An overall design approach will display a sense of order and rhythm through the repetition of design elements. Proper coordination will help pull together the buildings, streets, parking areas, public spaces, and pedestrian walkways into a pleasing experience that encourages the visitor to explore. However, Downtown is largely a In addition to the recommendations found in this report, there are other streetscape design issues that the City should address generally, including:

- Relocation of overhead power lines to underground conduit or alleyways.
- Sidewalk replacement and updating to current Americans with Disabilities Act Accessibility Guidelines (ADAAG) or the Proposed Right-Of-Way Accessibility Guidelines (PROWAG). Ideally, overhead power lines can be buried as sidewalks are reconstructed.

Without exception, all of the physical, public-owned elements of Downtown California must be maintained. Streetscape fixtures should be reviewed on a regular basis and repairs or replacements made as timely as resources will allow. Maintenance costs required by a physically improved Downtown are unavoidable, but California cannot



An example of signage to promote a streetscape project and funding sources from Warrensburg, Missouri.



The view looking north along High Street in Downtown California; showing existing decorative lighting.

afford to broadcast a message of neglect and decline. The City must commit to enhancing Downtown and strive to keep it that way. Public streetscape enhancements demonstrate that the City is an investment partner in the ongoing improvement of Downtown.

## 4.2 Infrastructure

Downtown cannot function without intact infrastructure, but this does not mean infrastructure should just be functional. The City should view infrastructure as a design element that can be enhanced aesthetically for the benefit of residents, visitors, merchants, and property owners. Recommendations for effective and attractive infrastructure include:

- Curbs should be in good repair and constructed of a consistent material. There should be no gaps or areas of uneven elevation along the curb line. At street intersections there should be ADAAG or PROWAG compliant ramps as noted in Section 4.3 to follow. Generally, existing curbs in Downtown California are in good condition. However, there are areas along side streets which are deteriorating.
- Poorly working storm drains can cause storm water run-off to pool at street intersections. This condition makes pedestrian access virtually impossible and must be corrected.
- Street improvements such as pavement, curbs, or sidewalks should coincide with other public works projects to minimize street closings and costs.
- New sidewalks should transition smoothly into the same grade as street surfaces. New or replacement curb and gutter should be vertical curb design.
- Relocating overhead utility lines, although costly, can improve Downtown aesthetics greatly. Above ground utility enclosures should not obstruct the pedestrian walkway.



An example of a new curb.



An example of poor curb and sidewalk conditions noted in Downtown California.

## 4.3 Accessibility

The U.S. Access Board is an independent Federal agency that has been established to monitor and issue updated accessibility guidelines for new or altered facilities covered by Americans with Disabilities Act (ADA) and the Architectural Barriers Act (ABA). These major civil rights laws prohibit discrimination on the basis of disability and

establish design criteria for the construction or alteration of both public and private sector facilities for public and commercial use. These guidelines address new construction and alterations and are referred to as the ADA Accessibility Guidelines (ADAAG). A recent addition to the Proposed Right-Of-Way Accessibility Guidelines (PROWAG), is meeting acceptance by various federal agencies and will soon expand upon the ADAAG for public improvements.

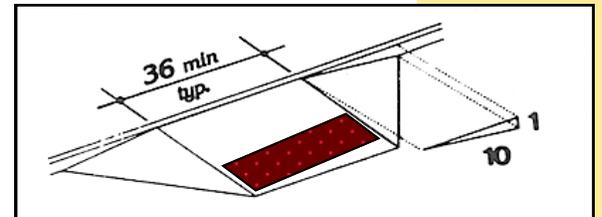
Without the required curb ramps, sidewalk travel is dangerous, difficult, and in some cases, impossible for people who use wheelchairs, scooters, and other mobility aids. Ramps allow people with mobility impairments to gain access to sidewalks and pass through center islands in streets. Additionally, vision impaired visitors to urban downtowns require detectable warning strips along ramps leading to streets. When streets and roads are newly built or altered, they must have ramps wherever there are curbs or other barriers to entry from a pedestrian walkway. When new sidewalks or walkways are built or altered, they must contain curb ramps or sloped areas wherever they intersect with streets or roads. Resurfacing a street or sidewalk is considered an alteration for these purposes. However, regular maintenance such as filling-in potholes will not trigger the requirements.

ADAAG and PROWAG provide for flexibility in many cases, such as Program Access; where an acceptable alternative route to a building may make use of existing ramps, provided people with disabilities must travel only a marginally longer route. Other accessibility considerations that will benefit Downtown pedestrians include:

- A clean, clear, and well-lit pathway should be provided from public parking areas to major Downtown activity centers.
- The City should occasionally review lot and stall conditions and conduct necessary repairs. The City should also consider enforcing private parking lot surface standards.
- To help reduce traffic and assist visitors with wayfinding, the City should encourage parking lot border treatments to help delineate private parking lots.
- Sidewalks should run continuously through an entire block to create a clearly defined pedestrian pathway and minimize conflicts between people and vehicles. All roadway crosswalks should be clearly marked with signage and striping.



An example of an ADAAG compliant ramp in Sedalia, Missouri.



An illustration of ADAAG ramp construction dimensions and location of warning strip.

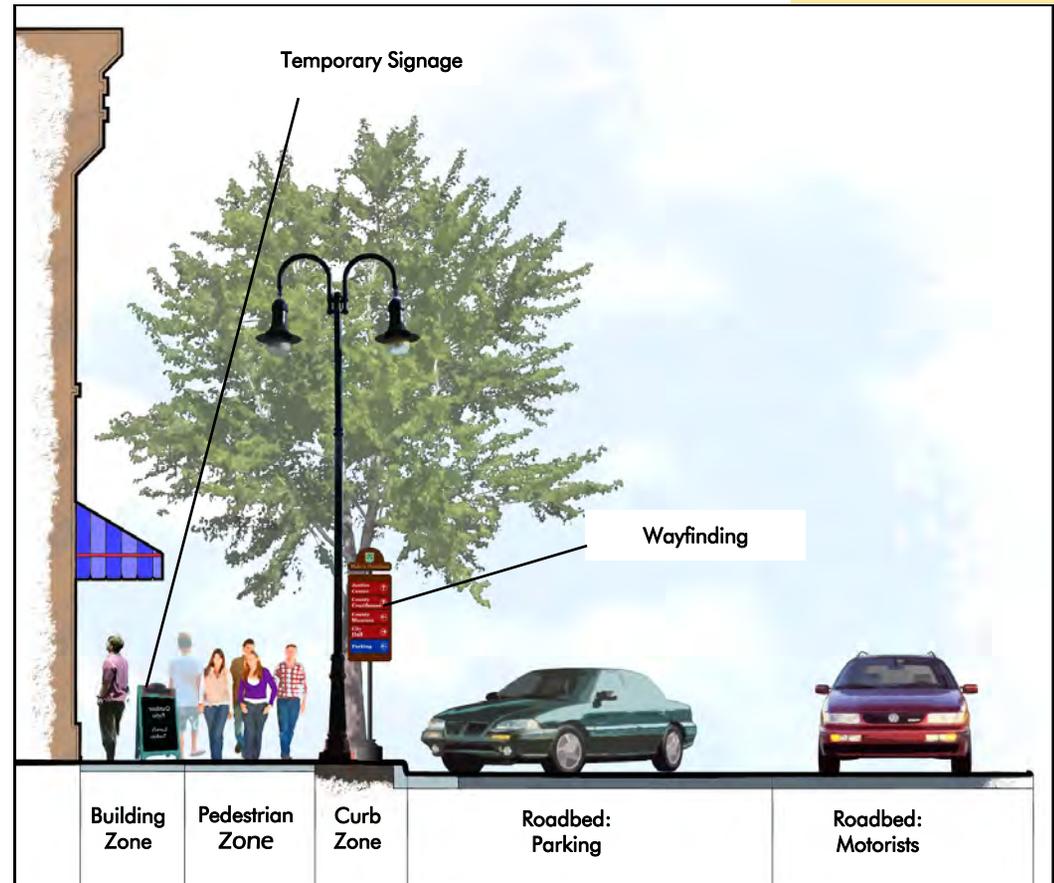
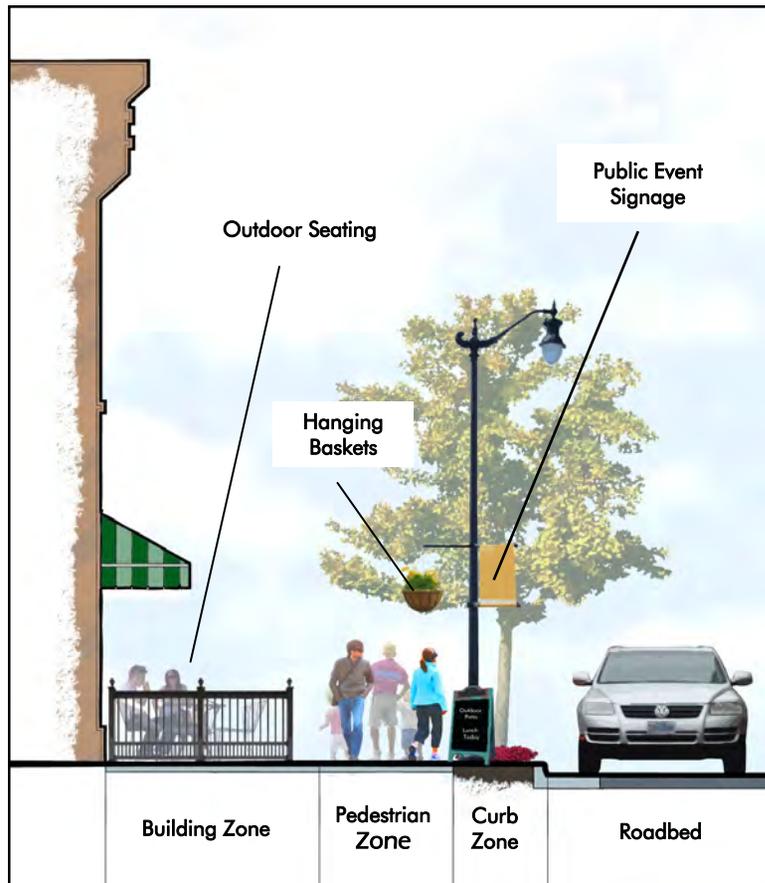
### 4.4 Sidewalk Zones

As a streetscape project is contemplated, it is important that adequate zones in front of a building are maintained. The Building Zone, Pedestrian Zone, and Curb Zone all have unique characteristics that should be regulated to ensure that private elements do not adversely impact public improvements.



Aside from ADAAG or PROWAG pavement improvements, streetscape amenities should remain clear of the Pedestrian Zone and allow for free movement of pedestrians. Amenities should enhance the pedestrian experience, not be obstructive. Businesses and property owners should be educated on the importance of maintaining Sidewalk Zones. Each business should care for the zones within their building's street frontage.

Street furnishings such as bicycle racks, bollards, and benches can add value and functionality to a streetscape, but must be installed in useful locations. Care must also be taken that furnishings are not excessive and do not block on-street parking lanes.



## 4.5 Parking and Service Areas

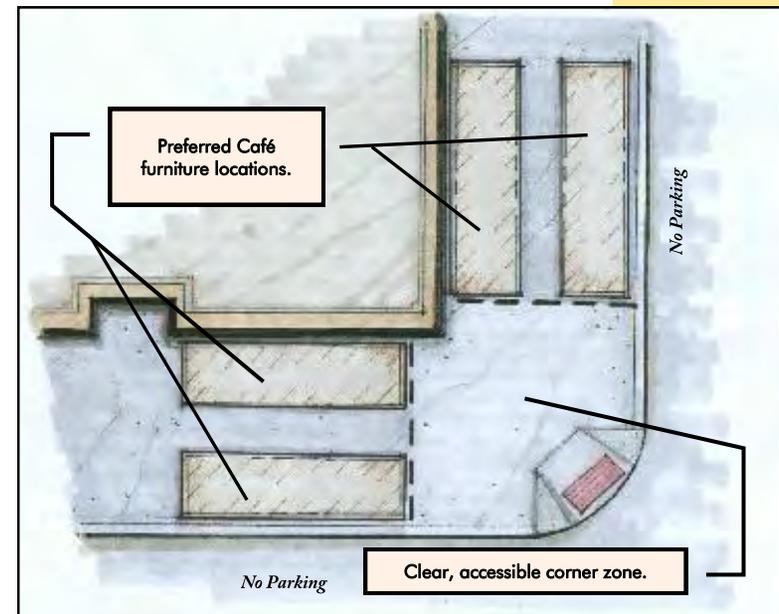
Parking lots and service areas are required to support Downtown California businesses. Street parking will accommodate some, but not all, of this required parking. Publicly owned parking lots can be improved with streetscape elements to provide pedestrian gathering places. Recommendations for Downtown parking and service areas include:

- Parking and service areas should be well-lighted and landscaped. Vacant lots may provide a temporary solution for additional parking, but a lot should be paved if it will be used as parking for more than six months. Planting buffers or decorative paving should be installed at the edges of lots to define the site border. Landscape islands should be included throughout the lot to improve aesthetics as well minimize storm water run-off.
- Parking lots and service areas should be located to the side or rear of the main business area. Parking should be clearly marked as public, or private, parking. In the case of California, some of the existing parking lots require maintenance and improvements. Additionally, there are several vacant lots that could be converted to parking as needed.
- Parking should provide a clear and well-lighted pathway for pedestrians to reach activity centers. Lots should also be maintained in good condition, along with all parts of the street, alley, and sidewalk. Parking should be connected to clearly-marked crosswalks. Visitors should feel safe in the parking lot and not have to navigate tripping hazards to reach Downtown destinations.

## 4.6 Outdoor Café Seating

Outdoor cafés or sidewalk seating is a common element of a vibrant downtown. Such seating areas can be accommodated in Downtown California, but require special attention. A proper arrangement will have the following characteristics:

- Be located in the sidewalk area fronting the restaurant. If located on the side or rear of the building, the seating should be far enough from the parking or street to avoid an unappetizing experience with car exhaust, inattentive motorists, and road grit.



- Allow a clear and unencumbered path along the sidewalk for pedestrian traffic. The sidewalk must maintain accessibility compliance and the restaurant owner should be held responsible for the pathway.
- Not obstruct entrances to the building and provide a clearly defined area connected with the restaurant. Areas adjacent to the building should be ideal.
- Use umbrellas or other patron shelter in a color and style that complements the building.
- Consist of furnishings that are durable, weatherproof, windproof, sturdy, and properly maintained. Furnishings should be stored off-site during the winter months.
- Should include sturdy trash receptacles. The restaurant owner should be held responsible for maintaining the cleanliness of the outdoor seating area.

## 4.7 Street Lighting

Street lighting should enhance the pedestrian experience and nighttime image of Downtown, while also providing an attractive installation during the day. Generally, street lighting should:

- Provide pools of light on the sidewalks at a higher level of illumination than the roadway. Storefront lighting can add to this illumination.
- Be on 12'-14' high poles and project light down onto the sidewalk, not into second floor windows.
- Be uniform in style, type, height, color, type of illumination (LED, compact-fluorescent, high-pressure sodium, etc.) and brightness throughout Downtown.
- Be equipped with brackets for banners and electrical outlets that can display banners and decorations.
- Be part of an overall lighting design strategy to ensure desired lighting levels. Street lighting should also illuminate parking areas, rear entrances, and alleys, as well as streets.

## 4.8 Signs and Banners

A unique design for public traffic signage should be used to identify and define Downtown California. This type of signage is often only thought of as a functional element to regulate traffic, with no concern for aesthetics. With every Downtown business having at least one business sign, public signs can quickly get lost. These signs are critical for wayfinding, traffic flow, and ensuring the safety of Downtown pedestrians.

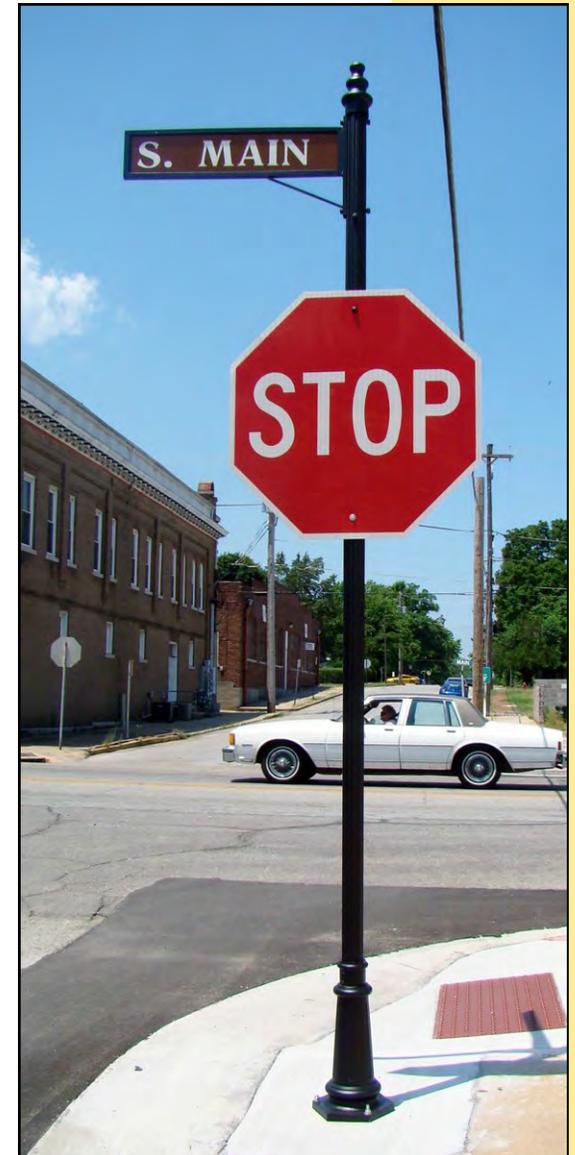


The existing lighting style, along with a street banner, in Downtown California.

Wayfinding techniques and components to assist in navigation through the area will be addressed in detail in Section 4.14 on Page 48 and include details regarding a conceptual wayfinding plan developed for Downtown California.

Coordinated signage will help define the boundaries of Downtown. Concepts to improve the public signage in Downtown California, include:

- The installation of decorative street name signs in Downtown California should be considered to help reinforce a feeling of place for Downtown. This is an important decorative feature the City would maintain as the streetscape is expanded. To reduce visual clutter, regulation and directional signage should be combined where possible.
- Encourage Downtown property owners to install street address signs on their buildings that complement or match the style of the street name signs. A historic plaque-type can provide a very elegant touch. The style, font, and colors of these signs should be easy-to-read for local emergency personnel.
- Temporary banners and other signs for public events and attractions can be allowed, but should be restricted as to size, number in one location, and length of display.
- Seasonal banners or decorations that are approved by the City can be installed to create a festive and vibrant atmosphere. Banners, such as the City's existing design found on Page 39, add a sense of civic identity. These banners are most effective with a simple, repetitive, design. Lettering should be kept to a minimum and sponsor panels should only be allowed within a uniform design panel, if at all. The City should provide all maintenance of public signage, banners, outlets, and brackets. The City should also change the banners on a regular schedule, replacing hardware or faded banners as needed.
- Balloons, pennants, and other distracting sign novelties should be strictly regulated in a downtown environment. These elements can be used on public signage, but this should be uncommon.
- Murals should always have an artistic component and should only be allowed by City approval. A mural is a professionally painted piece of art and should not be used as a business sign. Any mural not approved should be considered in violation of the sign code.



An example of decorative traffic signs in a downtown environment. (*Webb City, Missouri*)

## 4.9 Public Furnishings

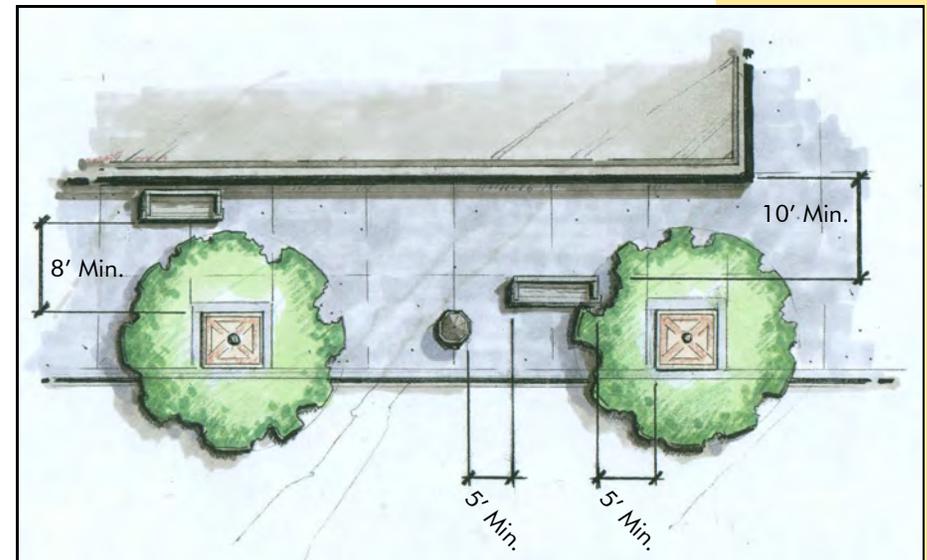
The furnishings included along a street or in a pedestrian plaza, parking lot, or park should be considered as part of the overall streetscape in terms of design. Elements should complement and introduce users to the theme of Downtown California. Clusters of furnishings will provide gathering places for pedestrians and will encourage visitors to park their vehicles and explore.

The City of California has some installed furnishings in Downtown. General guidelines for the future installation of public furnishings, include:

- Benches within the streetscape encourage social interaction which contributes to a successful Downtown. However, some benches should be oriented so that a pedestrian can sit facing into the storefront. Suggested minimum distances for bench placement are shown below right. Actual distances may vary due to site conditions.
- Planters and window boxes provide color and an opportunity to include volunteers from local garden clubs.
- Trash receptacles, bollards, tree grates, and boundary fencing should all be an attractive Downtown accent, not just utilitarian components.
- Sturdy materials that can be painted are preferred for public furnishings. Wood and soft materials can be vandalized and should be avoided. Concrete is a sturdy material, but is not easily moved, repaired, or replaced. Painted metal is a better choice.
- When grouped together, streetscape furnishings help to enhance Downtown's appearance and create a pedestrian-welcoming environment with functional gathering spaces.
- Furnishings should complement light and sign posts to present a unified streetscape design. The styles of furnishings should be simple and not too intricate or flashy. Furnishings should be economical to maintain and of a style that can be easily repaired or replaced.



An example of a streetscape bench that is sturdy and easily maintained.



An example of Downtown bench placement.

- The City may wish to restrict private property owners from installing their own furnishings on the right-of-way in front of their building; instead, providing a catalog of approved choices that are complementary to the rest of the streetscape design components.
- Many site furnishing manufacturers offer customization options for their products, usually via plaques or laser-cut designs (logos, icons, text, etc.) This provides a great way to personalize streetscape elements and creates opportunities for community/organizational involvement, dedications, memorials and sponsorship fundraising.

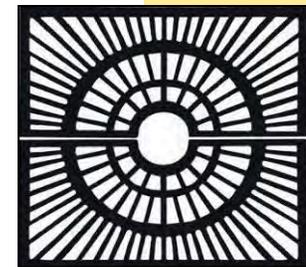
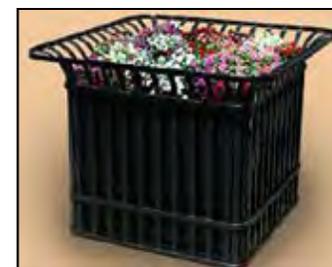


Downtown California may wish to consider customizable trash receptacles.

### 4.10 Bicycles

Downtown California should be friendly toward bicyclists as well as pedestrians. Issues for a cyclist include routes of travel, clearance, access to water, type of traffic signals, traffic lanes, signage, drainage grates and curbing obstacles, and parking. Downtown plans should implement bicycle facilities which can be used by local citizens and visitors. The City should identify opportunities for future bicycle facilities in Downtown and throughout the community. Considerations to improve the bicycle friendliness of Downtown California, include:

- Bicycle racks which should be of uniform design of materials, color, and style as other streetscape elements and located at useful activity nodes throughout Downtown.
- Directional and regulatory signage which identifies bike routes and share-the-road routes. Dedicated bicycle lanes on streets, where feasible.
- Wayfinding signage to various destinations within Downtown.
- Public restrooms and drinking fountains.



Examples of sturdy furnishings suitable for installation in Downtown California.

## 4.11 Fountains and Public Art

Fountains and public art can enhance Downtown and the pedestrian experience greatly. These features will be most effective as simple interactive elements which Downtown visitors can enjoy. Fountains could be simple bubblers that provide a refreshing respite in the summer months, and are lower maintenance than fountains in a pool. General guidelines for future Downtown California streetscape projects, include:

- Fountain water should be left in its natural state without coloring.
- Commissioned professional public art and sculpture provides an inspirational atmosphere in which people enjoy lingering. Professional temporary or seasonal exhibits could also be installed.
- Fountains and art can also serve as memorials, commemorating City founders or other notable citizens or stories of historical events.
- Fountains and art should not encroach upon the pedestrian walkway.

## 4.12 Landscaping

Existing landscaping in Downtown California includes street trees. Street trees are typically the most prevalent and obvious landscaping element in a downtown, but often street tree efforts fail due to concerns with the trees dropping fruit, attracting birds, or hiding business signage. The City of California is no exception to these problems and should consider developing a standardized list of suitable street trees in an attempt to minimize these concerns. Issues to consider when formulating such a list, include:

- Species selection should be adapted to the local climate, soils, etc. (native species)
- Avoid species that are susceptible to insects or disease
- Avoid species that produce fruiting berries to reduce sidewalk maintenance
- Select species that provide a minimum of 7 feet of vertical clearance before lateral branching begins to minimize blockage of businesses
- Specify species with downward growing taproots, not lateral growth that will damage surrounding pavement



An example of art district signage, and art itself, installed on a light pole.



An existing street tree installation in Downtown California.

Although certain tree species can attract birds more than others, it is important to note that whenever trees are provided, so too is a habitat for birds. Even though the concern cannot be completely avoided, steps can be taken to lessen the potential impacts. A suitable tree list should recommend species that are resistant to insects, are non-fruiting, and have smaller leaf coverage. Also, the Missouri Department of Conservation has wildlife damage and urban wildlife biologists on staff to help the City determine ways of managing these types of problems.

The tree list for Downtown California should not be too limiting, ensuring many different suitable species to foster creativity and sustainability. A common misconception is that named trees on a list will work in any conditions within Downtown, regardless of site conditions (soil type, moisture, light availability, etc.) This misconception can result in many failed planting projects. With these factors in mind, the list below and on the following pages is an example of potential suitable street trees for Downtown California.

**Native to north-central Missouri** (these should be the first choice)

**SMALL TREES**

*Amelanchier arborea* (service berry)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h290/amelanchier-arborea.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h290/amelanchier-arborea.aspx)

*Cercis canadensis* (redbud)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h550/cercis-canadensis.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h550/cercis-canadensis.aspx)

*Crataegus viridis* (green hawthorn)

(look for spineless and disease-resistant varieties such as “Winter King”)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/b964/crataegus-viridis.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/b964/crataegus-viridis.aspx)

*Ostrya virginiana* (hop hornbeam)

<http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/l330/ostrya-virginiana.aspx>

*Viburnum prunifolium* (black haw)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/g240/viburnum-prunifolium.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/g240/viburnum-prunifolium.aspx)



Example of Redbud street tree.



Example of Hornbeam street tree.

**LARGE TREES** (for wider tree lawns, public spaces, parks, etc.)

*Acer nigrum* (black maple)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a844/acer-saccharum-subsp.-nigrum.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a844/acer-saccharum-subsp.-nigrum.aspx)

*Acer saccharum* (sugar maple)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h240/acer-saccharum.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h240/acer-saccharum.aspx)

(maples tend to be over-planted in urban areas; conduct a tree inventory before adding)

*Betula nigra* (river birch)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/x800/betula-nigra.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/x800/betula-nigra.aspx)

*Celtis laevigata* (sugarberry)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a857/celtis-laevigata.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a857/celtis-laevigata.aspx)

*Ulmus americana* (American elm)

(choose Dutch Elm Disease resistant varieties such as “Jefferson” and “Valley Forge”)

<http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a922/ulmus-americana.aspx>

**Other suitable Missouri Natives** (to add variety, but not to be planted in as large of numbers)

**SMALL TREES**

*Carpinus caroliniana* (musclewood)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h540/carpinus-caroliniana.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/h540/carpinus-caroliniana.aspx)

*Chionanthus virginicus* (fringe tree)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/c120/chionanthus-virginicus.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/c120/chionanthus-virginicus.aspx)



Example of Black Maple.



Example of River Birch.

**LARGE TREES**

*Liriodendron tulipifera* (tulip tree)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a878/liriodendron-tulipifera.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a878/liriodendron-tulipifera.aspx)

*Nyssa sylvatica* (black gum)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a670/nyssa-sylvatica.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a670/nyssa-sylvatica.aspx)

*Quercus phellos* (willow oak)

(has small acorns, so requires minor extra maintenance as a street tree)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a191/quercus-phellos.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/a191/quercus-phellos.aspx)

*Taxodium distichum* (baldcypress)

[www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/m510/taxodium-distichum-var.-distichum.aspx](http://www.missouribotanicalgarden.org/gardens-gardening/your-garden/plant-finder/plant-details/kc/m510/taxodium-distichum-var.-distichum.aspx)

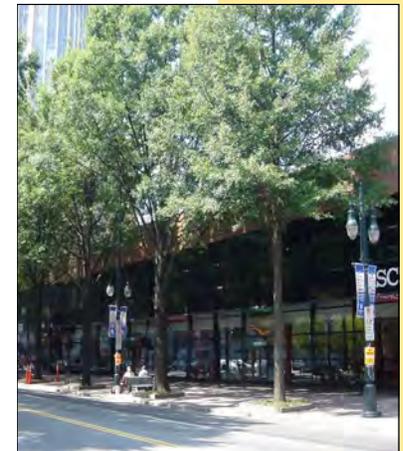
The City of California should also conduct a tree inventory. The inventory will highlight the numbers of trees and species that currently exist and will help the City avoid overplanting any one family, genus, or species. The inventory will also help to identify problem trees that require targeted maintenance, replanting, or removal. Funding for urban forestry activities may be available from agencies such as the Missouri Department of Conservation.

For added color in Downtown, other landscaping should include shrubbery and flower planting beds, hanging baskets, or rain gardens. There are parking areas that could be defined with landscaping and there are some opportunities where excess pavement could be removed for landscaping beds. Planting palettes for shrubs and rain gardens (discussed in section 4.13 on page 47) are included in the Appendix as Exhibit B on Page 71. Other general landscaping concepts for Downtown California, include:

- If landscaping in front of a business is desired, plants in movable containers should be used where no landscape strip is present. Containers should be placed immediately adjacent to buildings or curbs.
- Shrubs should be massed in groupings of five to seven plants with no more than two different species within a planting bed.



Example of Black Gum.



Example of Willow Oak.

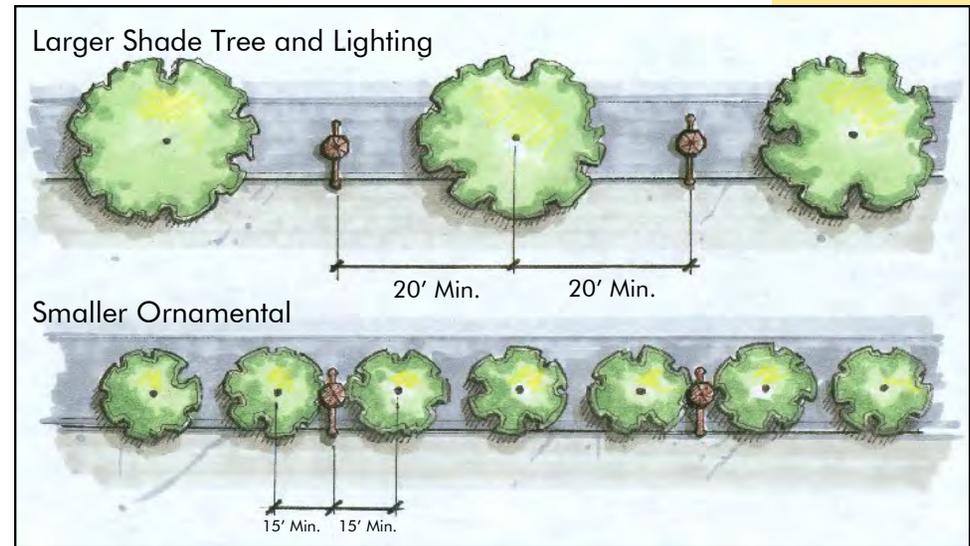
- Suggested minimum tree/shrub placement distances are shown at right. Actual distances may vary due to site conditions.

All Downtown landscaping will require adequate soil volumes and rooting space, as well as regular maintenance and watering schedules. The City should also consider decorative elements such as metal tree grates.

### 4.13 Rain Gardens

Rain Gardens are low-lying landscape beds designed to collect rainwater from adjacent impervious areas. A properly designed and installed Rain Garden will ease the load of the existing storm sewer system and reduce erosion and pollution. Rain Gardens also help to facilitate filtration and absorption of rainwater back into the ground. Rain Gardens require proper design and plant selection for maximum efficiency and reduced maintenance costs. Careful consideration to these factors will reward Downtown California with significant environmental impacts for a low cost. Even small Rain Gardens can have a large impact on storm water run-off. General recommendations for Rain Gardens include:

- Low maintenance native plants are recommended due to their greater tolerance for climatic and soil conditions, as well as extreme moisture.
- Rain Gardens should provide aesthetic benefits to the streetscape.
- Rain Gardens should be located so that they do not create an obstacle for street cleaning and maintenance.
- As with any landscape bed, regular weeding and clearing of litter is required.
- Rain Gardens can replace existing landscape beds so as to not require a reduction of parking spaces.



An example of a Downtown Rain Garden from Springfield, Missouri.

- Species that have trouble thriving should be immediately removed and replaced.
- The City should consider the use of porous pavement in nearby parking areas to help reduce the amount of overall standing water impacting Downtown.
- Rain Gardens should be viewed as a treatment for storm water before it enters the water system. Access to conventional drainage structures should be maintained to prevent flooding during heavy storm periods.
- Curb inlets should be provided to allow water to enter and exit the Rain Garden if necessary. The curb inlets should be tapered to minimize damage from maintenance vehicles.

#### 4.14 Wayfinding

Wayfinding is an indispensable tool for directing travelers to destinations and creating a positive first impression. The term wayfinding was originally coined by Kevin Lynch in his 1960 book *The Image of the City*. Lynch presented the concept that people use a cognitive map to move through their environment to their destination. Wayfinding is a system to assist travelers in interpreting their cognitive maps. The goal of a wayfinding system is to make the journey to a destination as transparent and seamless as possible.

As part of the DREAM process, a potential wayfinding system was developed. The overall concept was provided for the entire City. However, there was a Downtown component and detail developed and presented to Downtown California leaders. This concept includes the elements noted in this section and promotes the goal of ease of navigation, primarily for visitors to Downtown California. Illustrations of the elements for Downtown are found on the following pages, including mapping for potential locations of signs. By providing such a comprehensive approach to the wayfinding system, California can reinforce its unique identity and sense of place.

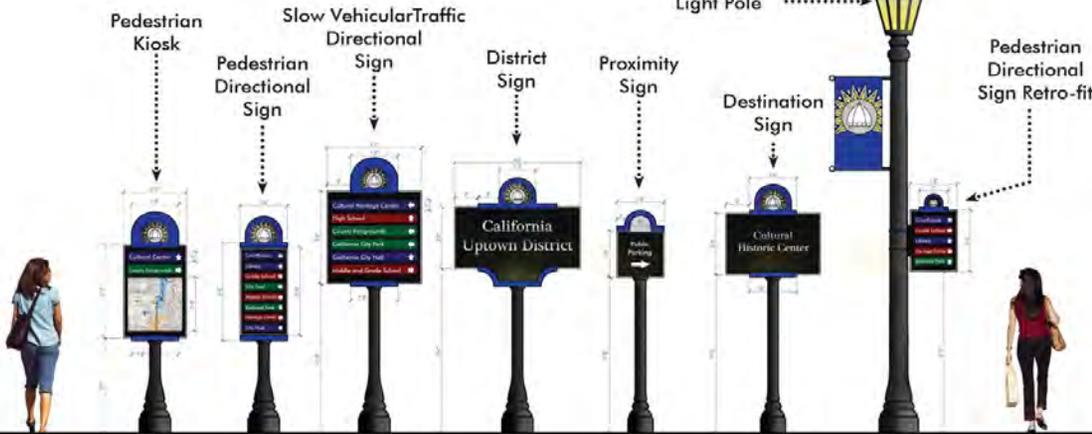


A City of California Primary Gateway sign concept developed during the DREAM program.

# DREAM CALIFORNIA - WAYFINDING SIGNAGE

## PROPOSED DESIGN CONCEPTS

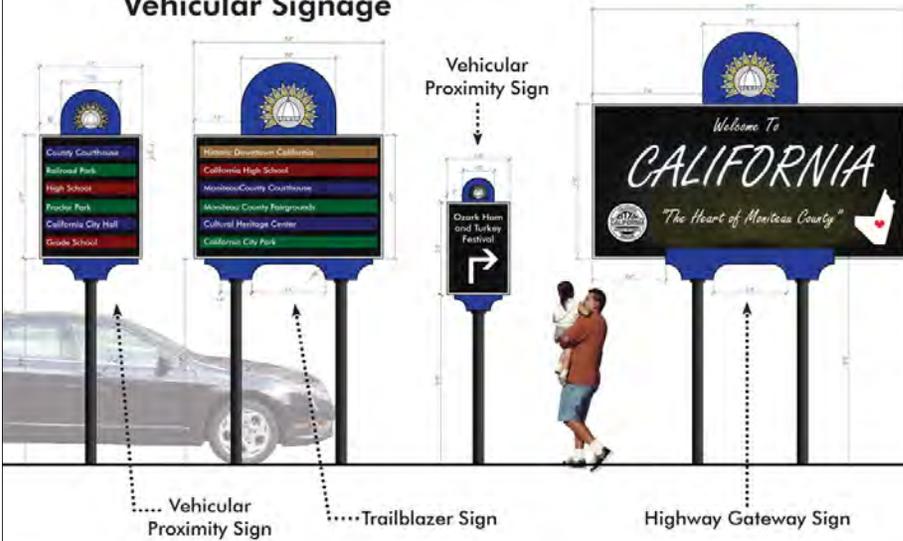
### Pedestrian / Downtown



#### Notes

- Pedestrian sign base are to match decoration of existing antique street light poles, unless otherwise shown on plan.
- Sign panels are to be constructed of 1/4" thick aluminum, with rounded edges.
- Sign Panels are to be attached with aluminum sleeves.
- Base should be set on breakaway footer as designated by MODOT standards.

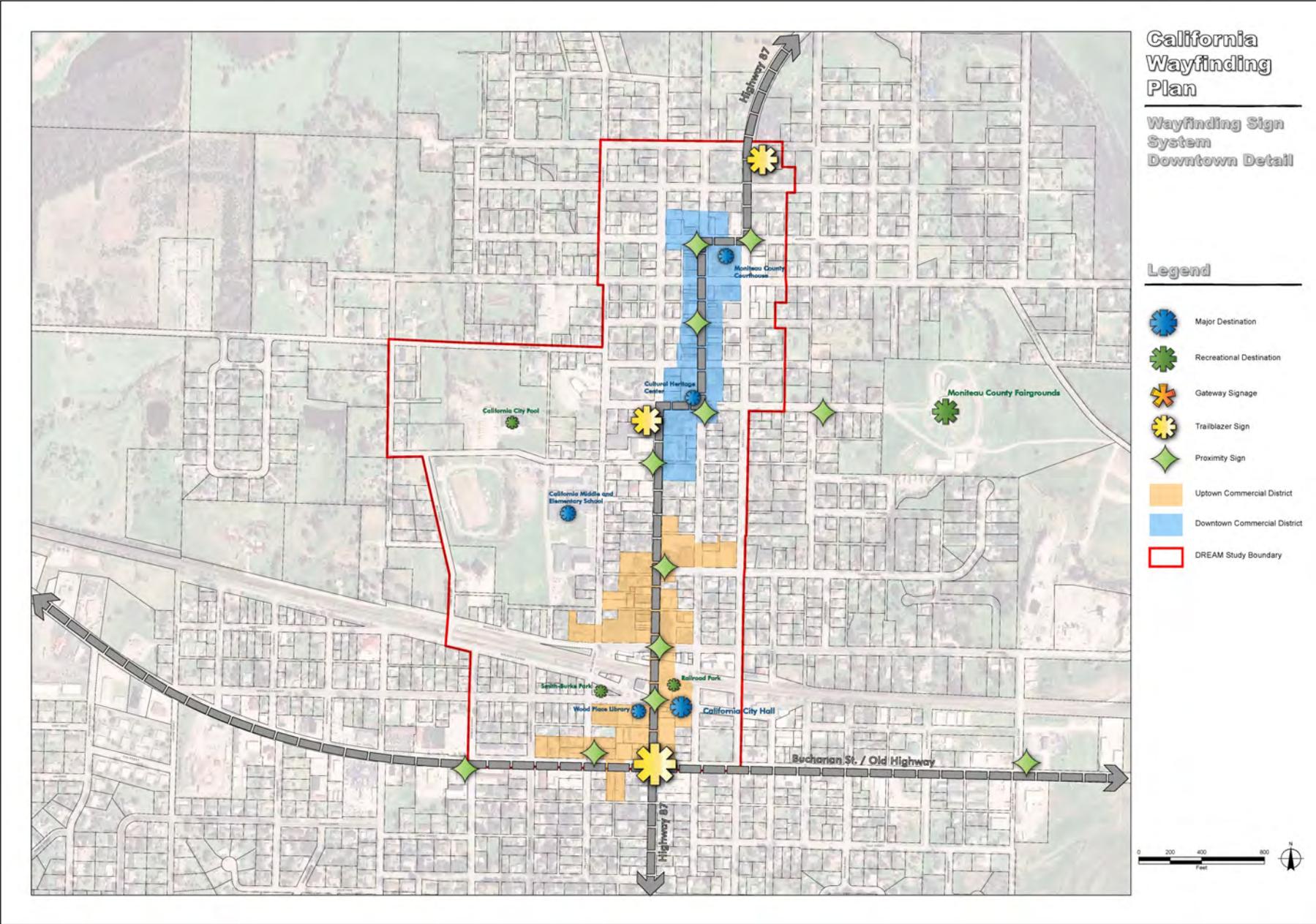
### Vehicular Signage



### Gateway Signage



\*Note: This gateway sign can not be placed in Highway ROW



Although the concept illustrated on the previous pages has not yet been implemented, any approach to a wayfinding system in California should include design and building codes centered on four primary aspects:

**Architecture:**

- Visual clues of buildings and other features of a street aid people in knowing their location and the direction of their destination without the use of signage. Such clues also draw the pedestrian's eyes to where an entrance or display window is expected to be located.
- Buildings presenting strong architecture serve as landmarks and orientation points. These points are often destinations and starting points.

**Sight Lines:**

- Visitors will feel most comfortable if they can maintain visual contact with their destination and will want to make few directional changes. Clear sight lines down streets at key intersections should be maintained. Avoid allowing buildings to encroach on, or block, these lines.
- Repetitive landscaping and furnishings can enhance the view down streets, but care must be taken that streetscape components do not obstruct important navigational landmarks.

**Lighting:**

- Lighting can be used to encourage routes and pathways. Warmly-lit sidewalks and streets draw the customer onward, while similarly lit storefronts and entrances will be appealing to the customer.
- A repetitive line of lighting can be an effective navigation tool.
- Poor lighting causes missed information and leaves an unsafe impression in a visitor's mind.

**Signage:**

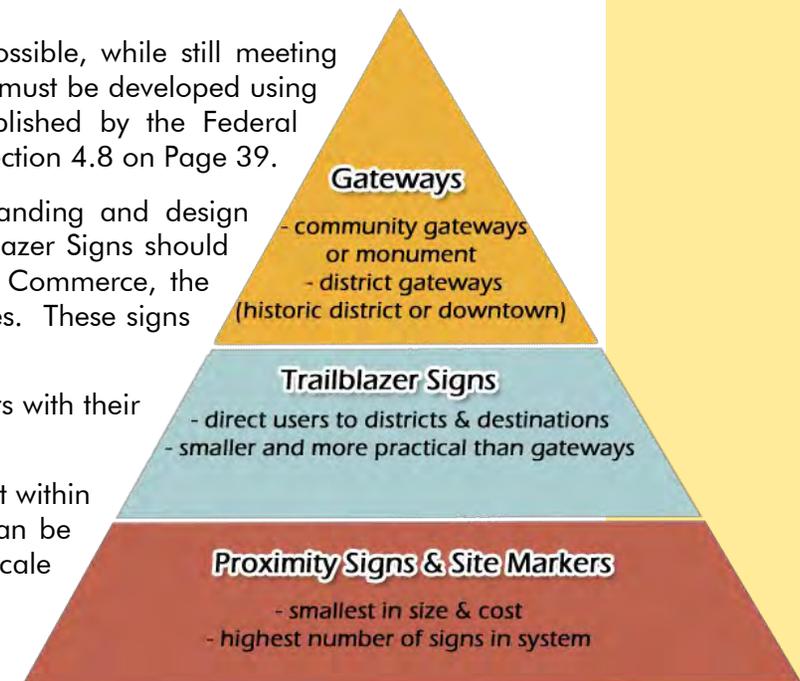
- Uniform signage at important decision points is a critical element of wayfinding. For the concept already developed, California attractions and activity centers were inventoried and considered for their destination potential and draw.
- Kiosks can direct visitors to various attractions, advertise events, and consolidate signage.
- Excessive signage will lessen the effectiveness of any individual sign. Fewer, easy to read signs placed at strategic locations are preferred.

Wayfinding systems create an arrival sequence to Downtown. The system consists of themed signs of various types that direct travelers to attractions. All too frequently existing wayfinding systems are inadequate. Typical problems include:

- Lack of accuracy, with arbitrary sign location.
- Visual clutter from too many signs or diffuse allocation of signs across many entrances.
- Lack of focus in directing traffic to Downtown or routes directing travelers around Downtown.
- Lack of charm or just standard Department of Transportation signage.
- Signs are too small with inconsistent sizes, colors, and types or are too wordy and hard to read.

Components of successful wayfinding systems seamlessly integrate the visitors experience with the messages needed to navigate to, and around, Downtown. These components include:

- Primary Gateway Sign—Should serve as the “Welcome” to a visitor, project a positive image of Downtown, and be a significant landmark to help visitors navigate.
- Traffic & Directional Signs—As unobtrusive and attractive as possible, while still meeting Department of Transportation guidelines for safety. Traffic signs must be developed using the Manual of Uniform Traffic Control Devices (MUTCD) published by the Federal Highway Administration. This type of signage was discussed in Section 4.8 on Page 39.
- Trailblazer Sign—Utilitarian purpose combined with unique branding and design elements. Area attractions to consider as destinations on Trailblazer Signs should include City Hall and other municipal offices, the Chamber of Commerce, the Farmer’s Market, public parking, and nearby recreational facilities. These signs should be located near key transportation nodes.
- Proximity Signs—Installed near attractions, these signs help visitors with their final steps to the destination.
- District Gateway Sign—Creates a boundary for a particular district within the Downtown, such as a historic or arts district. These signs can be used within the district to be defined and should reflect the size, scale and character of the existing architecture of the district.



## 5.0 NEXT STEPS

The City should continue collaborating with Downtown leaders. The City should consider launching an effort to develop building design standards for Downtown, including rehabilitation to existing buildings. These standards could be voluntary, but should help preserve Downtown buildings with significant architecture, while allowing innovative techniques to rehabilitate non-historic properties. Implementing design standards will require public support. The Downtown California Committee, Chamber of Commerce, and the City should provide an open dialog with Downtown property owners to explore design standards and inform citizens of current regulations. The City should base these standards upon the Secretary of the Interiors Standards for Rehabilitation; a summary of which is found in the Appendix as Exhibit A on page 69.

The City has made numerous public investments in Downtown and should continue with the effort to implement future phases to expand the High Street streetscape as funding becomes available. Additionally, the City should work to implement the conceptual wayfinding system for Downtown that is illustrated on pages 49 and 50 of this report. The other plans that follow in this section were prepared to illustrate potential updates to California's existing streetscape. The proposed projects in this section focus on the Courthouse Square, the Commerce Bank parking lot, the intersection of Buchanan Street and Route 87, and the Farmer's Market building improvements.

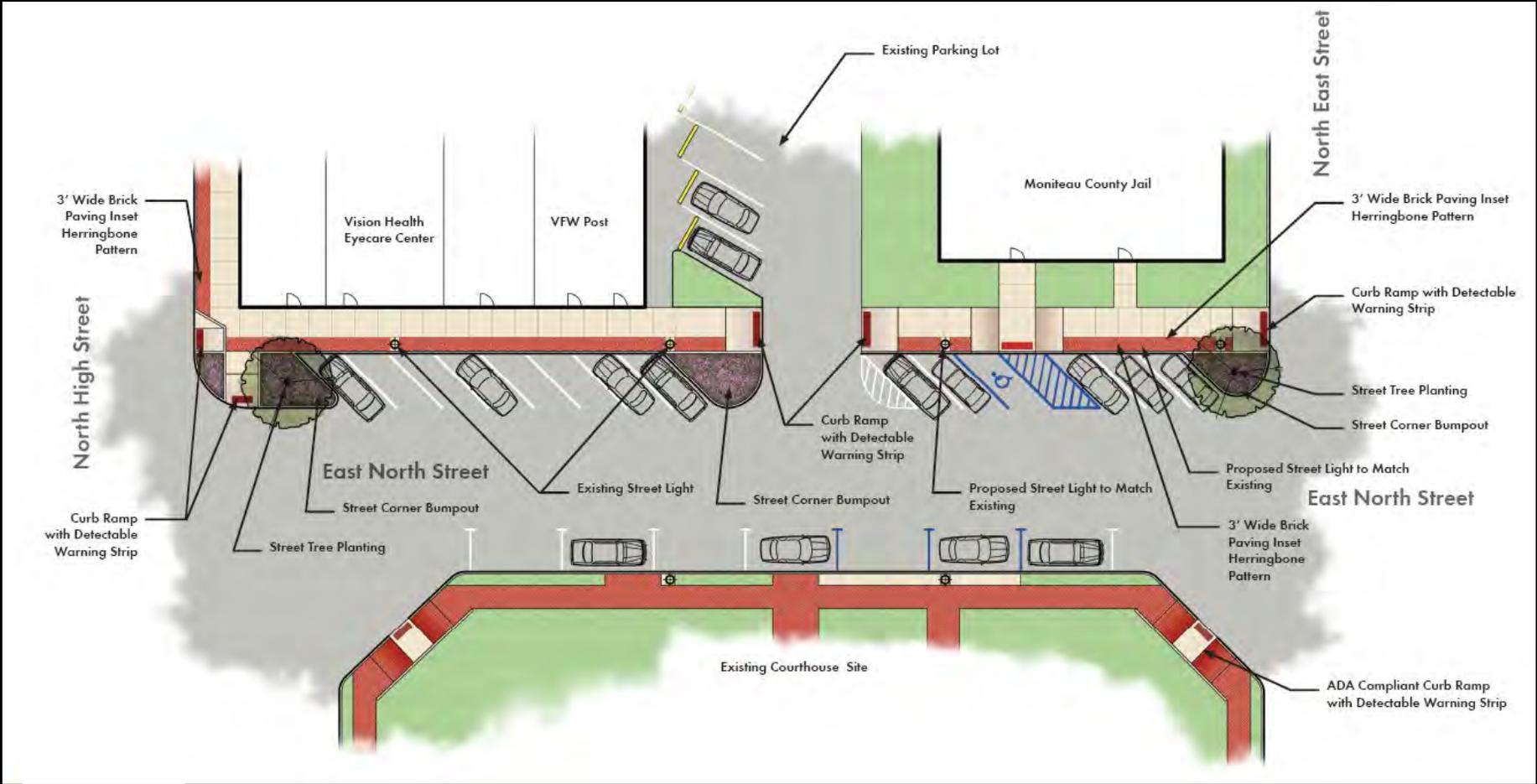
### 5.1 Courthouse Square Streetscape:

The following are proposed updates to the existing sidewalk master plan for the area around the Courthouse Square:

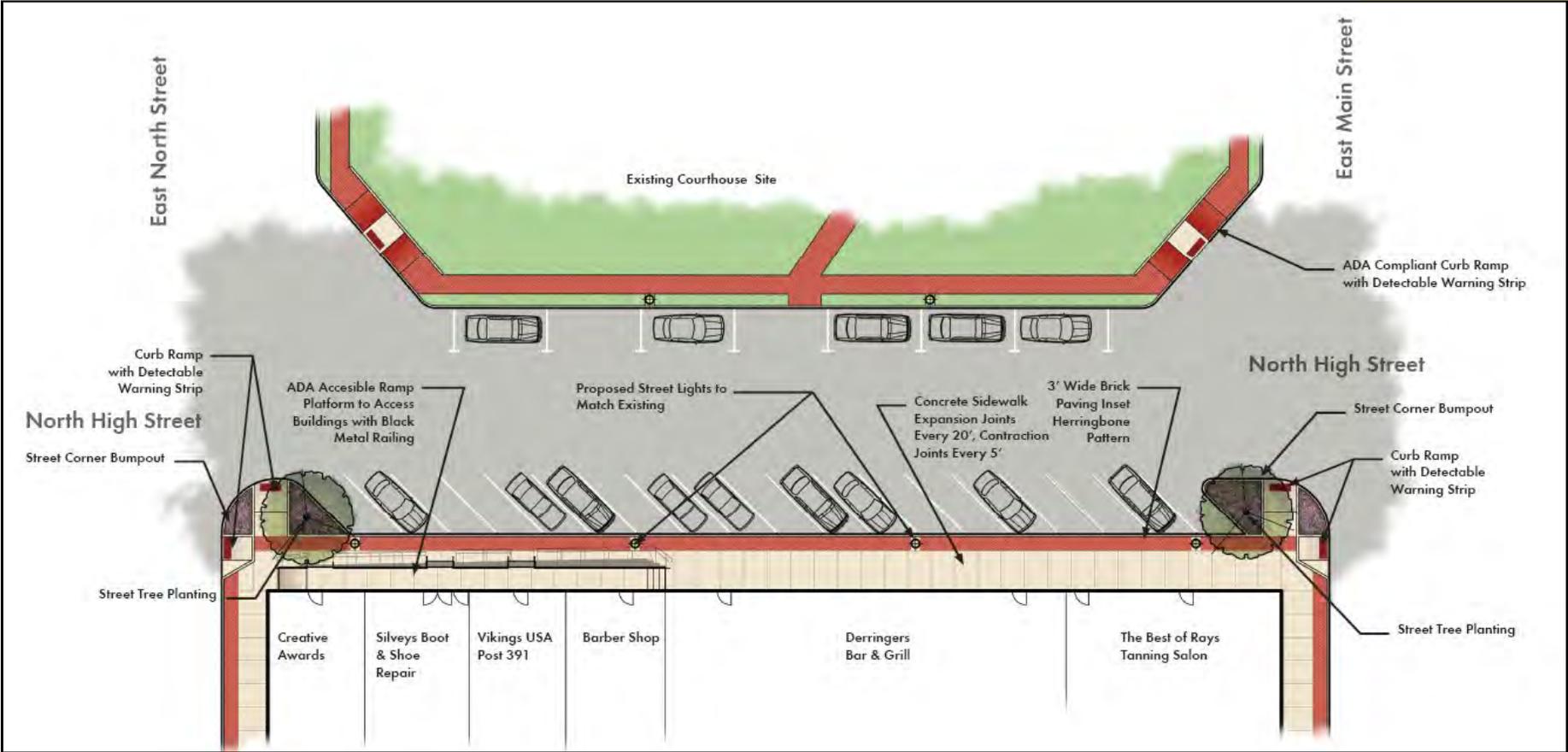
- Add street corner "bumpouts" with landscaping and street trees on the corner of each block.
- Add 2 curb ramps at each block "bumpout" and the four corners of the courthouse square to provide safe pedestrian crossing. The curb ramps are to be ADAAG compliant.
- Add 3' wide brick paver insert along the curb edge to provide a thematic design element.
- Retain existing decorative streetlights.
- ADA accessible ramp platform on west side of the courthouse square to provide universal access to the businesses noted on the plans.
- Relocate and add street decorative lights (to match existing style) as shown in plans.
- Consider decorative traffic signage.

The following pages in this section include five plans to illustrate the above proposed updates. The plans are divided into four sections: north, south, east, and west sides of the Courthouse Square. A street corner detail is also included.

North Courthouse Square



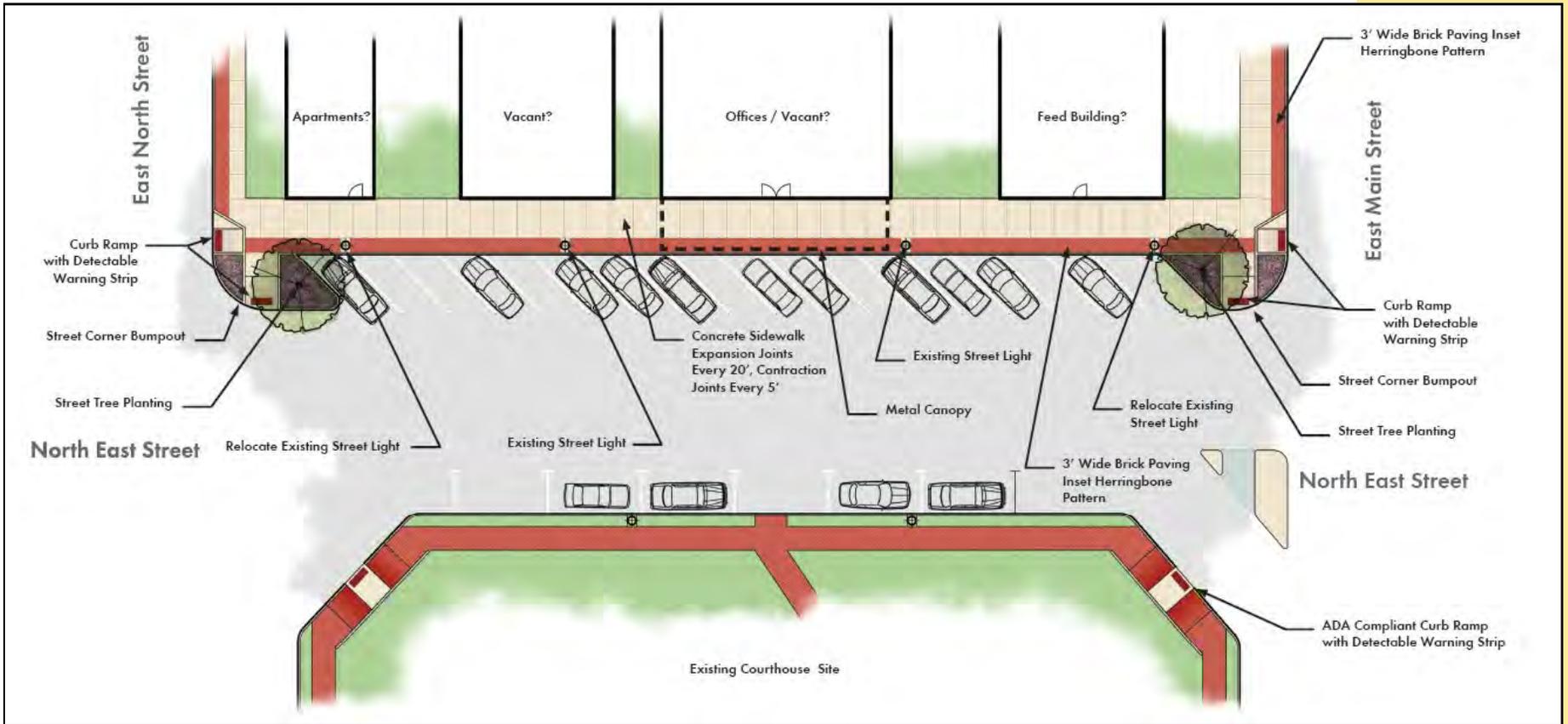
West Courthouse Square



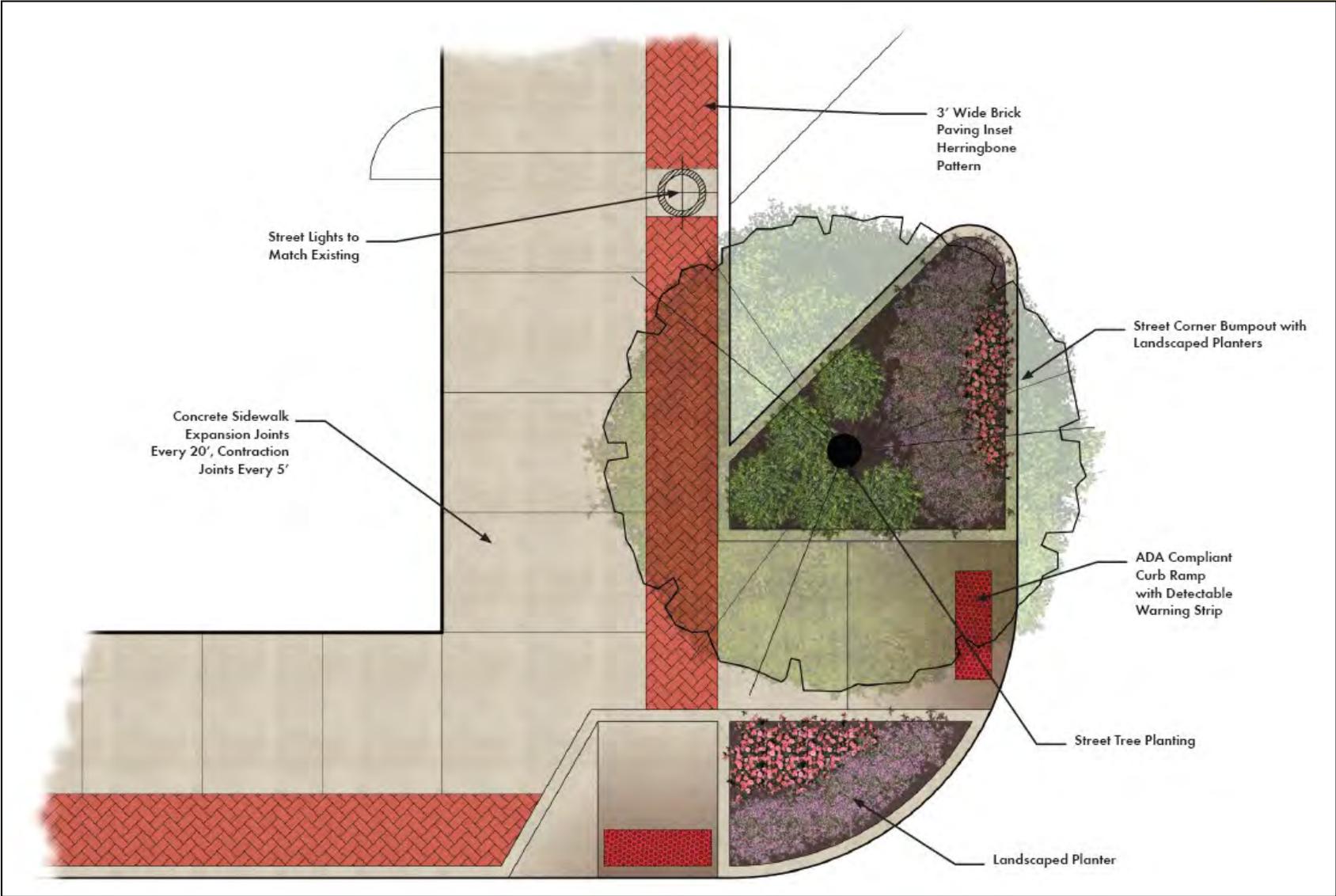
South Courthouse Square



East Courthouse Square



Street Corner Detail



## 5.2 Commerce Bank Parking Lot:

An often overlooked aspect of the downtown visitor's experience is the walk to and from the visitor's vehicle. Safe, well-maintained, well-lit, and appealing pedestrian walking paths between parking lots and businesses or structures can positively influence a visitor's perception of the health of a downtown.

The following are recommended streetscape and parking lot improvements to the area in and around the Commerce Bank parking lot in Downtown California. This parking lot serves a number of popular businesses and Downtown activity centers. These recommendations were formulated with the purpose of improving the pedestrian experience between the lot and the Finke Theatre or other nearby attractions. Recommendations are included below and illustrated on the plan found on the following page:

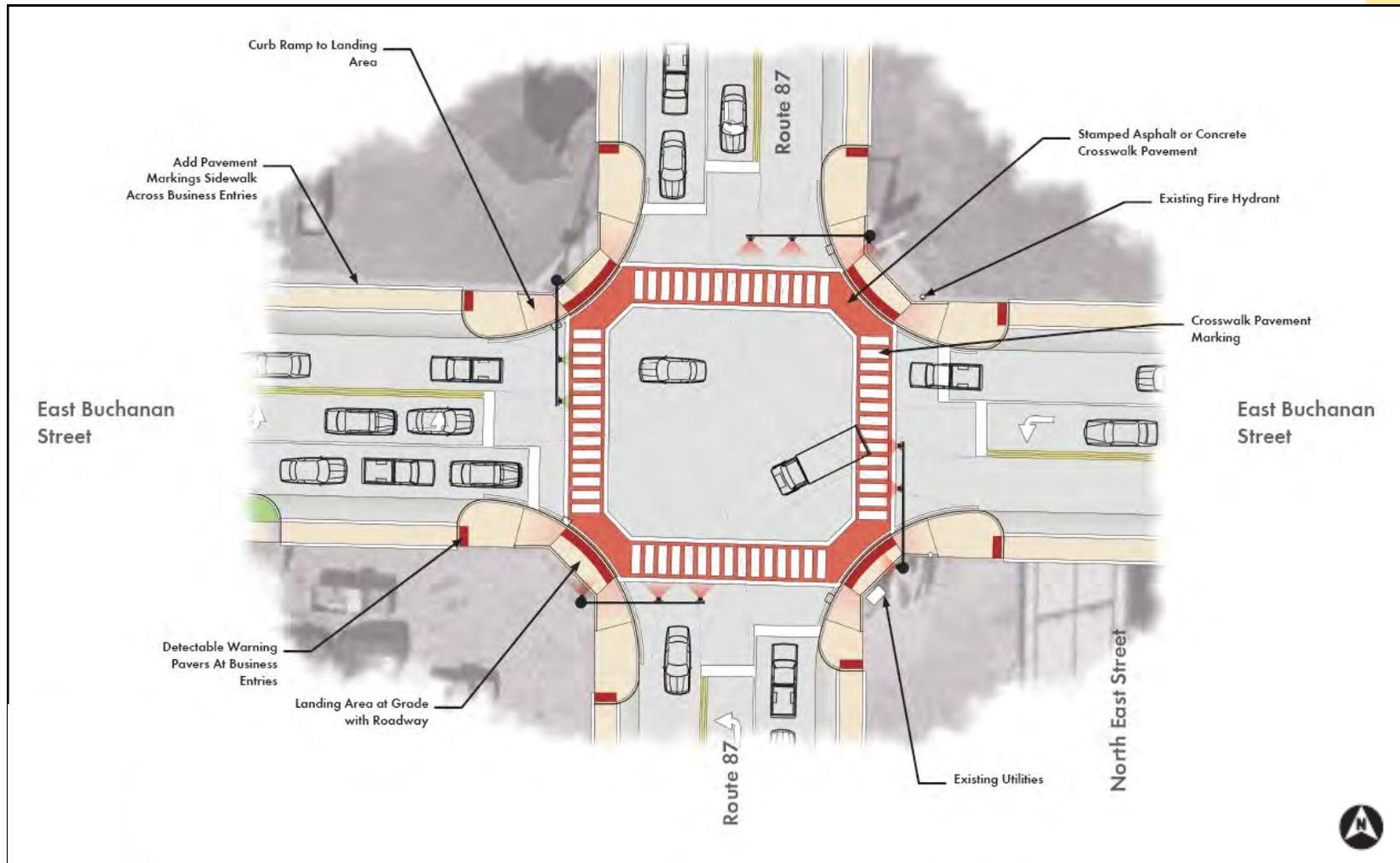
- Reconfigure parking layout and entry points.
- Include this lot in a Downtown wayfinding system.
- Add ADAAG compliant curb ramps at entry points.
- Add 3' wide brick paver insert along the curb edge to match Downtown thematic design elements.
- Retain existing decorative streetlights.
- Relocate and add street decorative lights (to match existing style) as shown in plans.
- Add landscaped parking islands to help define parking lot. (storm water capturing optional)

Commerce Bank Parking Lot Plan



### 5.3 Intersection of Buchanan Street and Route 87:

The intersection of Buchanan Street and Route 87 is a major entryway into Downtown California. This intersection should receive treatment similar to other streetscape work noted, but include stamped concrete or asphalt crosswalk pavement with crosswalk markings and sidewalk pavement markings at each business entry near the intersection. The plan illustration is shown below.



### 5.4 Farmer's Market Building and Site:

A potential location for this project is the property at the intersection of South Oak Street and Railroad Avenue. A robust Farmer's Market can be a significant activity generator for Downtown California. The proposed site plan and a building photo showing existing conditions is provided on this page. Potential building improvements to the front, rear, and south elevations are provided on the following pages.

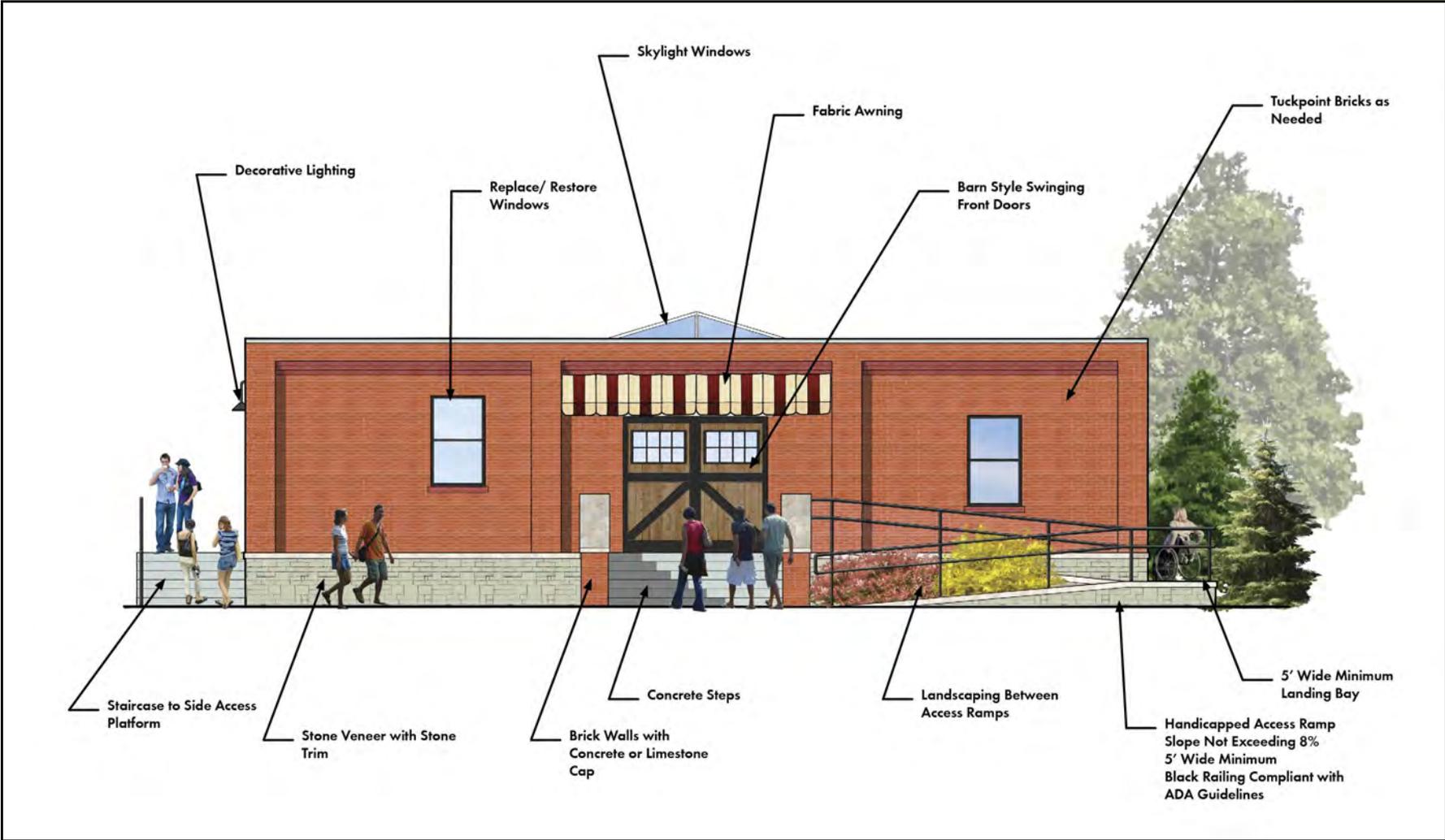
Existing Conditions.



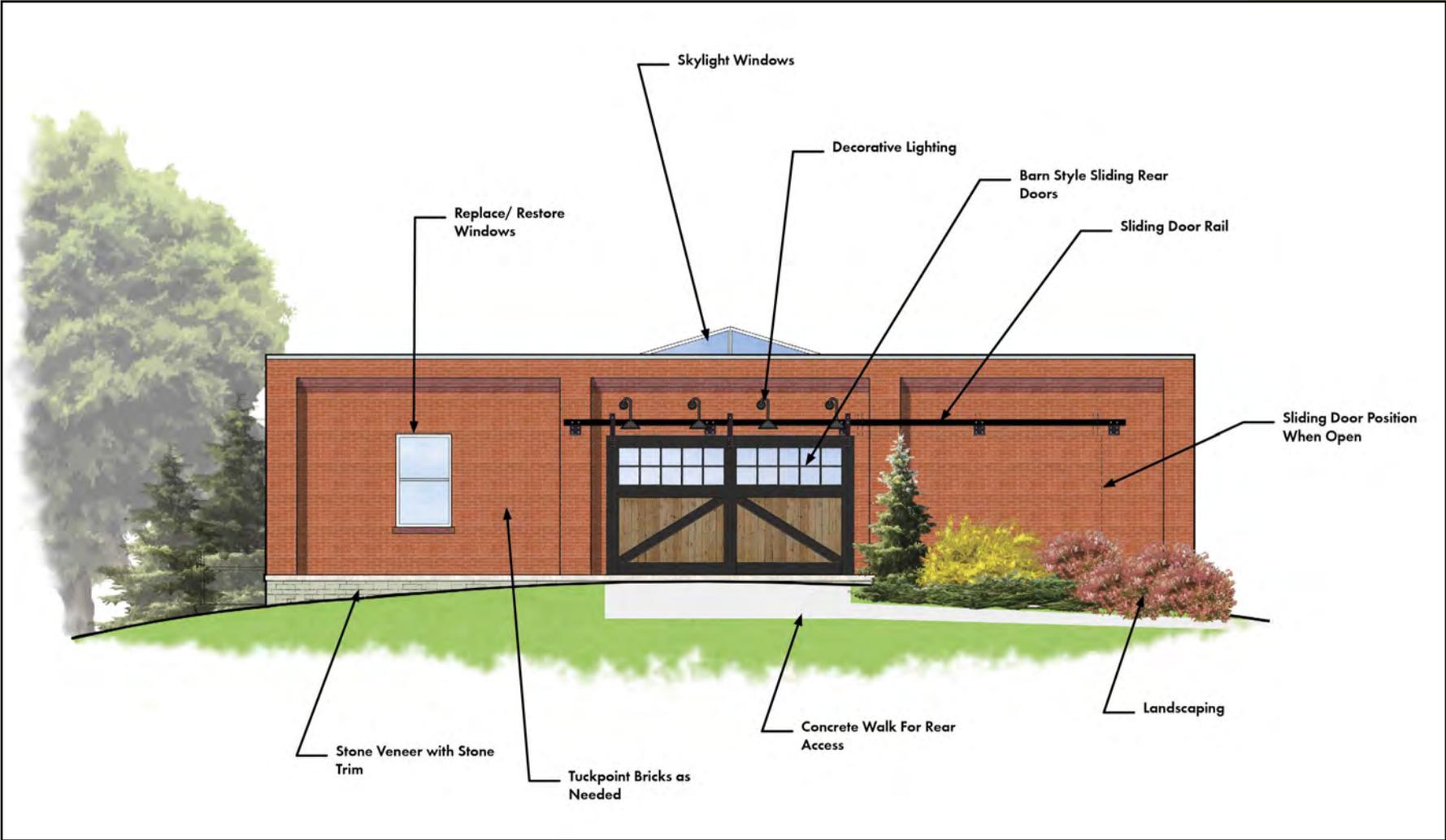
Potential Site Layout.



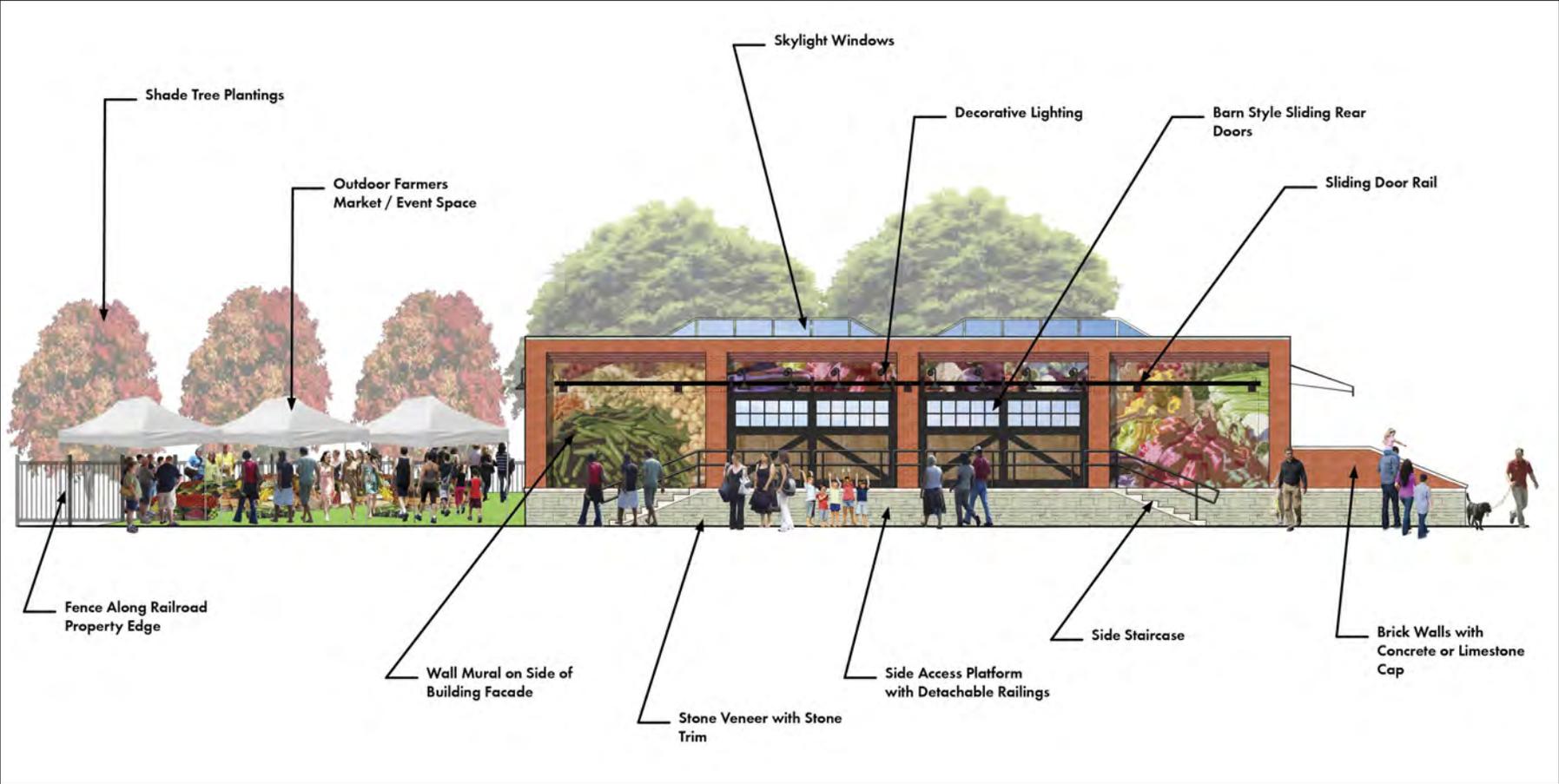
Proposed Farmer's Market Front Elevation



Proposed Farmer's Market Rear Elevation



Proposed Farmer's Market South Elevation



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APPENDIX

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## EXHIBIT A:

### SECRETARY OF THE INTERIOR STANDARDS FOR REHABILITATION (36 CFR Part 67)

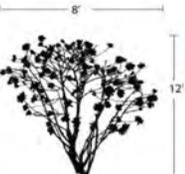
1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

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**EXHIBIT B:**

**LANDSCAPING PALETTES FOR MIDWEST DOWNTOWN PLANT SPECIES**

**SHRUB PLANTING PALETTE**

Species Information	Form / Ave. Size	Selected Images	
<p><b><i>Ilex verticillata</i></b>  <b>Winterberry</b>                      The Winterberry, a Missouri native, is a slow growing deciduous shrub adaptive to many diverse conditions. The species tolerates poorly drained and wet soils. This species is a deciduous holly with an upright-rounded form. Female cultivars will produce showy reddish-orange berries in fall after flowering and pollination from male plants. The Winterberry is best used in a shrub border, foundation planting, accent planting, or as a hedge.</p>			
<p><b><i>Callicarpa americana</i></b>  <b>Beautyberry</b>                      The Beautyberry is a native deciduous shrub that prefers medium soil and sun to part shade. Once established this species requires little care. This species produces an insignificant bloom in late summer that develop into small clusters of fruit. The profuse attractive fruit, a small berry-like drupe, is violet to magenta in color and persist to winter. The loose form of the Beautyberry lends itself to shrub massing and informal borders.</p>			
<p><b><i>Itea virginica</i></b>  <b>Virginia Sweetspire</b>                      The Sweetspire is a deciduous shrub, native to Missouri, that is tolerant of many soil and light conditions. The species prefers moist to wet soil conditions in full sun or part shade. The Sweetspire produces 2-5" long drooping flowers in late spring / early summer. The dark green leaves will turn to a red color in the fall. This plant is best used in moist areas as a shrub border or massing.</p>			
<p><b><i>Amelanchier x grandiflora 'Autumn Brilliance'</i></b>  <b>Serviceberry</b>                      The Serviceberry, a Missouri native, is tolerant of a wide range of soil and cultural conditions. This hybrid variety is a large multi-stemmed shrub. The species produces showy white flowers in early spring which lead to purplish-black berries in June. The leaves turn to an orangish-red color in fall.</p>			

RAIN GARDEN PLANTING PALETTE

Native Flower and Forb Species

Black-Eyed Susan - *Rudbeckia fulgida*



Marsh Milkweed - *Asclepias incarnata*



Southern Blue Flag - *Iris virginica*



Prairie Blazing Star - *Liatris pycnostachya*



Butterfly Milkweed - *Asclepias tuberosa*



Wild Bergamot - *Monarda fistulosa*



Foxglove Beardtongue - *Penstemon digitalis*



Ironweed - *Vernonia arkansana*



Native Grass, Rush, and Sedge Species

Bottlebrush Sedge - *Carex lurida*



Brown Fox Sedge - *Carex vulpinoidea*



Virginia Wild Rye - *Elymus virginicus*



Winter Scouring Rush - *Equisetum hyemale*



Common Rush - *Juncus effusus*



Prairie Dropseed - *Sporobolus heterolepis*



Tussock Sedge - *Carex stricta*

